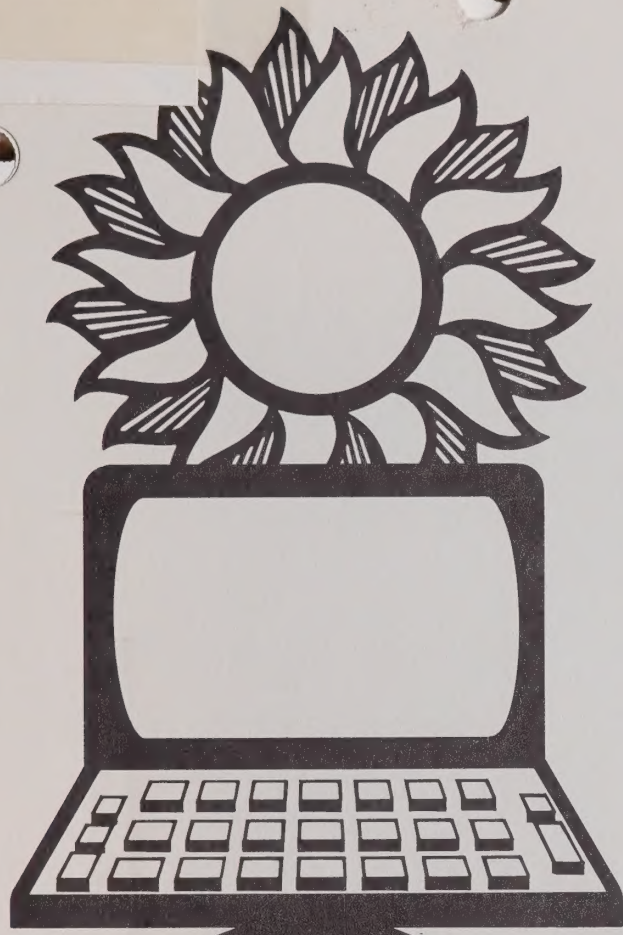


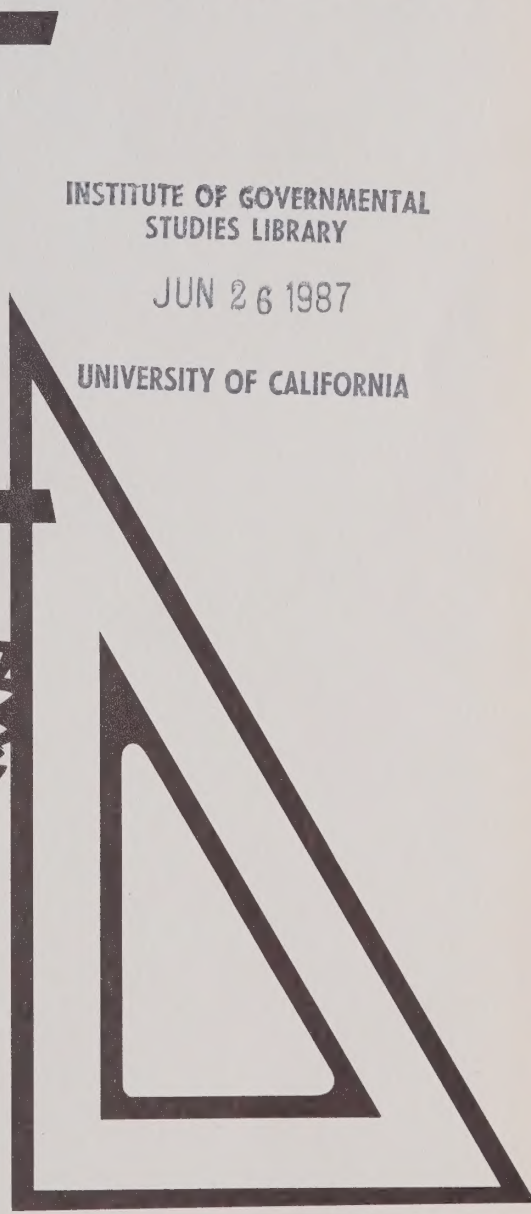
# Industrial Area Specific Plan



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## City of Rancho Cucamonga





## **INDUSTRIAL SPECIFIC PLAN AMENDMENTS**

The attached City Council Resolutions and Ordinances reflect amendments made to the Industrial Specific Plan since its adoption.

### **Resolution No. 83-110 (Adopted July 6, 1983)**

A Resolution of the City Council of the City of Rancho Cucamonga, amending the Industrial Area Specific Plan regarding Development Standards for Interim Uses.

### **Resolution No. 85-110 (Adopted July 24, 1985)**

A Resolution of Planning Commission of Rancho Cucamonga, California, recommending approval of Industrial Specific Plan Amendment 85-02, Section E, Parking and Loading Requirements (pg. III-26) to include defined interior building areas that can be deducted from the overall parking requirements; Section E.3, Parking Spaces Required (pg. III-29) to include a parking ratio for Research & Development Uses; and Table III-2, Land Use Definitions for Research and Development Uses and identification of the applicable Subareas (Table III-1).

### **Resolution No. 86-88 (Adopted June 25, 1986)**

A Resolution of the Planning Commission of the City of Rancho Cucamonga, California recommending the adoption of revisions to the specific plan for the Industrial Area.

### **Ordinance No. 249 (Adopted March 6, 1985)**

An Ordinance of the City Council of the City of Rancho Cucamonga amending Part III of the Industrial Area Specific Plan to establish a Haven Avenue Overlay District located on both sides of Haven Avenue between Foothill Boulevard and 4th Street. (Incorporated into I.S.P.)

### **Ordinance No. 272 (Adopted September 4, 1985)**

An Ordinance of the City Council of the City of Rancho Cucamonga, approving Industrial Area Specific Plan Amendment 85-02, amending the Industrial Area Specific Plan regarding parking standards for research and development and delineating interior building areas that can be deducted from parking requirements.

### **Ordinance No. 297 (Adopted September 17, 1986)**

An Ordinance of the City Council of the City of Rancho Cucamonga, California, adopting revisions to the Industrial Area Specific Plan.

RESEARCH DESIGN AND METHODS

The research was designed to investigate the relationship between the variables of interest and the outcome variable.

The study was conducted in a controlled environment.

The data was collected through a series of experiments and observations.

The results of the study are presented in the following sections.

The first section discusses the theoretical background of the study. The second section describes the research design and methods. The third section presents the results of the study. The fourth section discusses the implications of the findings.

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RESOLUTION NO. 83-110

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF RANCHO  
CUCAMONGA, AMENDING THE INDUSTRIAL AREA SPECIFIC PLAN  
REGARDING DEVELOPMENT STANDARDS FOR INTERIM USES

WHEREAS, the Planning Commission held a duly advertised public hearing in order to consider amending the Industrial Area Specific Plan; and

WHEREAS, the Planning Commission did recommend amending the Industrial Area Specific Plan to provide for interim uses; and

WHEREAS, the City Council held a duly advertised public hearing in order to consider amending the Industrial Area Specific Plan.

NOW, THEREFORE, BE IT RESOLVED, that the City Council of the City of Rancho Cucamonga does hereby amend the Industrial Area Specific Plan as follows:

SECTION 1: That Part III, Section II (Page 111-2), Land Use Types, be amended to include interim uses, as follows:

Interim Uses The following Interim uses shall be permitted, provided that such uses and their locations shall not preclude full development in accordance with the development regulations of the Plan and no permanent buildings are constructed. Agricultural uses, including roadside stands, and private parks and picnic areas are permitted uses prior to development. Parking and storage areas, park-and-ride lots, and other uses similar in character shall be permitted.

A Conditional Use Permit shall be approved for a two (2) year period. Extensions may be granted up to an additional 36 months for a maximum time limit not to extend more than five (5) years beyond the original date of approval.

As a condition of approval, an agreement between the City and applicant shall be completed stipulating timing, installation of permanent improvements and buildings, and/or restoration of the site to its original condition. At the end of five (5) years, the use shall be removed or the site developed in accordance with full development regulations of the Plan.

SECTION 2: That Part III, Section III (Page III-33), Development Standards, be amended to add subsection H, as follows:

H. Interim Use Standards

The purpose of interim use standards is to establish minimum standards for setbacks, landscaping, screening, and parking which meet the intent of the Plan. Unless specifically modified through the Conditional Use Permit, all other

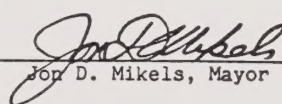
development standards of the City of Rancho Cucamonga shall apply, including, but not limited to, grading and drainage, and street improvements. The following standards shall apply in all areas of the Plan:

- H.1. The minimum streetscape and parking setback requirement shall be contiguous with the ultimate right-of-way line, but in no case less than ten (10) feet.
- H.2. There shall be no minimum landscape coverage requirement, except that which is necessary for screening purposes as determined by the City Planner.
- H.3. All parking and storage areas shall be paved with slag, crushed aggregate, asphaltic concrete or concrete. The location, number and design of said parking shall be in accordance with the Plan.
- H.4. All parking and storage areas, and other interim uses which require screening as determined by the City Planner, shall be screened from public view through a combination of landscaping and fencing. Fencing may include a 6' chain link fence with slats, masonry or concrete, wood, or decorative metal. Screening must be maintained in good condition at all times.
- H.5. Landscaping required for screening purposes shall include 15-gallon trees and 5-gallon shrubs to provide a dense landscape buffer to afford maximum screening from the public view, satisfactory to the City Planner.

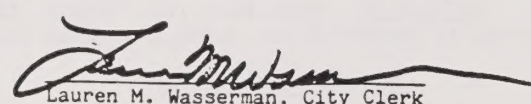
BE IT FURTHER RESOLVED, that the City Council finds that this amendment will not cause significant adverse impacts upon the environment and issues a Negative Declaration for this Amendment.

PASSED, APPROVED, and ADOPTED this 6th day of July, 1983.

AYES: Dahl, Buquet, Schlosser, Frost, Mikels  
NOES: None  
ABSENT: None

  
Jon D. Mikels, Mayor

ATTEST:

  
Lauren M. Wasserman, City Clerk



RESOLUTION NO. 85-110

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF RANCHO CUCAMONGA, CALIFORNIA, RECOMMENDING APPROVAL OF INDUSTRIAL SPECIFIC PLAN AMENDMENT 85-02, SECTION E., PARKING AND LOADING REQUIREMENTS (pg. III-26) TO INCLUDE DEFINED INTERIOR BUILDING AREAS THAT CAN BE DEDUCTED FROM THE OVERALL PARKING REQUIREMENTS; SECTION E.3, PARKING SPACES REQUIRED (pg. III-29) TO INCLUDE A PARKING RATIO FOR RESEARCH & DEVELOPMENT USES; AND TABLE III-2, LAND USE DEFINITIONS FOR RESEARCH AND DEVELOPMENT USES AND IDENTIFICATION OF THE APPLICABLE SUBAREAS (TABLE III-1).

WHEREAS, on the 22nd day of May, 1985, the Planning Commission directed staff to prepare an amendment to the Industrial Area Specific Plan; and,

WHEREAS, on the 24th day of July, 1985, the Planning Commission held a duly advertised public hearing.

SECTION 1: The Rancho Cucamonga Planning Commission has made the following findings:

1. That the amendment is warranted in order to provide a parking ratio for Research and Development uses and define interior building areas that can be deducted from the overall parking requirements.
2. That the proposed amendment would not have significant impacts on the environment; and,
3. That the proposed amendment is consistent with the policies of the General Plan.

SECTION 2: The Rancho Cucamonga Planning Commission has found that this amendment will not create a significant adverse impact on the environment and recommends issuance of a Negative Declaration on July 24, 1985.

NOW, THEREFORE, BE IT RESOLVED:

1. The Planning Commission hereby recommends that the City Council approve and adopt Industrial Area Specific Plan Amendment 85-02 regarding a parking ratio for research and development uses and define interior building areas that can be deducted from the general parking requirements.
2. That a certified copy of this resolution and related material hereby adopted by the Planning Commission shall be forwarded to the City Council.

APPROVED AND ADOPTED THIS 24th DAY OF JULY, 1985.

PLANNING COMMISSION OF THE CITY OF RANCHO CUCAMONGA

BY:

Dennis L. Stout  
Dennis L. Stout, Chairman

ATTEST:

Jack Lam  
Jack Lam, Secretary

I, Jack Lam, Secretary of the Planning Commission of the City of Rancho Cucamonga, do hereby certify that the foregoing Resolution was duly and regularly introduced, passed, and adopted by the Planning Commission of the City of Rancho Cucamonga, at a regular meeting of the Planning Commission held on the 24th day of July, 1985, by the following vote-to-wit:

AYES:	COMMISSIONERS:	CHITIEA, MCNIEL, BARKER, STOUT
NOES:	COMMISSIONERS:	NONE
ABSENT:	COMMISSIONERS:	REMPEL



RESOLUTION NO. 86-88

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF RANCHO CUCAMONGA, CALIFORNIA RECOMMENDING THE ADOPTION OF REVISIONS TO THE SPECIFIC PLAN FOR THE INDUSTRIAL AREA

WHEREAS, it is the desire of the City to encourage an orderly and planned system of growth throughout the industrial area of the City, and

WHEREAS, a Specific Plan for the industrial area was adopted by the City in 1981 to provide for such a system of growth, and

WHEREAS, planning law of the State of California provide for the City's adoption of a Specific Plan which sets forth land uses and standards for development and any revisions thereto, and

WHEREAS, the City of Rancho Cucamonga Planning Commission held duly advertised public hearings to solicit public review and comment on the Draft Revisions to the Industrial Area Specific Plan and Environmental Assessment in accordance with Government Code Section 65450, and

WHEREAS, full consideration has been given to each comment or request made concerning the Draft Revision to the Industrial Specific Plan and Environmental Assessment, and

WHEREAS, all items have been accomplished in accordance with the State Law of California regarding adoption of the Specific Plan and granting of a Negative Declaration.

NOW, THEREFORE BE IT RESOLVED, that the Planning Commission of the City of Rancho Cucamonga certifies that the Initial Environmental Study prepared for the Revision to the Industrial Area Specific Plan as being an adequate review of the potential significant environmental impacts and that a Negative Declaration is granted, and

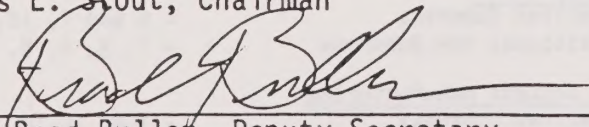
BE IT FURTHER RESOLVED, that the Planning Commission of the City of Rancho Cucamonga recommends adoption by the City Council of the Revision to the Industrial Area Specific Plan as prepared by staff and amended by the Planning Commission.

APPROVED AND ADOPTED THIS 25TH DAY OF JUNE, 1986.

PLANNING COMMISSION OF THE CITY OF RANCHO CUCAMONGA

BY: 

Dennis L. Stout, Chairman

ATTEST: 

Brad Buller, Deputy Secretary



AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF RANCHO CUCAMONGA, CALIFORNIA, APPROVING INDUSTRIAL AREA SPECIFIC PLAN AMENDMENT 85-02, AMENDING THE INDUSTRIAL AREA SPECIFIC PLAN REGARDING PARKING STANDARDS FOR RESEARCH & DEVELOPMENT AND DELINEATING INTERIOR BUILDING AREAS THAT CAN BE DEDUCTED FROM PARKING REQUIREMENTS.

WHEREAS, the Planning Commission held a duly advertised public hearing on July 24, 1985, in order to consider amending the Industrial Area Specific Plan; and,

WHEREAS, the Planning Commission did recommend amending the Industrial Area Specific Plan to provide for a parking ratio for research and development uses and deductible areas from parking requirements; and

WHEREAS, the City Council held a duly advertised public hearing in order to consider amending the Industrial Area Specific Plan.

NOW, THEREFORE, BE IT RESOLVED, that the City Council of the City of Rancho Cucamonga does hereby amend the Industrial Area Specific Plan as follows:

SECTION 1: That Part III, Table III-2 (Page III-6), Land Use Types Definitions, be amended to define Research and Development and two subcategories:

1. Research and Development: Uses engaged in the research, design, analysis, development, and/or testing of a product.
2. Research & Development/Office: Activities typically include, but not limited to; building/use types resembling office more than research and development facilities, such as labs, acoustical chambers, microwave testing chamber, wind tunnels, circuit board assembly rooms, "clean rooms", and computer facilities. Such uses are typically less intense than pure office, but more intense than traditional industrial space. The uses do not produce odors, noise, vibration, or particulates which would adversely affect uses in the same structure or on the same site. Where 24 hours on-site surveillance is necessary, a caretaker's residence may be permitted when approved by a Conditional use Permit.
3. Research & Development/Light Manufacturing: Activities typically include, but not limited to: "high-tech" production and assembly operations which include some R & D and office space, "product assembly rooms" and other facilities associated with high technology manufacturing. The uses do not produce odors, noise, vibration, or particulate which would adversely affect uses in the same structure or on the same site. Where 24 hours on-site surveillance is necessary, a caretaker's residence may be permitted when approved by a Conditional Use Permit.

SECTION 2: That Part III, Table III-1 (page III-5), summary of land use type by Subarea, be amended to include research and development uses in the Industrial Park and General Industrial categories as follows:

<u>R &amp; D/Office</u>	
Permitted Subareas	- 6 and 7, 12, 16
Conditional Use Subareas	- 1, 3, 4, 8, 11, 13, 14
<u>R &amp; D/Light Manufacturing</u>	
Permitted Subareas	- 1-8, 10-14, and 16



SECTION 3: That Section E.3 Parking Spaces Required (page III-29) be amended to add Subsection e, as follows:

e. Research & Development: 1 space per 350 Square Feet

SECTION 4: That Section E.3, Parking Spaces Required (page III-29) be amended to add Subsection f as follows:

f. Following interior building areas can be deducted from the overall parking requirements: electrical/mechanical rooms, elevator shafts, stairwells, and multi-story lobbies.

BE IT FURTHER RESOLVED, that the City Council finds that this amendment will not cause significant adverse impacts upon the environment and issues a Negative Declaration for this amendment.

SECTION 5: The Mayor shall sign this Ordinance and the City Clerk shall cause the same to be published within fifteen (15) days after its passage at least once in The Daily Report, a newspaper of general circulation published in the City of Ontario, California, and circulated in the City of Rancho Cucamonga, California.

PASSED, APPROVED, and ADOPTED this 4th day of September, 1985.

AYES: Wright, Buquet, Mikels, Dahl, King

NOES: None

ABSENT: None


  
Jon D. Mikels, Mayor

ATTEST:

  
Beverly A. Authelet, City Clerk

I, BEVERLY A. AUTHELET, CITY CLERK of the City of Rancho Cucamonga, California, do hereby certify that the foregoing Ordinance was introduced at a regular meeting of the Council of the City of Rancho Cucamonga held on the 21st day of August, 1985, and was finally passed at a regular meeting of the City Council of the City of Rancho Cucamonga held on the 4th day of September, 1985.

Executed this 5th day of September, 1985 at Rancho Cucamonga, California.

  
Beverly A. Authelet, City Clerk





ORDINANCE NO. 297

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF RANCHO CUCAMONGA, CALIFORNIA, ADOPTING REVISIONS TO THE INDUSTRIAL AREA SPECIFIC PLAN

WHEREAS, it is the desire of the City to encourage an orderly and planned system of growth throughout the industrial area of the City; and

WHEREAS, a Specific Plan for the industrial area was adopted by the City in 1981 to provide for such a system of growth; and

WHEREAS, planning law of the State of California provide for the City's adoption of a Specific Plan which sets forth land uses and standards for development and any revisions thereto; and

WHEREAS, the City of Rancho Cucamonga City Council and Planning Commission held duly advertised public hearings to solicit public review and comment on the Draft Revisions to the Industrial Area Specific Plan and Environmental Assessment in accordance with Government Code Section 65450; and

WHEREAS, full consideration has been given to each comment or request made concerning the Draft Revision to the Industrial Area Specific Plan and Environmental Assessment; and

WHEREAS, the Planning Commission recommended to the City Council the certification of a Negative Declaration of the Environmental Assessment and adoption of the Revisions to the Industrial Area Specific Plan; and

WHEREAS, the City Council does hereby certify the Environmental Assessment as a complete and final statement of environmental impacts of the proposed project and a Negative Declaration is approved; and

WHEREAS, the City Council finds that the project is in conformance with the General Plan for the City.

NOW, THEREFORE, BE IT RESOLVED, that the City Council of the City of Rancho Cucamonga does hereby adopt the Revisions to the Industrial Area Specific Plan attached hereto as Exhibit "A".

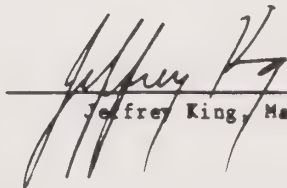
The Mayor shall sign this Ordinance and the City Clerk shall cause the same to be published within fifteen (15) days after its passage at least once in The Daily Report, a newspaper of general circulation published in the City of Ontario, California, and circulated in the City of Rancho Cucamonga, California.

PASSED, APPROVED, and ADOPTED this 17th day of September, 1986.

AYES: Mikels, Buquet, King, Dahl, Wright

NOES: None

ABSENT: None

  
Jeffrey King, Mayor

ATTEST:

  
Beverly A. Authalet, City Clerk



**INDUSTRIAL AREA SPECIFIC PLAN**

FOR THE

CITY OF RANCHO CUCAMONGA

(REVISED)

Recommended for Adoption by  
City of Rancho Cucamonga Planning Commission

By  
Resolution No. 81-82, July 22, 1981  
Resolution No. 86-88, June 25, 1986 (Revision)

Adopted by City of Rancho Cucamonga City Council

By  
Resolution No. 81-128, August 19, 1981  
Ordinance No. 297, September 17, 1986 (Revision)





CITY COUNCIL

Dennis L. Stout, Mayor  
Charles J. Buquet, II                      Jeffrey King  
Deborah Brown                              Pamela J. Wright  
Lauren Wasserman, City Manager

PLANNING COMMISSION

E. David Barker, Chairman  
Larry McNeil, Vice Chairman  
Suzanne Chitrea  
Peter Tolstoy

INDUSTRIAL EXECUTIVE COMMITTEE OF  
THE CHAMBER OF COMMERCE

Paul R. Mindrum, Chairman  
Jack Corrigan                              Richard Ortwein  
Joe DiIorio                                  Jack McNay  
Bob Howes                                  Jeffery Sceranka  
Michael Jauron                              Jim Westling

Special Thanks to:

Rancho Cucamonga Chamber of Commerce  
Industrial League

## STAFF PARTICIPANTS

### **1981**

Tim J. Beedle, Senior Planner, Project Manager  
Jerry Grant, Building Official  
Dan Hendryks, Assistant Planner, Research  
Karl Hill, Planning Aide, Graphics  
Lloyd Hubbs, City Engineer  
Joan Kruse, Administrative Secretary  
Steve McCutchan, Associate Planner, Research, Graphics  
Terry Nemer, Traffic Analyst  
Janice Reynolds, Secretary  
Karen Paul, Clerk Typist, Mag Card  
Paul Rougeau, Senior Civil Engineer  
Michael Vairin, Senior Planner, Current Planning Liaison

### **1986 Update**

Otto Kroutil, Senior Planner, Project Manager  
Dan Coleman, Senior Planner, Current Planning Liaison  
Alan Warren, Associate Planner, Project Planner  
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Cynthia S. Kinser, Assistant Planner, Graphics  
Janice Reynolds, Planning Commission Secretary  
Nancy Sasse, Office Assistant  
Kathy Van Der Sluis, Office Assistant  
Kelly Orta, Office Assistant



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## INTRODUCTION

The purpose of the Rancho Cucamonga Industrial Specific Plan is to provide objectives, standards and guidelines for development in the industrial area in an efficient, orderly and attractive manner. The Specific Plan for the Industrial Area is prepared in accordance with California State requirements on a specific plan, Government Code Section 65451. California law requires that a specific plan include detailed regulations, conditions, programs and proposed legislation necessary for the systematic implementation of each element of the General Plan.

The Specific Plan is divided into five parts and an appendix. These five parts are described as follows:

- Part I - Regional and Community Background, describing the location of the City within the West Valley area of San Bernardino County and the location of the Industrial Area within the City.
- Part II - Industrial Area Development Framework, describing the overall organizational concept of the industrial area including land use, access and circulation, urban design, flood management, public services and housing implementation.
- Part III - Development Standards and Guidelines, describing specific land use definitions, development standards, and urban design and architectural guidelines.
- Part IV - Subarea Development Standards, providing a summary of standards which apply within each of the 17 subareas in the industrial area.
- Part V - Implementation Program, describing the regulatory procedures and potential sources of financing.





# **PART I**

## **REGIONAL AND COMMUNITY BACKGROUND**

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## PART I

### REGIONAL AND COMMUNITY BACKGROUND

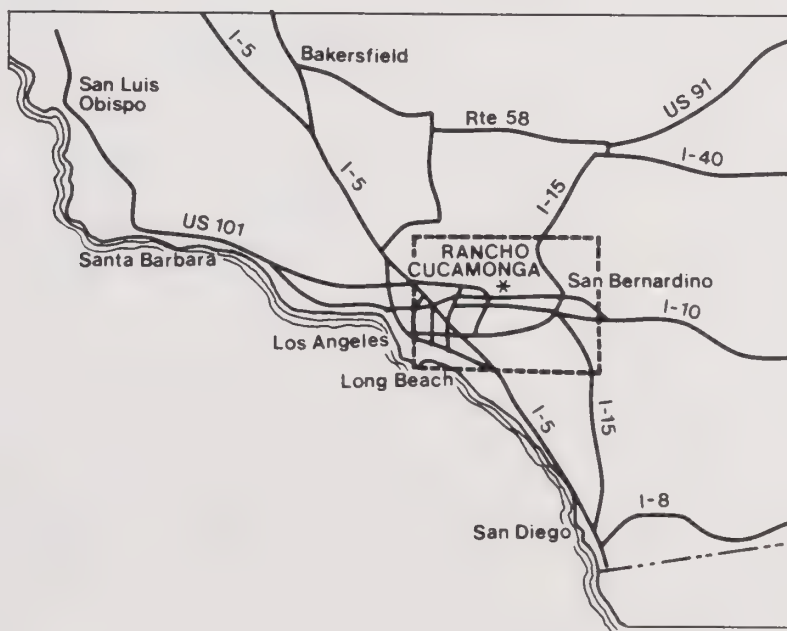
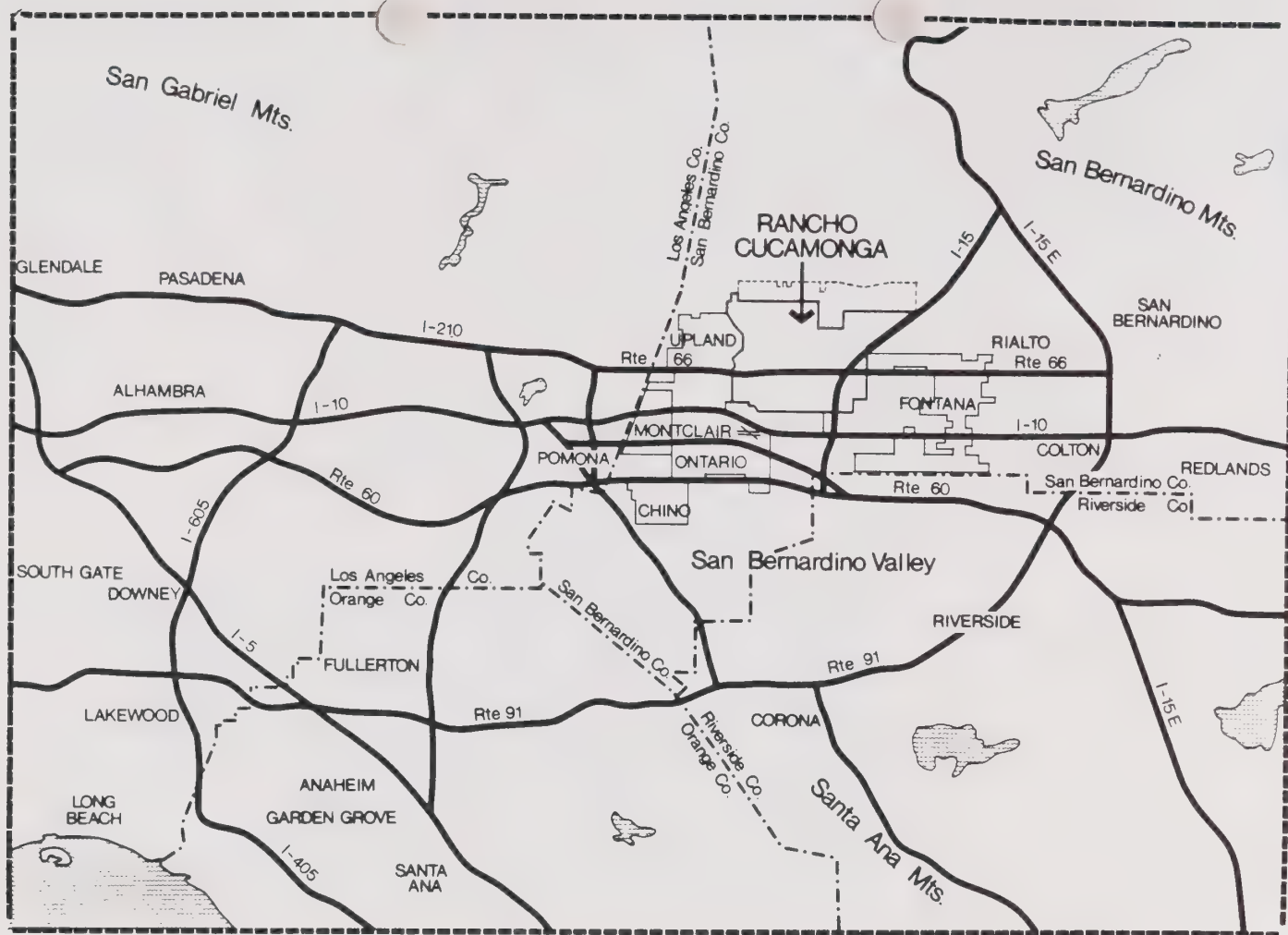
#### I. REGIONAL SETTING

The City of Rancho Cucamonga, located in the southwest corner of San Bernardino County, is 37 miles east of downtown Los Angeles and 15 miles west of downtown San Bernardino. The San Gabriel Mountains rise majestically to the north and the communities of Upland, Ontario, and Fontana surround the City on the west, south, and east, respectively. In a region known as the West Valley, Rancho Cucamonga is strategically located at the hub of an extensive transportation network and within short commute distance of major employment centers in Southern California (see Figure I-1).

Much of the metropolitan development pressures in Southern California were directed toward Orange County in the 1970's. However, because of a rapid decrease in land supply combined with the constraints imposed by the area's transportation network capacity, the development pressures are being channeled to the West Valley region.

The entire West Valley region has experienced strong growth pressures from Los Angeles and Orange Counties. As of January 1, 1986, population estimate for Rancho Cucamonga is 73,842 persons. During the six year period between 1980-85, population growth was greater than 33.7%.

A review of development proposals in the region clearly indicates the area's attractiveness for industrial uses. Major projects are being reviewed by Ontario, Rialto, Pomona, and Upland as well as Rancho Cucamonga. Industrial growth has been most significant recently. By mid-1986 approximately 4.4 million square feet of new industrial space received approval.



**Figure I-1**  
**REGIONAL SETTING**

The excellent freeway and railroad networks that surround Rancho Cucamonga have been major drawing factors. In addition, the Ontario International Airport is currently upgrading and expanding its facilities. The growth projections for the airport can be expected to reflect the growth of the area as a major employment and industrial center. In 1985, the airport handled over 3.65 million passengers and 176,000 tons of air cargo freight.

## II. COMMUNITY SETTING

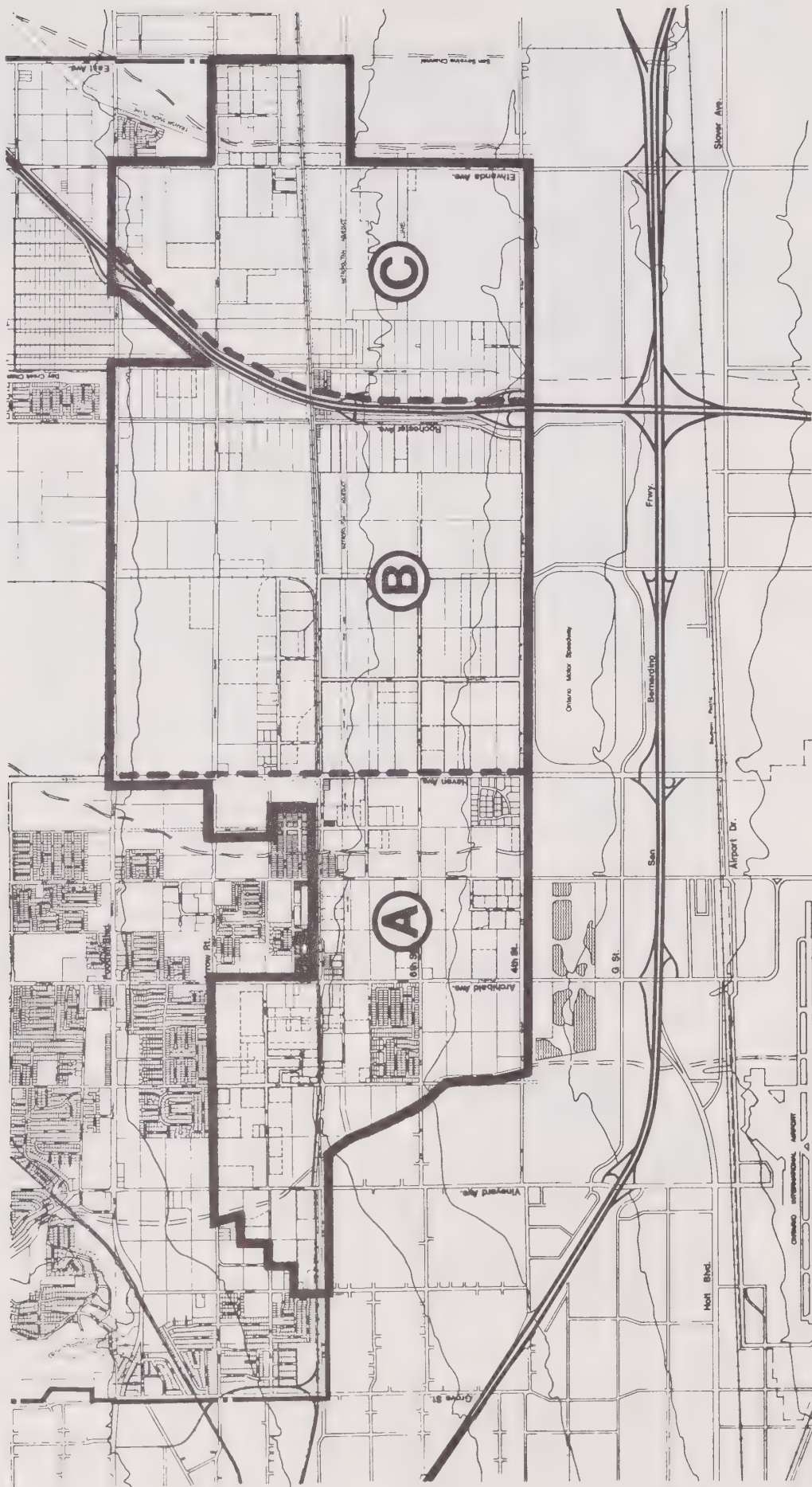
The Industrial Area of the City of Rancho Cucamonga is located in the southern portion of the City, encompassing nearly 5,000 acres. The Industrial Area has been divided into three zones (see Figure I-2).

Generally, Zone A is bounded on the north by Arrow Route and the North Town area, on the east by Haven Avenue, on the south by 4th Street and on the west by Cucamonga Creek and the City of Ontario. As of 1979, Zone A contained 370 acres of industrial uses, nearly 40 percent of the City's total existing currently developed acreage. There are additional 630 acres of vacant land currently zoned for industrial activities.

Zone B is bounded on the north by Foothill Boulevard, on the east by Devore Freeway (I-15), on the south by 4th Street and the City of Ontario, and on the west by Haven Avenue. Zone B is largely undeveloped--nearly 90 percent of this 2,300 acre area is vacant or abandoned vineyards. Much of the existing 220 acres of industrial activity is focused in an area bounded by the railroad and Arrow Route. Because of the availability of land and the area's excellent transportation access, this zone is experiencing the greatest development pressure of the three industrial zones.

Zone C is bounded on the north by Foothill Boulevard, on the east by Etiwanda and East Avenues, on the south by 4th Street and the City of Ontario, and on the west by Devore Freeway. Zone C is characterized by heavy industrial uses such as the Southern California Edison Power plant and rail-related activities. Within Zone C, there are over 340 acres of existing industrial uses, and an additional 360 acres of land zoned for industrial activities.





**FIG. I-2 INDUSTRIAL AREAS**



City-wide, the potential for industrial development is clearly manifested by the 1200 acres of vacant industrially zoned land. In addition, more than 2,500 acres, predominantly abandoned vineyards, lie within the industrial area that can be converted to industrial lands. The table below summarizes the existing and potential industrial development in each of the three zones.

INDUSTRIAL LAND USE SUMMARY FOR RANCHO CUCAMONGA INDUSTRIAL AREA - 1979			
Zone	Existing Developed	Vacant	Total
A	505 Acres	735	1240
B	240 Acres	1940	2180
C	393 Acres	922	1315
Total Acreage 1138		3537	4735

Aside from land availability and convenient access, industrial developers look for locations where their employees can be housed nearby. The undeveloped area north of Zone B, north of Foothill Boulevard, between Haven Avenue and Etiwanda Avenue is planned for two major planned community proposed for up to 18,000 housing units, and to include a major regional shopping center of more than one million square feet of gross leasable floor space. These projects, along with the development of the regional transportation system, are transforming the area's "bedroom communities" to economically viable cities.

### III. ISSUES, CONSTRAINTS AND OPPORTUNITIES

The combination of regional and community factors, land availability, excellent transportation, and adequate supply, contribute to the growing attractiveness of the Rancho Cucamonga area. In fact, market studies prepared for local developers project the West Valley's share of regional industrial growth will climb to nearly 25 percent by 1990 and to 30 percent by the year 2000. Much of this growth will be concentrated around the Ontario International Airport.

The City has initiated the preparation of this Industrial Area Plan in order to ensure the development of a high-quality industrial district that is well integrated with the residential and commercial sectors of the City to the north. In order for the Plan to be successful, it must capitalize on the area's locational advantages as well as confront the local issues and constraints which can hinder implementation of the Plan. This section provides a general overview of the issues and opportunities.

#### Land Use Compatibility

The location of industrial activities within the Industrial Area should respect and, to the extent possible, enhance the character of the land uses along the periphery of the Industrial Area. The compatibility of land uses in zone A is of particular concern because the area is 50 percent developed with a mix of residential, commercial and industrial activities. The northern boundary of Zone B fronts onto proposed residential/commercial developments which will influence the type of industrial development that can locate in these areas. Zone C is characterized by rail-related and heavy manufacturing operations that could impact residential or commercial activities. The type of uses appropriate for Zone C are restricted to similar heavier industrial activities, although some transition toward medium and light uses is necessary to the northeast where residential tracts already exist.

#### Circulation

Improvements to major streets through the Industrial Area will be necessary to accommodate traffic flows generated by planned residential communities, commercial, as well as by local industrial development. The north/south streets, are expected to receive increased usage as the Industrial Area develops. Consequently, the potential for increased automobile/railroad conflicts increase. Alternatives to avoiding these conflicts involve grade separation of the facilities. Careful design of the road system is necessary to allow the railroads the flexibility of providing spurs, and vice versa, and to minimize potential conflicts. As previous studies noted, there will also be a need for a new interchange on Devore Freeway near 7th Street.



Other potential conflicts arise when motorists use local and collector streets in the Industrial Area to get to and from residential and shopping areas to the north. This additional traffic may interfere with industrial operations.

### Transit

The growth of the industrial area into a major employment center in Southern California will certainly increase commuter inflow to the City. Because the City is connected to a potential regional transit system, the opportunities for providing access to the Industrial Area by means other than the private automobile are substantial. The distribution of land uses, particularly those that are labor intensive, should strive to take advantage of the transit possibilities which include a commuter rail system along the AT & SF trackage, High Occupancy Vehicle express lanes along the San Bernardino Freeway, and shuttle buses providing access to the Ontario International Airport.

### Geotechnic

The Industrial Area lies in close proximity to a potentially active fault system. The effects of these faults, during an earthquake, could create serious problems for the existing and future development within the Industrial Area. A complete analysis of the fault system in and around Rancho Cucamonga is discussed within the Environmental Impact Report. Building construction within the Industrial Area must consider all probable affects of a severe earthquake. Any necessary site investigation should define the conditions which may affect the intensity period or duration of seismic shaking relative to the design of the proposed structure. The seismic design features of the proposed structure shall be reviewed and approved by a Civil Engineer specializing in seismic structural design.

Soils in the Industrial Area are of the Tujunga-Delhi and Hanford-Greenfield Associations. Figure I-3 indicates the location of these soil types. Table I-1 indicated the two soil associations found in the Industrial Area and their capability classification and other characteristics. A complete analysis of the soil conditions within the Industrial Area is contained in the EIR. The Tujunga-Delhi Association is moderate to

highly susceptible to wind erosion. The resultant affects of the soil characteristics could cause erosion and dust control problems, especially during construction stages of any project.

During construction, measures should be made to reduce short-term impact to soil erosion and, following construction activity, vegetative ground cover should be provided as soon as possible over graded areas to reduce the potential for wind erosion.

**Figure I-3  
SOILS, DEVELOPMENT  
SUITABILITY**

**SOILS ASSOCIATIONS, SLOPE**

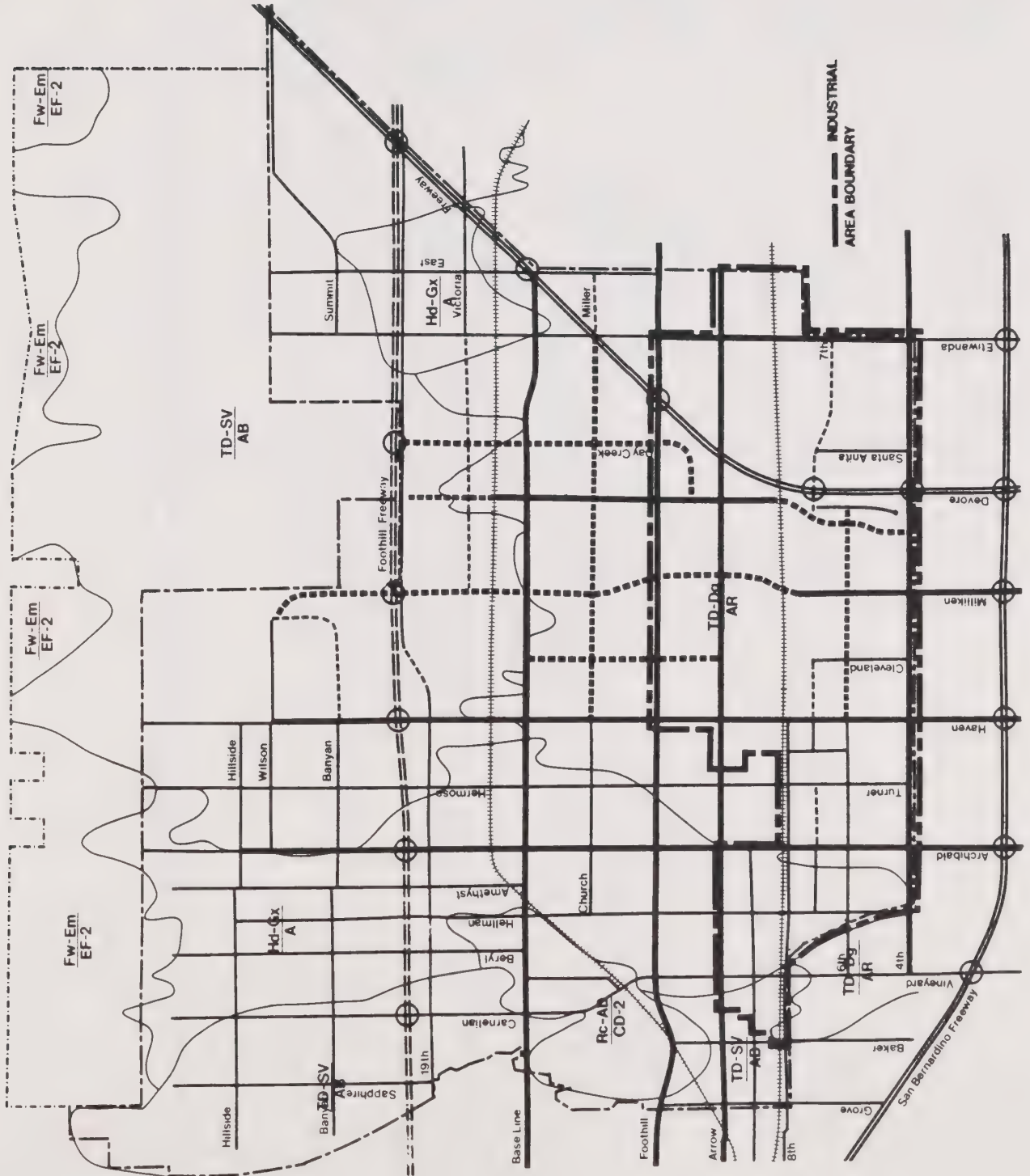
Hd-Gx  
A Hanford-Greenfield, 0-2%

TD-Dg  
AR Tujunga-Delhi, 0-2%

TD-SV  
AB Tujunga-Soboba, 0-5%

Rc-AB  
CD-2 Ramona-Arlington, 5-15%

Fw-Em  
EF-2 Friant-Escondido, 15-50%  
eroded



**CITY OF RANCHO CUCAMONGA  
INDUSTRIAL AREA PLAN**



TABLE 1-1  
SOIL ASSOCIATION CHARACTERISTICS

Soil Association	Inherent Fertility	Crop Suitability	Erosion Hazard	Runoff Potential	Septic Tank Filter Field Limitations	Soil Bearing Limitations
Hanford-Greenfield	High	Irrigated crops: grain, alfalfa grapes, truck crops, and citrus	Slight	Low	Slight	Moderate
Tujunga-Delhi	Low to moderate	Irrigated crops: citrus, truck crops, grapes, grain, and alfalfa Non-irrigated: grain and pasture	Slight by water; moderate to high by wind	Very low	Slight	Severe

Source: West Valley Planning Agency, Report and General Soil Map Southwest Portion and San Bernardino County, California, prepared by Soil Conservation Service, Redlands, California June 1968.



### Flooding

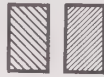
The portions of the Industrial Area lie within recognized hazard area as determined by the Flood Insurance Administration Flood Hazard Area. Flood hazard areas in the City are shown in Figure I-4. The Day and Etiwanda Creek systems contribute significantly to the storm water runoff problem. Improvements to the Day Creek system are now being designed at the local and County level. It is anticipated that improvements to this creek system will occur late in 1986. Until such improvements are completed and the flood hazard is reduced, areas subject to potential flood hazards must comply with design considerations discussed within the Flood Management area of Part II Industrial Area Development Framework.

### Noise

Development in the Industrial Area, as designated in the Industrial Plan, will cause an increased noise level in the area. This increase will be due to the increased vehicle traffic to the area and added industrial activity. As shown on Figure I-5, all major streets are projects to generate an Ldn of 60 dB or more outside of the right-of-way boundary. Noise levels up to Ldn of 70 dB are normally accepted in areas of commercial buildings, manufacturing, and industrial uses, assuming structure is of normal construction. It is not anticipated that major impacts will be occurring through the introduction of added development in the Industrial Area; however, as development does occur adjacent to residential or non-industrial uses, particular attention should be paid to any potential noise problems. These considerations are addressed within the Performance Standards as set forth in Part III of Development Standards.

## 100 YEAR FLOOD PLAIN

<1' DEPTH

CITY OF RANCHO CUCAMONGA  
INDUSTRIAL SPECIFIC PLAN

**FIGURE I-5  
FUTURE NOISE CONTOURS\*  
INDUSTRIAL SPECIFIC PLAN**

**FUTURE NOISE EXPOSURE  
CONTOURS -- Ldn METRIC  
-60- NOISE CONTOUR**

**\*PROPOSED FOOTHILL FREEWAY CONTOUR  
ASSUME 10dB ATTENUATION**



**CITY OF RANCHO CUCAMONGA  
INDUSTRIAL SPECIFIC PLAN**







## **PART II**

# **INDUSTRIAL AREA DEVELOPMENT FRAMEWORK**

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## PART II INDUSTRIAL AREA DEVELOPMENT FRAMEWORK

### I. GOALS AND OBJECTIVES

Goals and objectives are statements that define the community's aspirations and intentions. This set of goals and objectives has evolved from sessions with the industrial communities and with the City's Planning Staff. They represent the current conception of what is in the best interest of the Industrial Area and the City. Collectively, they constitute a framework by which to plan and design a successful Industrial Area.

#### URBAN DESIGN

##### GOAL

A. Provide a pleasant, attractive and safe working environment.

##### OBJECTIVE

- A.1 The City shall encourage distinctive industrial development by establishing design characteristics for varying industrial activities.
- A.2 Development standards shall provide a guide to ensure minimum design criteria throughout the plan area.
- A.3 Development standards shall provide the means for practical and pleasing transitions between different levels of industrial activity.
- A.4 Major transportation corridors shall have special design and use standards to provide differing, yet compatible streetscape characteristics throughout the plan area.
- A.5 The City shall encourage high quality architecture to ensure each project is compatible with existing developments and is a positive enhancement to the immediate area.
- A.6 Land uses shall be organized to avoid creating nuisances among adjacent land uses.
- A.7 The physical organization of land uses within the Industrial Area shall foster the provision of alternative modes of transportation.

## LAND USE

### GOAL

B. Promote employment opportunities as an integral part of a balanced community.

### OBJECTIVES

- B.1 Encourage a mix of industrial land uses to develop a sound and diversified economic base for the City of Rancho Cucamonga and San Bernardino County.
- B.2 New development in the Industrial Area shall provide a range of employment opportunities with respect to income and skills for present as well as future residents of the City of Rancho Cucamonga and nearby communities.
- B.3 The timing, location, and intensity of development in the Industrial Area should be closely integrated with the overall development of the City as set forth in the General Plan.
- B.4 Establish a specific, well-defined pattern of industrial activities, while providing flexibility to respond to changing future conditions.
- B.5 The development of the Industrial Area should accommodate the personal needs of workers and business visitors as well as the service needs of local businesses.
- B.6 Industrial development shall be encouraged by eliminating uncertainty and time and money consuming delays in the governmental processes.



## PUBLIC FACILITIES/SERVICES

### GOAL

C. Provide a method for logical and effective development of public facilities and services.

### OBJECTIVES

- C.1 The provision of improvements within the Industrial Area should seek comprehensive solutions to service and facility needs, rather than approaching these needs on an ad hoc, case-by-case basis, thereby enabling the costs of these improvements to be spread over as broad a tax base as possible.
- C.2 The phasing of development shall be coordinated with the development of public improvements.
- C.3 When public transit and other related improvements occur within the Industrial Area, the opportunities for joint development with private development shall be considered to enable the public to recapture some reasonable portion of the development benefits thereby created.
- C.4 Promote an efficient pattern of land use and encourage the use of energy efficient modes of transportation, the use of renewable energy resources such as solar energy, and the conservation of energy.
- C.5 Provide a safe and healthy environment for workers including adequate levels of police and fire protection.
- C.6 Promote the use of existing railroad lines and enhance opportunities for future rail services.
- C.7 Require water conserving, irrigation techniques and landscape/hardscape designs for new development.
- C.8 Develop a toxic and hazardous materials storage and waste program in cooperation with other responsible public agencies.

## II. URBAN DESIGN CONCEPT

This section deals with the physical appearance of the Industrial Area. While more specific urban design guidelines for the Area are incorporated in Part III, this section describes the overall, Area-wide design concept addressing physical form and appearance, open space network, special features and gateways. Figure II-8 summarizes the Area-wide urban design policies.

### Physical Form and Appearance

The Industrial Area has been divided into 16 subareas, each of which is designated with one of the four Land Use Categories. The development standards specified for each of these subareas will distinguish them from each other by the types of uses permitted or conditionally permitted, parcel sizes, setback requirements, landscaping, performance requirements, vehicular and rail access, and parking provisions. The distinction of each subarea would serve as a mechanism to create and enhance the identity of each subarea.

The Industrial Area forms a part of the community fabric by integration into an arterial roadway network which serves the entire City. The area's identity is further enhanced by the development of specially designed landscape themes at arterial roadway entry points into the plan area.

Each land use category is further distinguished by general architectural themes based upon the planned land use intensity. While the themes may vary, high quality of design is expected in all land use categories of the Industrial Specific Plan. There are four land use categories in the Industrial Specific Plan:

INDUSTRIAL PARK CATEGORY has been designated for five (5) subareas. Primary uses in this category include custom light manufacturing, light wholesale storage and distribution, administrative and office, and professional services. This area is reserved for firms seeking attractive and pleasant working environment and a location which has prestige value. High quality architecture is required and site planning must emphasize a pedestrian oriented, campus-like setting with the greatest amount of landscaping. The development of prefab, all metal for sheathing of building is considered inappropriate for this category. The Industrial Park category is typically located adjacent to special boulevards (major thoroughfares) to enhance major gateways into the community and create a high quality image. In addition, the Industrial Park category is used as a transition from industrial to residential uses.

GENERAL INDUSTRIAL CATEGORY has been established for seven (7) subareas. This category is intended to accommodate a wide range of light to medium manufacturing and wholesale, storage, distribution uses. Typically, administrative and office and professional services are not allowed to reduce potential land use conflicts. Design and technical standards are less demanding than the Industrial Park category, but safeguards are provided to ensure a pleasant, well functioning environment. The development of prefab, all metal for sheathing of buildings is considered inappropriate for this category.

MINIMUM IMPACT/HEAVY INDUSTRIAL is designated for Subarea 9. This category provides for heavy manufacturing and wholesale, storage, distribution uses which do not have a significant aesthetic impact on the surrounding area. Not permitted in this area are massive outside structures such as cranes, or unscreened storage of raw or finished products. Landscaping requirements in Subarea 9 along Arrow Highway have been increased to create a smoother transition to adjoining industrial park and general industrial property.

HEAVY INDUSTRIAL CATEGORY has been designated in Subarea 15. Permitted uses include Medium, Minimum Impact, and Heavy Industrial Manufacturing and each of the wholesale, storage, distribution uses. Design and technical standards in this category allow massive outdoor structures and open air storage in an unscreened manner. A high degree of rail usage may also be typical.

The Plan also emphasizes the following areas:

THE HAVEN AVENUE OVERLAY DISTRICT is located on both sides of Haven from Foothill Boulevard south to 4th Street. As the City develops, Haven Avenue will become a major north-south travel route and has a potential for high end office development with the unique combination of direct access to the Ontario Airport and Interstate 10 freeway. The standards in the Overlay District are designed to enhance Rancho Cucamonga's image by providing an intensive, high quality and prestigious gateway into the community. A progressive, sophisticated, and urban style of development is required and special site planning and landscaping requirements are included to enhance the pedestrian environment and create a campus-like atmosphere.

INTERSTATE 15 (Devore Freeway) runs north-south through the industrial area and borders seven (7) subareas in each land use category except General Industrial/Rail Served. Since this area is directly south of the future regional mall and related commercial activities at I-15 and Foothill Boulevard, and views along the freeway have a major impact on the image and identity of Rancho Cucamonga. Highest consideration should be given to design aspects that affect a positive image of the community as viewed from the freeway. Special consideration are contained in the Development Standards of each subarea which abutts the I-15 freeway. (An I-15 Overlay District could provide special development criteria to eliminate unsightly views.)



SPECIAL BOULEVARDS. The General Plan states that travel routes are predominant elements of the community's image and encourages the distinctiveness of individual districts and roadway corridors. In addition, the General Plan states that a consistent design theme is necessary to reinforce the image of perception of a route. Within the Industrial Area Specific Plan, nine (9) special boulevards are included. The following is a brief description of the primary function and identity for the most significant special boulevards including Foothill, Haven, Milliken, Archibald, and 4th Street.

Foothill Boulevard serves as the major east-west transportation corridor in the City. It also links the emerging area east of Haven Avenue including the planned communities and regional shopping mall with the existing commercial area to the east. Within the Industrial Area Specific Plan, new development along Foothill must provide a transition to commercial and office facilities within Terra Vista, Victoria, and Etiwanda. Given the high visibility, office and commercial service type uses will predominate. Any light manufacturing or warehouse buildings will have to be designed to the highest quality and be compatible with commercial and residential environment on the north side of Foothill.

Milliken Avenue will function as a primary north-south arterial through the Industrial Area and connect the planned communities of Victoria and Terra Vista south to Interstate 10. Unlike other special boulevards with adjoining subareas designated Industrial Park, Milliken Avenue cuts through six (6) different subareas and four (4) land use categories, including Minimum Impact/Heavy Industrial. Traffic counts will be similar to Haven Avenue, but a greater mix of truck traffic is expected along with the potential of different commuter traffic hours due to varying employee shifts in the area. Given this setting, the functional and visual identity of Milliken Avenue will be influenced by a wide range of land uses. Building types may range from offices in multi-tenant industrial to large scale distribution and manufacturing. However, to assure aesthetic quality and design uniformity, all architectural elevations along Milliken should be enhanced with variation of building form and articulation of services. In addition, outdoor storage, loading areas and truck parking should not be visible from the street.

Archibald Avenue serves primarily a support service function to the industrial area. A variety of industrial, commercial, and residential uses exist along major portions of the street. The major opportunity along Archibald Avenue is on the west side of the street south of 6th street. New development in this area must be compatible with adjacent residential uses and provide an appropriate transition in terms of architecture, site planning, and landscaping. One story buildings in scale with the single family homes fronting on 6th Street, and pedestrian connections to residential should be provided.



Fourth Street is the boundary between Rancho Cucamonga and the City of Ontario, east of Cucamonga Creek (Hellman Avenue). As such, a series of gateways are planned at major intersections along 4th Street (Archibald, Haven, Milliken, I-15, and Etiwanda). A consistent program of special landscape features and street monumentation signs with the City emblem is to be implemented.

## LANDSCAPE DESIGN

Landscaping serves as a major design component of the urban design image of the Industrial Area Specific Plan. Through the variation of landscaping design and standards, distinction and identity of individual subareas and roadways can be achieved. To this end, the average depth of landscaping along street frontages and the percentage of net land area devoted to landscaping is varied. To distinguish special boulevards, landscaped median islands, meandering sidewalks, and street name monumentation signs are planned.

Throughout the industrial area, these primary landscape design concepts should be followed:

Streetscape: Street Trees of similar species establish a consistent design pattern within the parkway of each street; evergreen trees shall be used as a backdrop for deciduous, particularly where needed to screen unsightly views and enhance building elevations; shrub planting and berming provides the desired screening of the parking areas.

On Site Landscaping: Provides canopy trees for shading outdoor pedestrian areas and parking lots; break up lengthy building elevations along the side and rear properties with tree planting; use evergreen pines for wind protection and to screen undesirable views.

Special Accent Treatment: Includes the addition of special landscape design features such as color accents, specimen tree planting, decorative rockscape, and pavement details to provide visual enhancements to roadway intersections, driveway approaches, pedestrian walkways, and building entries.

Pedestrian Amenities: Provide for the inclusion of a pedestrian circulation system and accompanying plaza and patios as an integral part of a unified site design. Such a plan provides for the safe and orderly transition of vehicular and pedestrian traffic by means of clearly identifiable and attractive walkways.

Tree Size/Staking: Specimen size trees (24 inch box or larger) are encouraged to promote the early establishment of mature plantings. Severe wind conditions require all trees to be staked or guidewires used per City standards.

Drought Tolerance: Because of the community's arid climate, the use of drought tolerant plant material is strongly encouraged. Turf should be limited to areas of major design emphasis with the use of rockscape/hardscape and groundcover to limit water consumption.

Irrigation: Water conserving irrigation techniques are required. Deep root watering systems for trees, particularly in turf areas are encouraged.

### Open Space Network

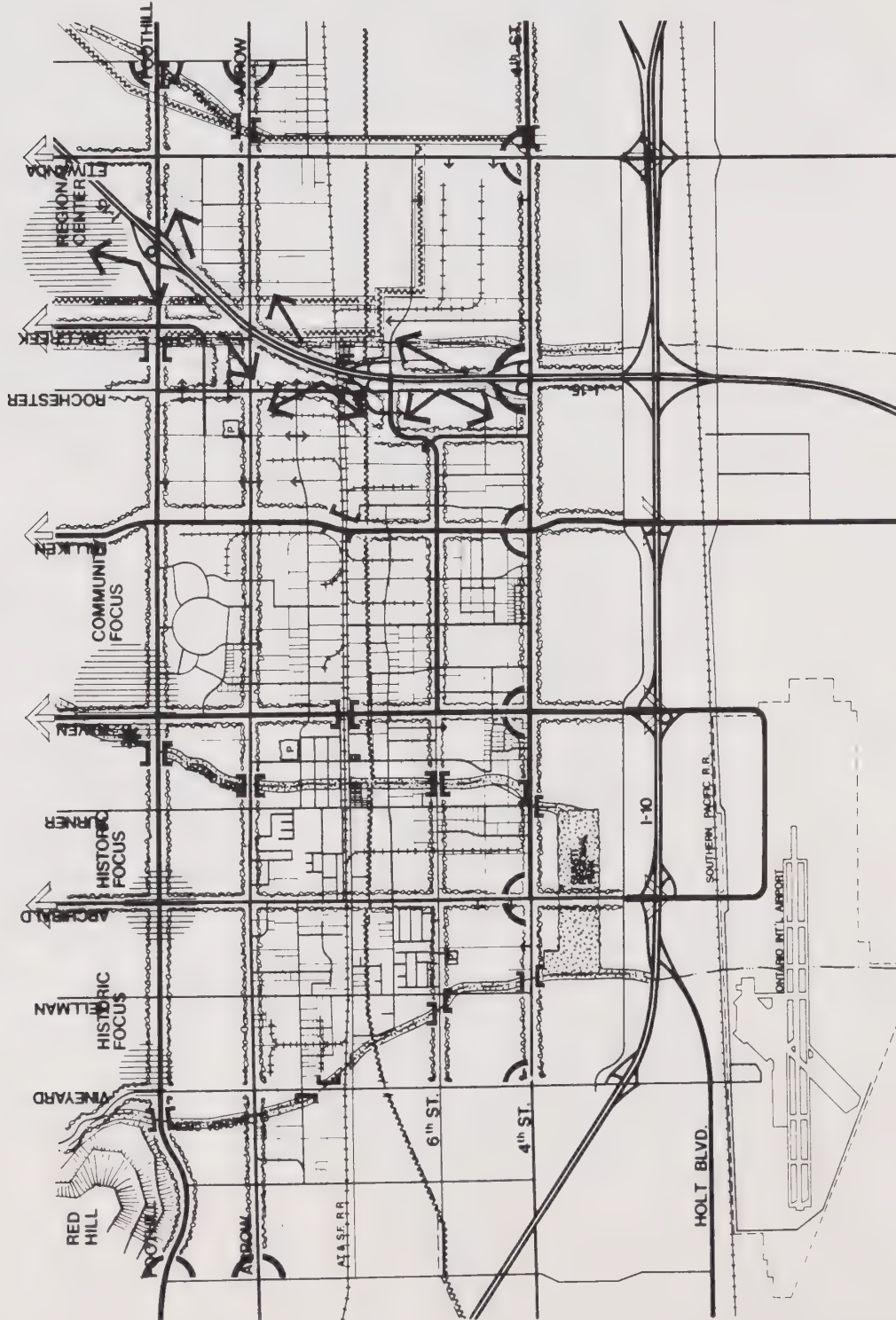
Elements that constitute open space include parks, outdoor recreation areas, creeks and channels, transmission corridors, and stormwater retention areas. In addition, to the extent that they provide amenities, development setback areas, and street landscaping with associated pedestrian and bicycle paths, also compliment open space. As indicated in Figure II-8, easements for creeks and channels and powerline corridors traverse the Industrial Area, linking them with the City's region's, and National Forest open space areas. Directly south of 4th Street between Cucamonga Creek and Turner Avenue is Guasti Regional Park, comprised of approximately 200 acres, to which both Cucamonga and Deer Creek Channels connect. While all of these linear open space components run north/south, the Plan proposes east/west connections of open space, incorporating the Metropolitan Water District's easement south of the AT & SF tracks, setback requirements along major arterials, as well as related buffer landscaping provisions along certain land use edges, as elaborated in Part III.

The private open space areas, developed internally within the planned industrial projects, should connect with the Area-wide open space network wherever possible, to avoid fragmentation of open space to unify the Area-wide appearance.

Two proposed 10 acre neighborhood level parks are located within the Industrial Area in the approximate location of Arrow and Rochester and southwest of Arrow and Haven. A five acre mini-park is proposed along 6th Street between Hellman and Archibald.



**Figure II-1**  
**URBAN DESIGN**  
**CONCEPT**



- PARK
- SPECIAL STREETSCAPE/  
LANDSCAPING
- CHANNELS AND CREEKS
- POWER LINE/  
AQUEDUCT EASEMENTS
- BRIDGES/VIADUCTS
- FOCAL POINTS
- HISTORIC FOCUS
- SIGNIFICANT LANDFORM
- GATEWAYS
- VIEW PENETRATIONS/  
VISTAS
- VIEWS

**CITY OF**  
**RANCHO CUCAMONGA**  
**INDUSTRIAL AREA PLAN**





### Special Features

Special features refer to areas of visual, historic, and activity focal points within and near the Industrial Area. These are also shown in Figure II-8. These features should be respected and development and redevelopment around them should compliment and enhance their scale and character. The coordinated improvements of the special features and the corridors along which they are located, should further help unify the overall appearance of the Industrial Area and City.

The old Cucamonga historic commercial center at the intersection of Foothill and Archibald and the location of Thomas Winery at the intersection of Foothill and Vineyard, are also of historic interest.

Also along Foothill Boulevard near its interchange at I-15 is proposed a major regional shopping center (over one million square feet of floor space containing six department stores on approximately 100 acres of land). This would be a significant commercial activity center of regional impact.

### Gateways

Certain points of access to the City will, by use, become major entrances to the Industrial Area. A significant portion of the visitors' and users' impressions of the Industrial Area are influenced by conditions at these locations. Hence, it is imperative that special considerations be given to the development and design of these gateway areas.

As shown in Figure II-1, gateway areas are identified along the major roadways that form the Industrial Area (and City) boundaries--4th Street, Foothill Boulevard, Etiwanda Avenue, and Arrow Route. The gateway of primary importance is located at the intersection of Haven and 4th. Other major gateways to the Industrial Area include Archibald at 4th, Milliken at 4th, Interstate 15 interchange at 4th, the proposed Interstate 15 interchange at 6th, and Interstate 15 interchange at Foothill.

### Utilities

To enhance the beauty and character of the community, all new and existing utilities lines including electric distribution line of 34.5KV and less shall be underground as a condition of development.

### III. LAND USE PLAN

Use of the industrial lands is organized to provide a broad range of industrial development opportunities ranging from light/industrial type of accommodations to sites for heavy, rail-served industrial plants. Based on consideration of road and rail access, the relationship to surrounding non-industrial uses, City-wide plan policies and the parcelization pattern, land within the Industrial Area is assigned to major land use categories:

- Industrial Park
- General Industrial
- Minimum Impact/Heavy Industrial
- Heavy Industrial/Rail-Served

The definitions of the four industrial land use categories follows in Table II-1, and the distribution of these uses are shown in Figure II-1 Land Use Plan. A more detailed definition of permitted and conditionally permitted uses is listed in Part III.

TABLE II-1  
INDUSTRIAL LAND USE CATEGORIES

INDUSTRIAL PARK GROUP. Land is reserved for industrial firms seeking an attractive and pleasant working environment and a location which has prestige value. Approximately 1238 acres have been allocated to this use, or 26% of the total Industrial Park group designation permit light industrial uses, office and administration facilities, research and development laboratories, limited types of warehousing, as well as support businesses and commercial service uses. The Industrial Park areas are characterized by a high employment density. The development and design standards are developed to protect lands in this use group from development which is inappropriate due to either function, appearance or environmental affects.

GENERAL INDUSTRIAL GROUP. This group provides for the widest possible range of light and medium industrial type activity, including manufacturing, assembling, fabrication, wholesaling, heavy commercial, and office uses; totaling approximately 1570 acres, or 33% of the Industrial Area land. This land use category is appropriate as a buffer between non-industrial uses and heavy industrial uses. In these areas, the applicable development and design standards are less demanding than those of the industrial park areas, but safeguards are provided to ensure a pleasant well-functioning environment. The required land area per parcel is also less demanding than other industrial areas so as to accommodate smaller firms.

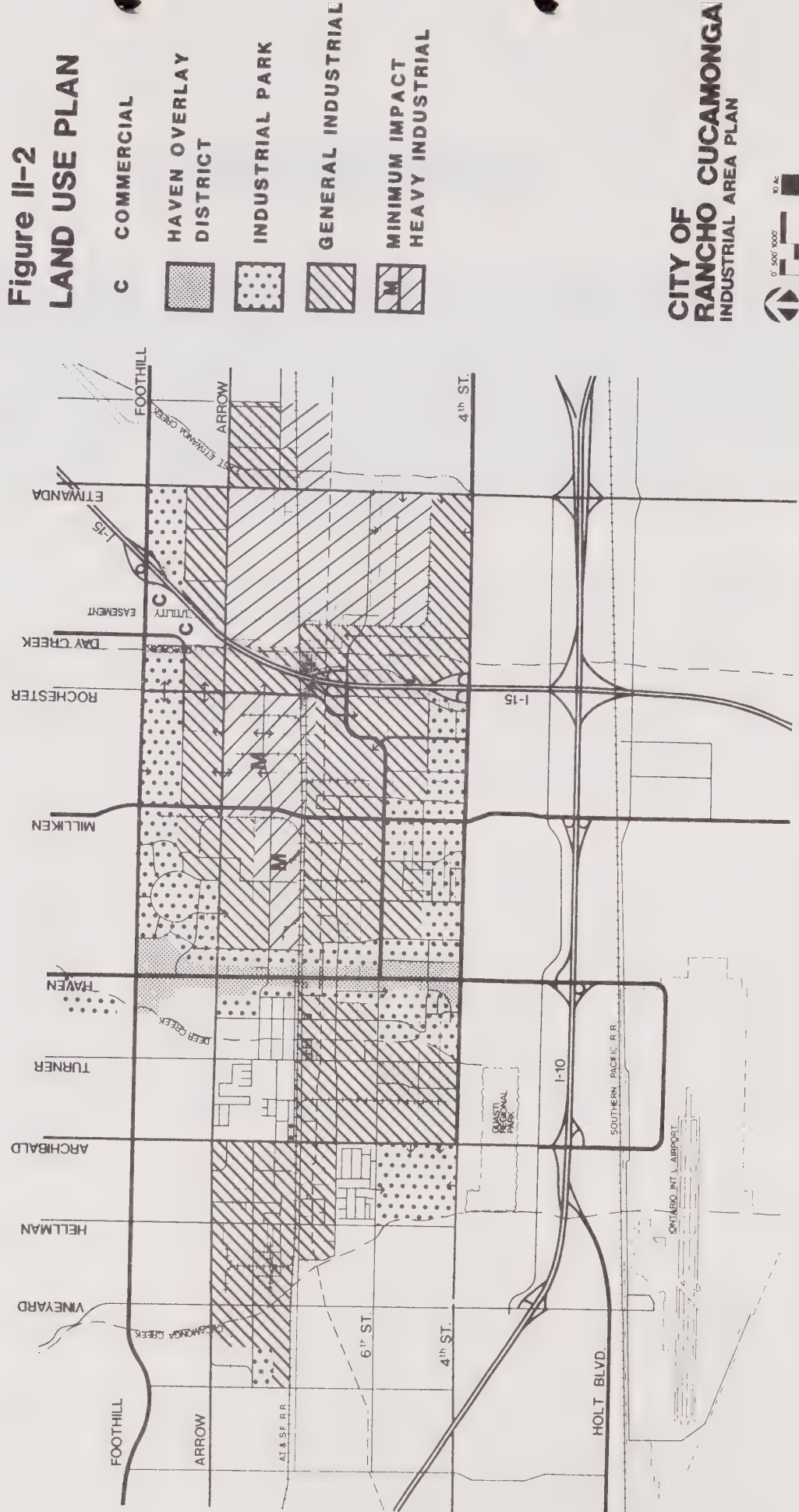
MINIMUM IMPACT HEAVY INDUSTRIAL GROUP. Approximately 537 acres or 11% of land is reserved for minimum impact heavy industrial uses. The development standards are designed to provide for heavy industrial uses which will not significantly impact on the surrounding area. Minimum Impact Heavy Industrial activities include manufacturing, compounding, processing, fabrication, warehousing, storage and freight handling. Uses typical of this group include but are not limited to forge shops, steel milling facilities, plastic plants, steel fabrication, metal fabricating facilities, woodworking facilities, heavy machine shops and chemical storage and distribution. Not permitted within the area are uses which require massive structures outside of buildings or unscreened open air storage of larger quantities of raw, semi-refined, or finished products.



TABLE II-1 (Continued)  
INDUSTRIAL LAND USE CATEGORIES

HEAVY INDUSTRIAL GROUP. Approximately 675 acres or 15% of land east of Devore Freeway is reserved for Heavy Industrial Uses. The development standards are designed to provide for all heavy industrial uses. Heavy Industrial activities include manufacturing, compounding, processing, fabrication, warehousing, storage, and freight handling. Use characteristics of this group would include large structures to facilitate processing and open air storage of large amount of raw or semi-refined products. Uses within this area typically include but are not limited to; vehicular assembly plants, power plants, concrete product manufactures and batch plants.

**Figure II-2  
LAND USE PLAN**



**CITY OF  
RANCHO CUCAMONGA  
INDUSTRIAL AREA PLAN**



#### IV. CIRCULATION AND ACCESS

This section addresses vehicular access and circulation, transit, rail, and pedestrian and bicycle routes. Parking provisions are discussed in Part III, under Parking Requirements. The plan for the circulation system is shown in Figure II-2 and described within this section.,

##### Traffic Analysis

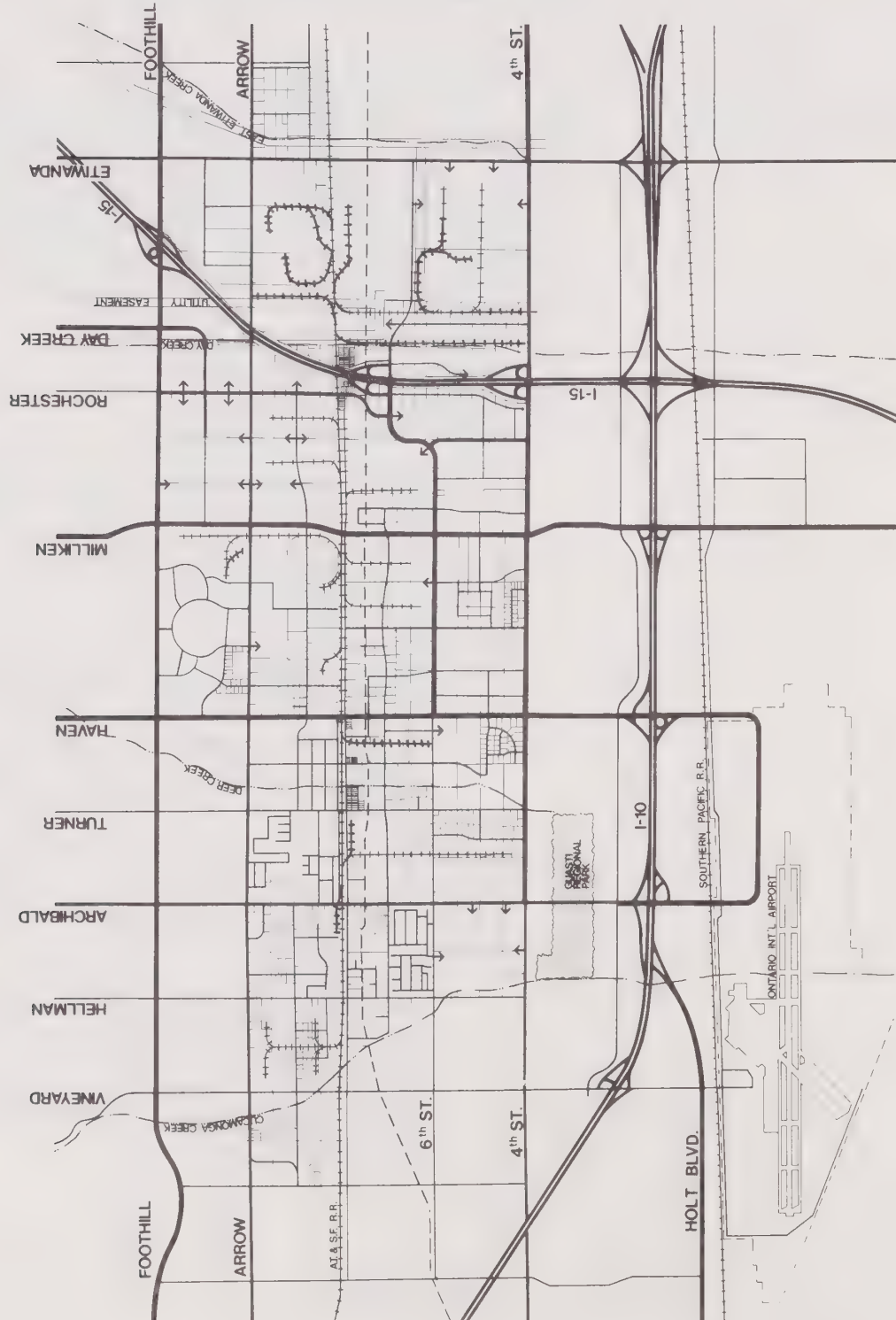
##### Traffic Projections

DKS Associates prepared an initial Rancho Cucamonga City-wide and Industrial Area Traffic Study in 1980. An additional analysis was conducted to better reflect local access within the Industrial Area and to estimate peak period traffic volumes for detailed analysis. A description of the process utilized to develop the traffic model can be found in Appendix A.

Significant observations made from the traffic model relative to the projected traffic volumes in the Industrial Area are:

- o Highest ADT volumes are along Haven Avenue and Foothill Boulevard. Haven Avenue volumes are on the order of 50-60,000 vehicles per day, assuming sufficient capacity is provided.
- o Total east/west traffic volumes accessing the Interstate 15 within the Industrial Area (4th Street and Foothill Boulevard) are projected to be high. This is only partially offset by the assumed presence of additional interchange at 7th Street.
- o The projected east/west traffic count on Foothill Boulevard is projected to 45,000 ADT at Haven Avenue which is at capacity.
- o ADT volumes on 6th and 7th Streets are increased substantially due to the assumption of a 6th Street/Interstate 15 interchange in the General Plan Update Program. However, volumes are not unreasonably high relative to capacities available.

**Figure II-3**  
**ACCESS/CIRCULATION**  
**PLAN**



- 120' ROW
- - - 100' ROW
- · · 88' OR LESS ROW
- + + + RAIL
- - - AQUEDUCT
- ~ ~ ~ CREEK
- ACCESS POINTS

**CITY OF**  
**RANCHO CUCAMONGA**  
**INDUSTRIAL AREA PLAN**





- o About 20,000 vehicles per day in the Haven-Milliken corridor are associated with the Ontario Center commercial development assumptions.

Figure II-3 shows the results of the volume projections.

#### Level of Service

Adequate capacity on existing and planned roadways is needed to ensure efficient traffic flows and an acceptable level of service on streets in the industrial area. Traffic volumes can be related to descriptions of traffic performance to determine the level of service available at an intersection. Table II-2 describes these performance levels.

#### Mid-block Roadway Capacity

Mid-block roadway capacities were determined for the major street segments in the Industrial Area. A description of the procedures used to determine mid-block capacities and volume-to-capacity ratios for the street segments are included within Appendix A. The streets within the Industrial Area projected to potentially experience substantial overloading are Foothill Boulevard and Haven Avenue. Heaviest volumes on Foothill Boulevard are projected toward the east near the I-15, indicating a significant freeway access problem. However, alternative Industrial Area freeway access routes--6th and 4th Streets-- are projected to operate satisfactorily throughout the Industrial Area. In the north-south direction, Haven Avenue is substantially overloaded along its length. Relief for Haven Avenue will be discussed further in the next section.

#### Vehicular Access and City) Circulation

The roadway network in the Industrial Area (and is laid out in a basic one mile gridiron pattern. In the already developed portion of the Industrial Area (west of Haven) this gridiron system is further broken down to 1/2 mile intervals. The proposed circulation system, as shown in Figure II-2, follows this gridiron pattern to the smallest unit of roughly 1/8 mile (which would form a 10 acre square grid), where smaller properties exist. Generally, a quarter-mile grid pattern has been adhered to.

**Figure II-4**  
**PROJECTED ADT VOLUMES**

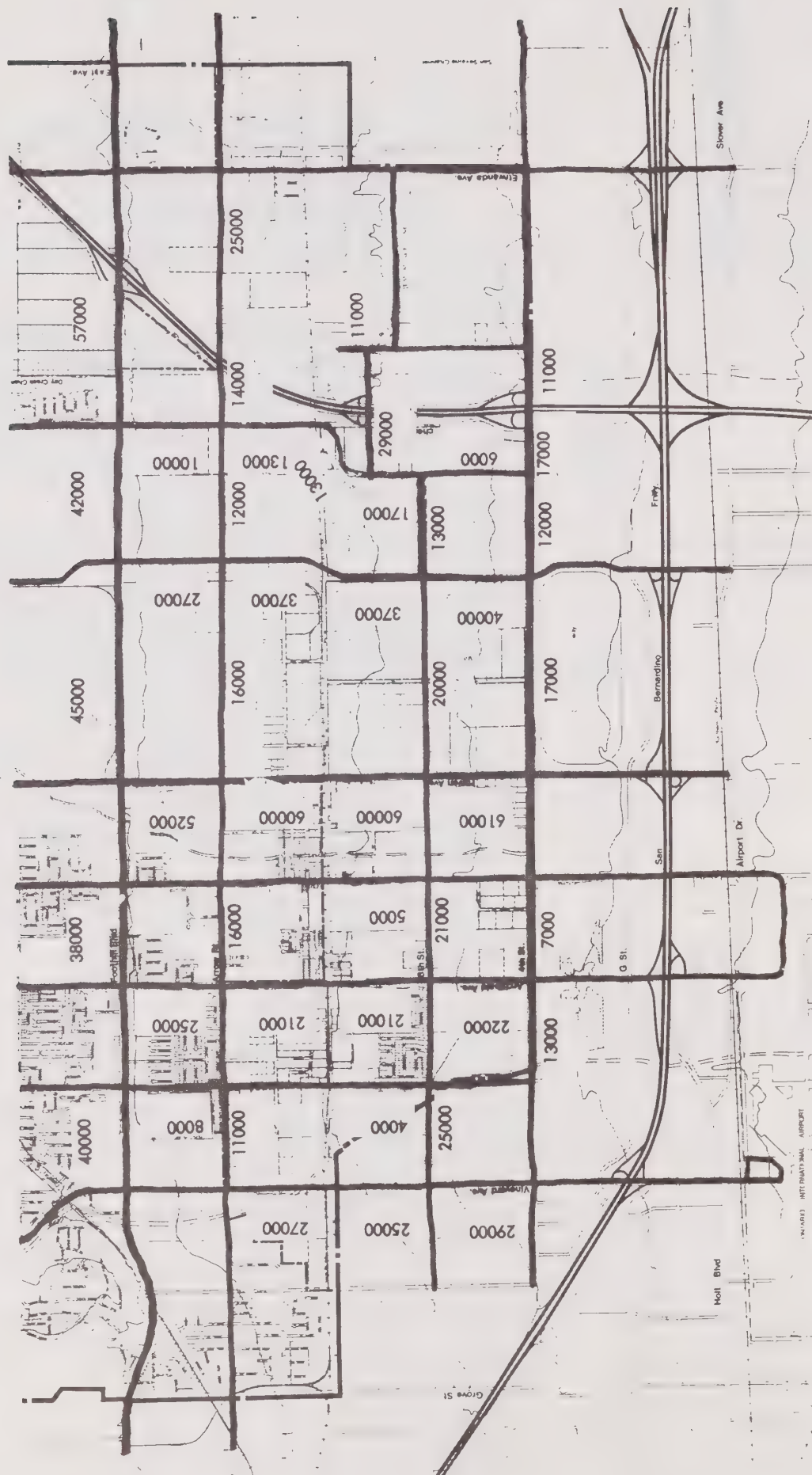


TABLE II-2: LEVELS OF HIGHWAY SERVICE INTERPRETATION

<u>Level of Service</u>	<u>Description</u>	<u>Delay Range (Sec. per Vehicle)</u>	<u>Volume to Capacity Ratio</u>
A	Excellent operation. All approaches to signalized intersections appear quite open, turning movements are easily made, and nearly all drivers find freedom of operation. No vehicles wait longer than one red traffic signal indication.	0-16	0-60 %
B	Very good operation. Many drivers begin to feel somewhat restricted within platoons of vehicles. This represents stable flow. An approach to a signalized intersection may occasionally be fully utilized, and a substantial number of cycles are approaching full use.	16-22	60-70%
C	Good operation. Occasionally drivers may have to wait through more than one red signal indication, and back-ups may develop behind turning vehicles. This level is typically associated with urban design practices.	22-28	70-80%
D	Fair operation. Cars are required to wait through more than one traffic cycle during short peaks. There are no longstanding traffic queues.	28-35	80-90%
E	Poor operation. Some longstanding vehicular queues develop on critical approaches to intersections. Delays may be up to several signal cycles.	35-40	99-100%
F	Forced flow. Represents jammed conditions. Backups from locations downstream or on the cross street may restrict or prevent movement of vehicles out of the intersection approach lanes; therefore, volumes are not predictable. Potential for stop and go type traffic flow.	40 or	Over 100%

Source: Based on National Academy of Sciences, Highway Capacity Manual, 1965, and the Draft Final Report for update of the manual.



## Roadway Cross-Sections

Cross-sections for roadway requirements were determined based on mid-block roadway capacities. Major arterials with 120 foot right-of-way in the Industrial Area include:

- o North/south roadways of Haven Avenue, Day Creek Boulevard, and Milliken Avenue.
- o East/west roadways of Foothill Boulevard, 6th Street between Haven Avenue and Interstate 15 and 4th Street.

Major arterials with 100 foot right-of-way include:

- o North/south roadways of Archibald Avenue, Rochester Avenue, Buffalo Avenue and Etiwanda Avenue.
- o East/west roadway of Arrow Route.

Secondary arterials with 88 foot right-of-way include:

- o Vineyard Avenue, Hellman Avenue, Cleveland Avenue, and 6th Street west of Haven Avenue.

These streets will form the basic 1/2 mile grid system. The other streets, which will further divide the circulation network into 1/4 and 1/8 mile grids, will have rights-of-way ranging from 66 feet to 88 feet. Figure II-4 presents mid-block cross-sections of each street classification type with lane requirements.

## Roadway Network Modifications

The following roadway modifications are recommended within the Industrial area.

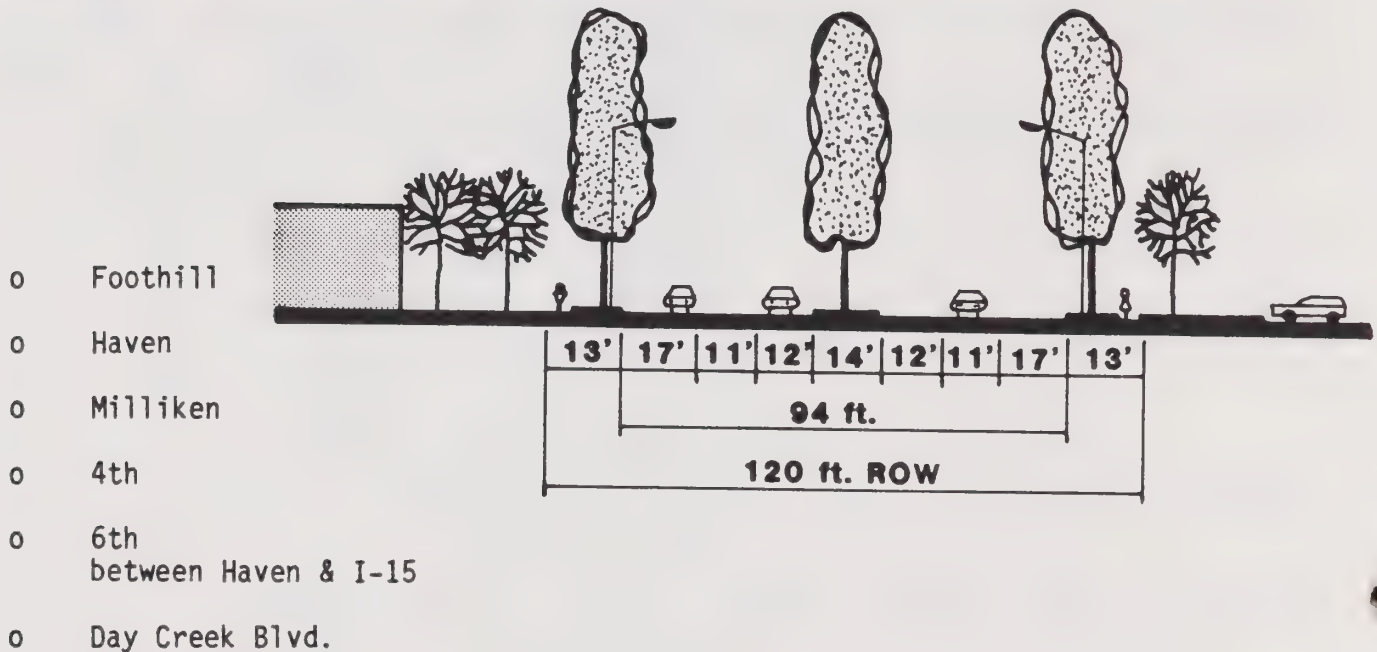
- o New Rochester. As discussed in the City-wide and Industrial Area Traffic Study and incorporated in the Industrial Area Plan, Rochester Avenue from 4th Street to 8th Street should be terminated north of 4th Street and connected to Buffalo Avenue. This will provide adequate distance for turning lanes between the Rochester and I-15 ramp intersections along 4th and (in the future) 6th Streets.



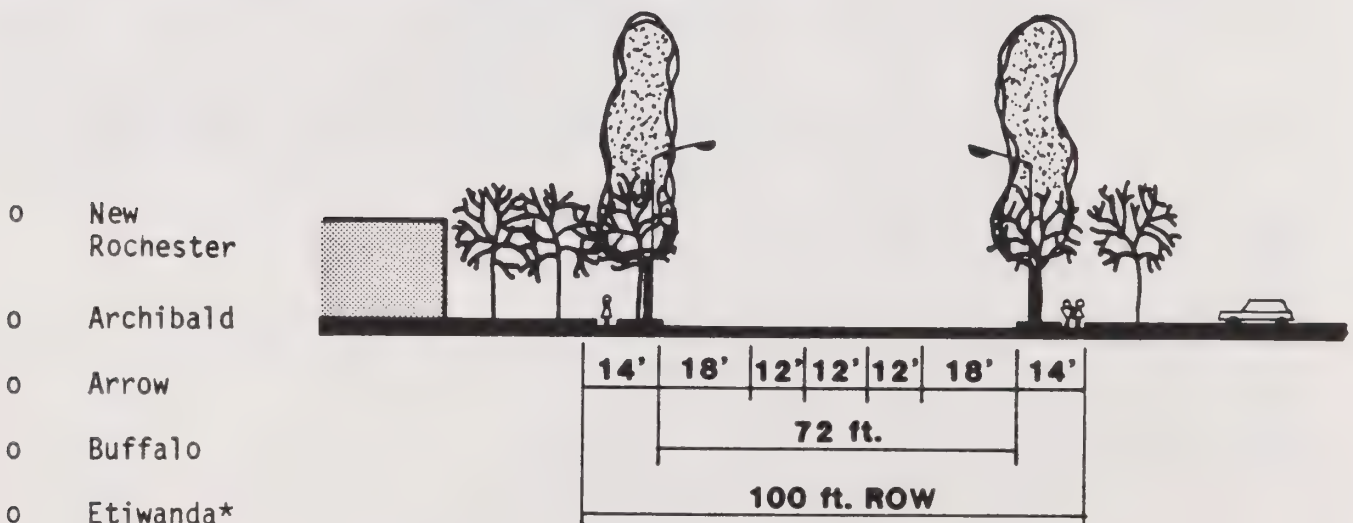
- o Day Creek Boulevard. This alignment is part of the Victoria Community Plan and will extend southerly across Foothill Boulevard and connect into Rochester Avenue north of Arrow Route.
- o Cleveland Avenue. The need for additional north/south roadway capacity through the Industrial Area is dependent upon future development level in the Ontario Center and in the area bounded by Haven Avenue, Base Line Road, Milliken Avenue and Arrow Route. At projected traffic levels, there appears to be a need for an additional route from Foothill Boulevard to I-10 to reduce traffic on Haven and Milliken Avenues and reduce overloads on Foothill/Haven and Foothill/Milliken. To accomplish this, Cleveland Avenue should extend south of 4th Street and connect across the San Bernardino Freeway to a new south frontage road. Cleveland Avenue would upgrade capacity sufficiently through Rancho Cucamonga's Industrial Area, but much of the traffic would still have to access the freeway at Haven or Milliken Avenue. Hence, this improvement, unless coupled with I-10 freeway access improvements in Ontario, would move the traffic bottleneck farther south. At a minimum, it would appear necessary to provide a grade-separated crossing of I-10 at Cleveland Avenue and to tie Cleveland Avenue into a one-way frontage road system paralleling the freeway. Existing "G" Street in Ontario could be used as a north (westbound) frontage road. These improvements should be considered in any redevelopment of the Ontario Center and vicinity by the City of Ontario.

# FIGURE II-5 STREET CLASSIFICATION

## MAJOR ARTERIAL DIVIDED (120 ft. ROW)



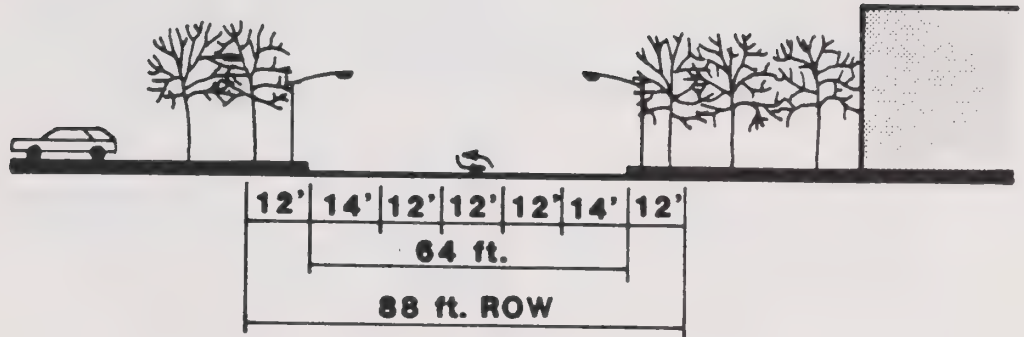
## MAJOR ARTERIAL (100 ft. ROW)



\*Median island between Arrow and Foothill.

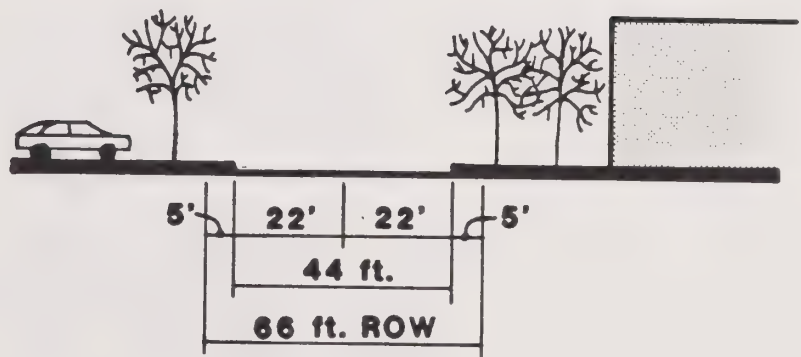
## SECONDARY (88 ft. ROW)

- o Vineyard
- o Hellman
- o Cleveland
- o 6th West of Haven
- o Turner
- o Jersey
- o 8th West of Vineyard



## ALL LOCAL INDUSTRIAL (66 ft. ROW)

- o 8th (east of Vineyard)
- o 9th
- o Center
- o Santa Anita
- o Old Rochester
- o Baker
- o Other Local Streets



## Energy Conservation

The largest component of energy consumption in Southern California and, for that matter, in this country, is transportation. The urban sprawl linked by the extensive freeway system has resulted in long commutes to work and home. Escalating energy costs and a nationwide push to conserve energy has made it essential that local communities encourage the development of housing opportunities near job opportunities, and vice versa. Nearly 60 percent of the land in the City of Rancho Cucamonga is available for development--including both new jobs and new homes. An extraordinary opportunity exists to develop a fully integrated, transit-oriented community with a diversified employment base and adequate housing.

The City's General Plan provides numerous policies toward the development of greater energy conservation. Implementation of those policies associated with the Industrial Area include:

- o Locating labor intensive land use along transit corridors.
- o Providing for the development of alternative transportation systems to the use of single passenger automobiles.
- o Providing housing programs which help to encourage housing placement for locally employed.

In the future, the City will develop an energy conservation program which, for the industrial area, should address such matters as the potential for cogeneration, passive and active construction of solar conservation methods, and resource conservation through alternative recovery methods.

- o Milliken Avenue. The at-grade portion of the proposed Milliken Avenue extension (4th Street to Foothill Boulevard) will be aligned to be compatible with a future railroad grade separation, mainline and spur railroad service needs and possible utility corridors. Specific alignment shall be determined at time of development of impacted properties. The alignment shall accept existing connection points at 7th Street and Foothill Boulevard and be compatible with a grade separation design approved by the City Engineer.



Haven and Milliken  
Separation Studies

Railroad grade separations are proposed at Haven and Milliken Avenues and the AT & SF railway. The precise alignments for Haven and Milliken Avenues at the separation locations are shown on plans prepared by DKS Associates and are included in Appendix C. These plans define the additional right-of-way needed to the adjacent parcels. Modification to these design requirements shall be approved by the City Engineer in conjunction with review of development proposals on properties abutting the grade separation.

Peak Period  
Intersection Volumes

Peak period intersection volumes were utilized to determine the number of lanes needed for an intersection to operate at Level of Service "D". The results are shown in Table II-3. A further explanation of the Volume/Capacity Ratio V/C is given in Appendix A.

TABLE II-3  
PROJECTED VOLUME/CAPACITY RATIOS-INTERSECTIONS,  
AT LEVEL OF SERVICE "D"

<u>Intersection</u>	<u>PM Peak Hour</u>		<u>V/C</u>	<u>Lane Additions*</u>
	<u>Volume</u>	<u>Capacity</u>		
Haven/Foothill	1890	1500	1.26	Dble. left turn-- all approaches
Haven/Arrow Route	1340	1500	0.89	Dble. left turn lanes--NB; WB
Haven/4th	1470	1500	0.98	Dble. left turn lanes--WB; SB
Milliken/Foothill	1590	1500	1.06	Dble. left turn lanes--NB; SB; EB
Milliken/Arrow Route	1200	1500	0.80	Dble. left turn lanes--NB; SB; EB
Milliken/4th	1500	1500	1.00	
Rochester/Foothill	1440	1500	0.96	

\*Unless otherwise stated, all approaches are assumed to have one left turn and one right turn lane in addition to mid-block through lanes.

As Table II-3 indicates the Foothill/Haven intersection is the most overloaded intersection in the City and is expected to operate substantially above capacity. Foothill/Milliken is expected to operate above capacity and Milliken/4th will operate just at capacity.

Further studies will be needed in many cases to determine lane lengths and where land use changes have occurred. A detailed presentation of all major intersection lanes and right-of-way requirements is presented in Figure II-5. At some intersections, double left turn lanes and/or a right turn lane may be necessary.

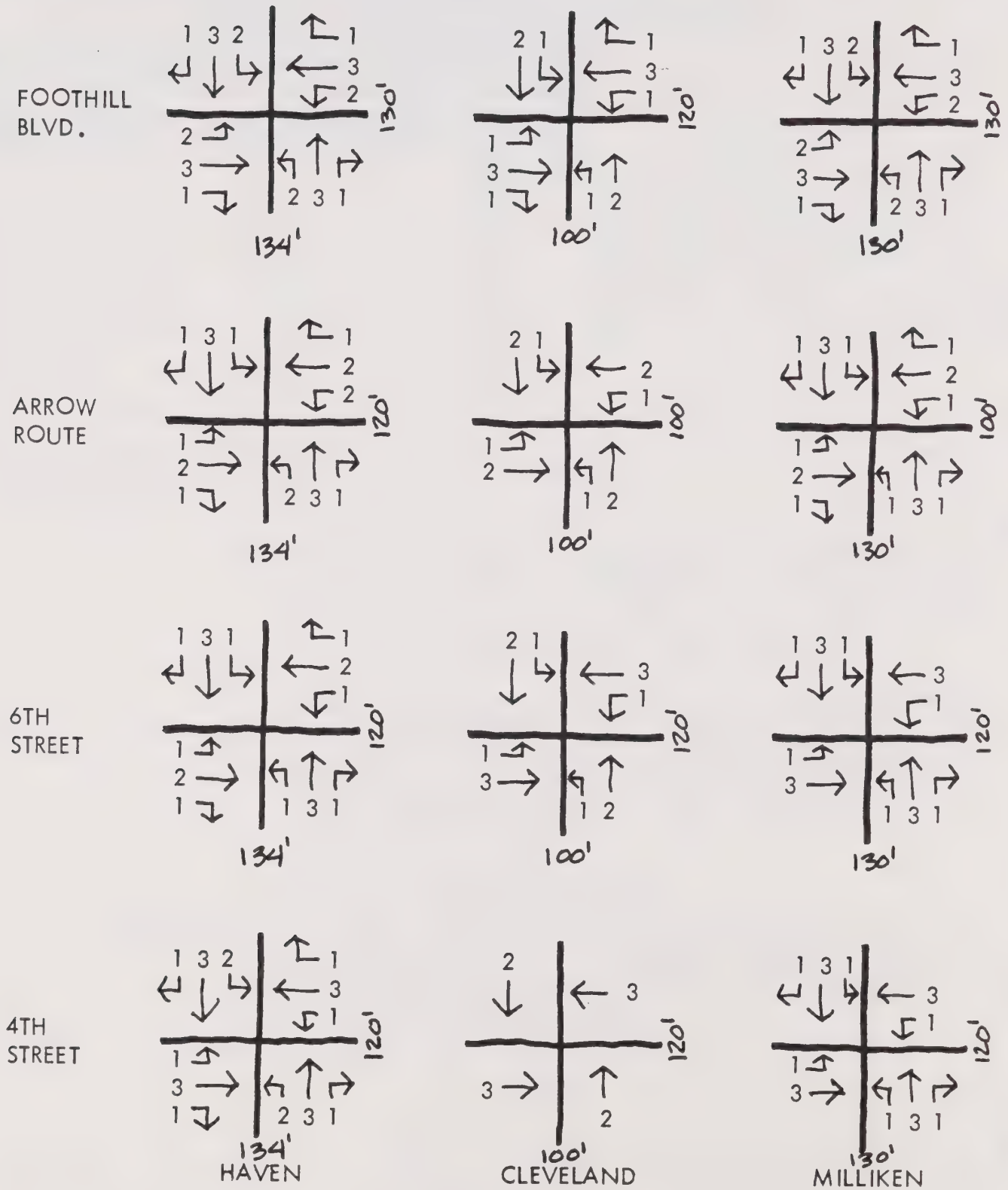
#### CIRCULATION AND ACCESS POLICIES

**Policies and Arterials** In order to ensure the effectiveness and capacity of arterials, it will be necessary to establish and enforce rigid access control policies. These controls are currently in effect under the provisions of the Planning Commission Resolution 78-29.

- o Non access to all arterials shall be dedicated to the City wherever suitable alternative access may be developed from local or collector streets.
- o Where access must be granted to an arterial, said access shall be limited to one point for 300 feet of frontage or one point per parcel with less than 300 feet of frontage. It is the intent of the policy to establish a minimum 300 foot spacing between driveways.

**Figure II-6**

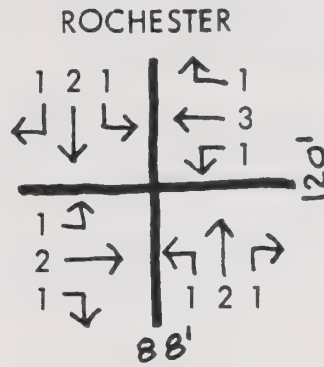
**DIRECTIONAL LANE AND R.O.W. REQUIREMENTS AT INTERSECTIONS**



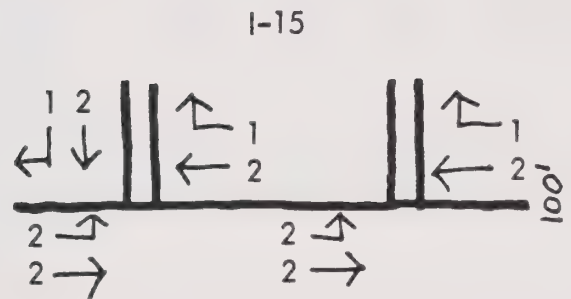


# DIRECTIONAL LANE AND R.O.W. REQUIREMENTS AT INTERSECTIONS

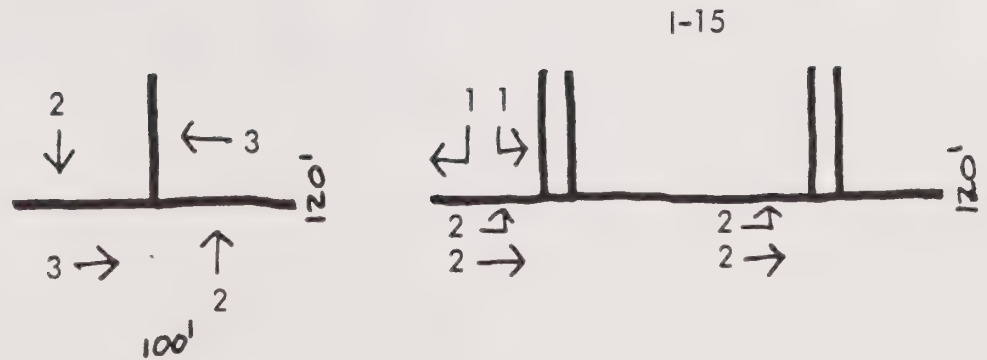
FOOTHILL  
BLVD.



7TH  
STREET



4TH  
STREET



- o Combined access to arterials between adjacent properties shall be encouraged wherever possible to reduce the number of encroachments.
- o Access points shall, wherever possible, be located a minimum of 100 feet from the back of curb returns at intersections on 4 lane or wider highways.
- o Where otherwise compatible with this policy, access shall be located opposite existing or planned points on the opposite side of the street.
- o In addition to the controls outlined in Planning Commission Resolution 78-29, several additional restrictions will be necessary.
  - Median island breaks and left turn access should be limited to approximately quarter mile spacings on the following major divided arterials: Haven Avenue, Foothill Boulevard, and Milliken Avenue, Fourth Street, and Sixth Street. Table II-4 shows the recommended arterial median opening spacing which will provide adequate storage lengths for left turn lanes, signal spacing, and provide adequate capacity.
  - Signalized intersections should be spaced a minimum 1/4 mile apart to achieve adequate two-way progression.
  - For safety and efficiency, side friction along major arterials should be minimized. Therefore, on-street parking should be eliminated, median obstructions should be minimized, and left-turn movements and access to driveways should be confined to designated locations.
  - Along Haven and Milliken, additional access restrictions may be required due to traffic volume constraints.

TABLE II-4

## LOCATIONS OF MEDIAN LEFT TURN OPENINGS ON MAJOR DIVIDED HIGHWAYS

<u>MAJOR STREET</u>	<u>MEDIAN OPENING LOCATIONS</u>
Haven Avenue	Foothill Boulevard, Civic Center Drive, Arrow Route, Jersey Boulevard, 7th Street, 6th Street, Trademark Street, 4th Street.
Milliken Avenue	Foothill Boulevard, Day Creek Boulevard Extension, Arrow Route, Jersey Boulevard, 7th Street, 6th Street, 5th Street, 4th Street.
Foothill Boulevard	Haven Avenue, Aspen Avenue, Spruce Avenue, Elm Avenue, Milliken Avenue, 1200' E/O Milliken Avenue, 1000' W/O Rochester Avenue, Rochester Avenue, Day Creek Boulevard, Route 15, Etiwanda Avenue, Cornell Avenue, East Avenue.
6th Street	Haven Avenue, Utica Avenue, Cleveland Avenue, 1300' E/O Cleveland Avenue, Milliken Avenue, Pittsburgh Avenue, 950' E/O Pittsburgh Avenue, Buffalo Avenue, Rochester Avenue, Old Rochester Avenue/Route 15.
4th Street	Archibald Avenue, Lucas Ranch Road, Turner Avenue, Center Avenue, Haven Avenue, Utica Avenue, Cleveland Avenue, 1300' W/O Milliken Avenue, Milliken Avenue, Pittsburgh Avenue, Buffalo Avenue, Route 15.

## Local Street Policy

The required local street pattern should allow the maximum flexibility to each parcel owner or developer while having the least adverse impact on arterial traffic flow. The circulation pattern developed for this plan and shown in Figure II-6, complies with this principle by recognizing the following:

- o Streets should be provided along property lines where possible.
- o Intersections with arterials should be limited to one per quarter mile wherever possible.
- o Local streets should be planned to relieve traffic demand on arterials by providing parallel routes.
- o The street pattern within a parcel should not be a part of this Plan, except where necessary to meet the above criteria.

## Freeway Access

Freeway access will be a significant concern in the Industrial Area. The majority of existing interchanges have a low capacity for traffic due to single-lane ramps, left-turn access onto and off arterial routes, inadequate left-turn stacking space, and narrow freeway crossing structures. The Interstate 10 interchanges are shared by the Industrial Area with the City of Ontario as well as by the northern part of Rancho Cucamonga. The Ontario Center area and the Ontario International Airport also utilizes the interchanges and are expected to generate increased traffic volumes in the future.

A new freeway interchange is recommended at 6th Street and Interstate 15. Improvements at the Foothill Boulevard/Interstate 15 interchange are recommended. DKS Associates evaluated service needs for both interchanges and proposed interchange improvements for each location are being pursued with CalTrans.

Approaches to I-10 show a potential deficiency of capacity across all interchanges including Milliken Avenue, Haven Avenue, Archibald Avenue, Vineyard Avenue and 4th Street south of the Industrial Area. This deficiency will be intensified with development of the existing Ontario Center. This situation will require careful monitoring in the future and a long range plan for mitigation should be developed to ensure that needs are met as development occurs. This effort will involve CalTrans, the City of Ontario, and Ontario International Airport.



## Alternative Transportation

### Transit

The Industrial Area, at full build-out, is expected to result in a significant traffic volume impact unless conscious efforts are made to reduce traffic volumes. Traffic volumes in this study were computed based on a strong transportation system management plan. The rates used to compute traffic volumes can be achieved only if strong measures to reduce volumes include public transit improvements, high occupancy vehicle lanes, ride-sharing incentives, modified work hours, bicycle facilities, on-site employee services, and regulation of employment intensity.

### Public Transit Improvements

Potential transit routes and transportation facilities within the Industrial Area include:

- o Regional bus service loops to all areas, with heavy emphasis on service to employment centers and shopping center.
- o Local transfer locations such as park and ride, and major bus transfer points to facilitate greater service use along local and regional commuter traffic routes.
- o Rail commuter lines utilizing existing rail lines.

Figure II-7 presents the basic transit circulation concept.

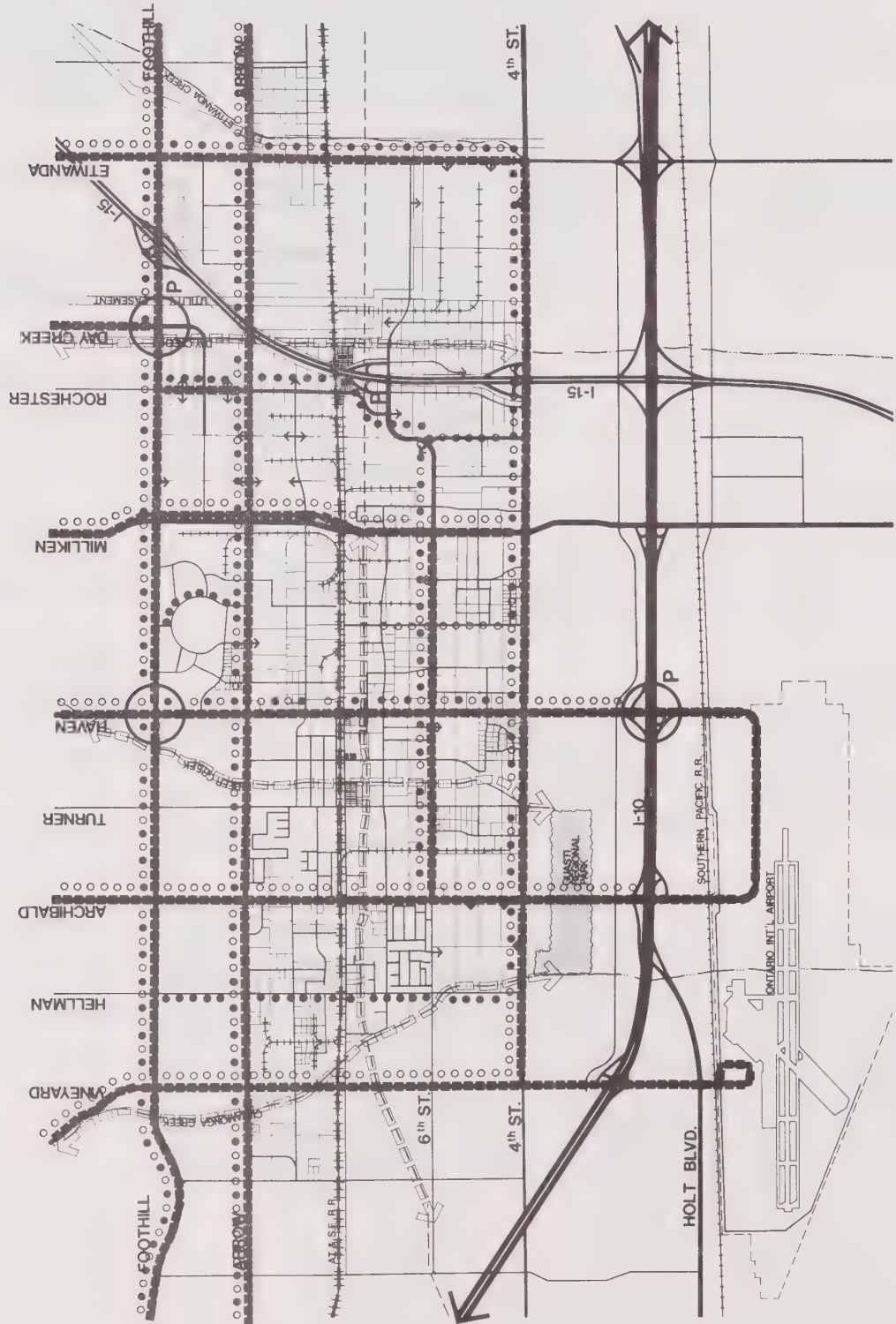
As of mid-1980, the State Department of Transportation (CalTrans) is considering a commuter rail demonstration project that would like San Bernardino and Los Angeles on the AT & SF which runs through the middle of the Industrial Area.

Serious, in-depth transit planning studies will have to be conducted to determine potential transit ridership, transit routes, locations for transit stops, operation in detail and implementation of a transportation system. The City and SANBAG should jointly prepare a Transportation System Management Plan (TSMP) for implementation in the City including the Industrial Area.

Figure II-7

**TRANSIT AND  
PEDESTRIAN  
CIRCULATION CONCEPT**

- TRANSIT ROUTES
- MAJOR TRANSIT  
TRANSFER AREAS
- POTENTIAL COMMUTER RAIL
- FREEWAY/HOV LANES
- BIKE ROUTES
- PEDESTRIAN ROUTES
- P PARK and RIDE
- REGIONAL MULTI-USE  
TRAILS



**CITY OF  
RANCHO CUCAMONGA  
INDUSTRIAL AREA PLAN**



Park and Ride lots have been built at several places in the City and at locations adjacent to the I-10 Freeway in Ontario. Since the Industrial Area is only one-half mile away from this freeway, and interchanges are located at Vineyard, Archibald, Haven, Milliken, and Etiwanda, this also provides an excellent opportunity to link the local transit system with the regional network.

In the age of energy shortage, the need for public transportation and alternative forms of transportation are increasingly apparent. Combined with the prospect that the roadway system in the City and Industrial Area could reach congestion levels in a rather short period of time without transit, the need is even more critical. The City of Rancho Cucamonga should conduct future transportation studies to determine specific implementation procedures within the Industrial Area in coordination with SANBAG Regional Planning Agency.

#### Ride-Sharing Incentives

Ride-sharing encompasses all forms of transportation that involve pre-arranged shared rides for travelers with similar origins, destinations and travel schedules. Included are carpools, vanpools and subscription bus (commuter club bus) services. The primary market for ride-sharing is the work commute trip, which makes ride-sharing a promising means to reduce traffic in the Industrial Area.

Within Rancho Cucamonga, ride-sharing is facilitated and promoted by Commuter Computer. Results elsewhere have indicated that employer-sponsored programs can be most effective, particularly for larger employers. Actions that can be taken by employers include:

- o Distributing ride-sharing matching forms to all new employees and regularly to continuing employees;
- o Surveys of employees to determine interest in ride-sharing;
- o Designation of staff member to assist other employees in finding carpool matches;
- o Advertising and promotions to generate interest and viability for the program;
- o Tailoring of work hours to facilitate ride-sharing;
- o Provisions of preferentially located or priced parking for carpoolers;



- o Leasing of vans, at cost, for employees who vanpool;
- o Provision of company fleet cars at nominal cost for commuting by carpoolers;
- o Subsidization of subscription bus services, particularly in the early period of formation; and
- o Modified work hours.

#### Rail Access

The Atchison, Topeka and Santa Fe (AT&SR) railroad currently has a single track main line running through the middle of the Industrial Area. There are several lead tracks and spurs serving some of the established industrial plants. It is in the intent of this Plan to promote rail usage in the Industrial Area of Rancho Cucamonga and certain provisions are incorporated.

Development within Subareas 2, 5, 9, 10, and 15 will have potential rail service access. Development standards for providing for future rail spur access are set forth in Part III of this Plan.

#### Bicycle Routes and Facilities

Within the Industrial Area, Rancho Cucamonga's relatively flat terrain and dry climate are particularly conducive to bicycling. However, relatively few commuters live close enough to bicycle to work on a regular basis. A bike route network which connects to a City-wide network is shown on Figure II-7. The system consists of 5' striped bike lanes on both sides of the improved streets with no side parking allowed.

#### Pedestrian Routes

Increased dependency on public transportation means an increase of pedestrian activities and the need for environmental amenities for pedestrians. A number of employee and business service uses will be dispersed throughout the Industrial Area for the convenience of workers. This means that an extensive network of pedestrian paths must be developed in the Industrial Area, linking places of work with eating and drinking places, office supply and business service establishments, transit stops, recreation, as well as residential areas to the north and west of the Industrial Area. As shown in Figure II-7, major pedestrian routes are proposed along all major streets in the area, which connect with the City-wide network.



Generally, the pedestrian paths should have a minimum width of 4 feet, where pedestrian traffic flow is limited. In the areas near transit stops and support retail and service centers, malls and plazas fronted by restaurants and shops should be encouraged. Special boulevards, major and some secondary arterials will have meandering sidewalks on both sides of the street. Other streets will have sidewalks as needed.

## V. Flood Management

There are two distinct facets of flood management, each of which requires a different type of analysis.

Drainage Programs are local in nature and involve draining development properties and streets. They tend to temporarily inconvenience motorists and pedestrians, and are typically handled through a network of storm drains, channels, or swales along streets.

Flood Hazards are considered threatening to life and/or damaging to property. Flows are generated by runoff from regions outside the Industrial Area, and require large, costly flood control facilities for containment.

### Storm Drainage

As part of the consideration of the Industrial Assessment District, the City constructed a drainage system for the Industrial Area bounded by Arrow, 4th Street, Deer Creek, and the Devore Freeway.

In addition, new development shall be regulated to ensure that permanent drainage facilities and/or outlets are available to serve the development. A plan for temporary drainage improvements, approved by the City Engineer, may enable developers to proceed prior to completion of permanent facilities. An approved plan must demonstrate that all flows in excess of natural runoff volumes generated by the proposed project will be contained on-site and will not increase or concentrate flows onto adjacent properties. To achieve this objective and promote water conservation, the City should encourage a variety of measures including those that increase the infiltration of precipitation at its source. The following are techniques which will be considered appropriate for application in the Industrial Area.

Dutch Drains are gravel filled ditches with optional drainage pipe at the base. They effectively intercept sheet flows and enable precipitation to infiltrate. The surface of the drain may be bare gravel, grass or porous paving. The minimum sizing must be adequate to ensure the infiltration of at least as much precipitation as before development.

Porous Paving can be used on street surfaces or parking lots. A layer of porous asphalt, minimum of 4 inches thick, is laid on top of a graded crushed stone base. The surface and base thickness should vary directly with the traffic load. While porous paving reduces runoff, preserves natural drainage patterns, and enhances groundwater supply, it may become clogged and should be cleaned regularly to function effectively.

Seepage Pits collect runoff and detains the water until it can infiltrate the ground. They are similar to Dutch drains except they do not conduct water along their length when saturated. Seepage pits may be designed to accommodate a maximum design frequency 24-hour storm, or they may be designed to permit infiltration at predevelopment rates. Soils should have a minimum porosity of 0.1 foot per day. Pits should be designed to take the equivalent of 1 inch the whole area of impermeable surface drained and accept runoff from paved area only.

Seepage Areas function in the same manner as seepage pits except they are larger in scale and provide for multi-purpose uses such as parking, open space, or recreational activities. These areas may be paved or porous, or grassed surfaced. The design of these facilities should include methods to prevent sediment clogging.

Retention Ponds are one of the most effective means of reducing peak flows at a point below the impoundment. This is particularly appropriate where frequent flooding occurs in areas immediately downstream. Drawbacks to retention ponds include the large areas required and their potential unsightliness, and costly ongoing maintenance. Where possible, multi-use facilities on a regional scale should be encouraged.

#### Flood Retention

At the current time, and until the completion of major flood control works, flood hazards in the Industrial Area have been identified in association with breakout of flows from Day and Etowanda Creeks. The extent of this flood hazard is identified on the latest flood hazard maps published by the Federal Emergency Management agency. Any project falling within a flood hazard area will be required to comply with the provisions of Municipal Code, Chapter 19.12.



In the Industrial Area, flood hazards and water overflow have been particularly acute in the area between Deer and Day Creeks. The overflow in this area has generally been a braided, vagrant sheet flow, primarily affecting the areas adjacent to Deer and Day Creeks.

Presently, the only major flood control project under construction in the vicinity of the Industrial Area is the U.S. Army Corps of Engineers' Cucamonga Creek Improvement Project. This project provides for a concrete channel and debris basin for Deer Creek and when completed (December of 1982), the major potential flood source for the western portion of the Industrial Area will be eliminated. Currently, there is no federal improvement project planned for Day Creek.

## VI. PUBLIC SERVICES

### Water Supply

The Cucamonga County Water District (CCWD) provides all water supply services to the Industrial Area. The CCWD uses three water sources to meet the domestic and industrial demands of the area: groundwater, canyon water, and imported water. Approximately 88% of the City's water supply comes from underground sources.

In 1980, the CCWD will have 16 wells in operation, pumping approximately 35,881,280 gallons per day (gpd). The 20 reservoirs in the district will have a storage capacity of 80,390,000 gallons. Within the Rancho Cucamonga area there are eight water conservation areas to capture surface water runoff during rains which helps recharge underground basins and provides flood control. None of these conservation/recharge facilities are located within the boundaries of the Industrial Area.

After groundwater, the second most important source of water supply for the Industrial Area is canyon water and tunnel flows drawn from Cucamonga, Deer, Day, and East Etiwanda Canyons. These surface diversions are introduced into the City's water supply after chlorination in accordance to state health regulations. However, the CCWD has completed construction of the Royer-Nesbit Treatment plant which will treat by chlorination and filtration surface flows from canyons on the east side of the District. The plant also has a connection to the Southern California Municipal Water District (MWD) Foothill Feder to receive state project water when necessary to meet future water demands.



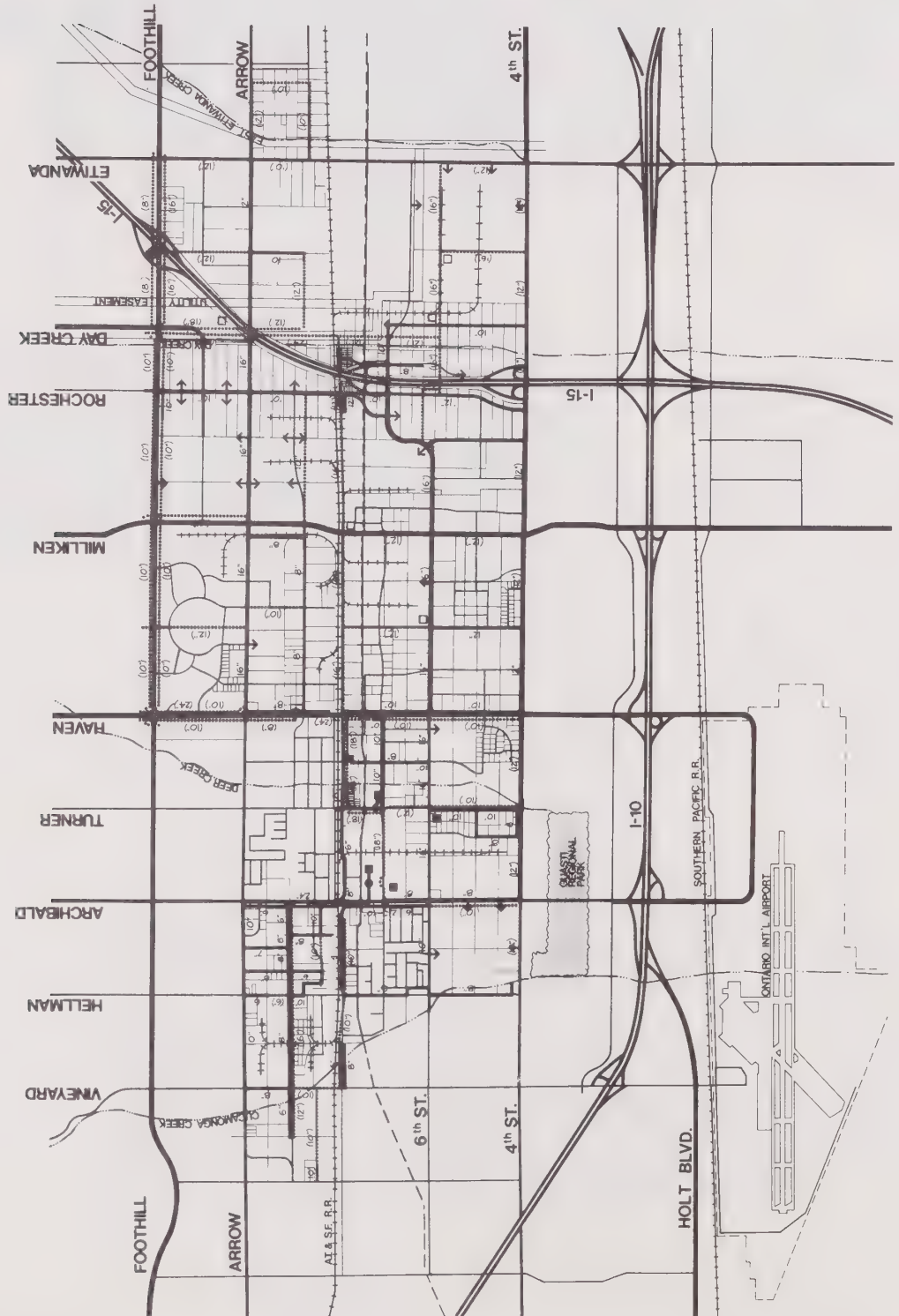
Over the entire year the average daily consumption rate is approximately 13 million gallons. The current breakdown of water consumption by user is shown on Table II-4.

TABLE II-5  
AVERAGE WATER CONSUMPTION

<u>User</u>	<u>Percentage of Total Water Supply</u>
Residential	60%
Industrial	20%
Commercial	12%
Agricultural	7%
Construction	<u>1%</u>
	100%

The District is responsible for construction of all water main lines and is presently ahead of the water pipeline construction schedule outlined in its 1974 Water Master Plan. See Figure II-9 which indicates the location of existing and proposed distribution pipelines in the Industrial Area. Any new service lines needed to connect a development with a water main within the City are the responsibility of the respective developer.

**Figure II-8  
WATER SYSTEM**



**EXISTING:**

— DISTRIBUTION PIPELINE

■ WELL

● RESERVOIR

**PROPOSED:**

- - - DISTRIBUTION PIPELINE

□ WELL

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### Wastewater Treatment

In addition to providing water distribution services, the CCWD collects domestic sewage within the Industrial Area. Under provisions of the Regional Sewerage Contract (1972), Chino Basin Municipal Water District (CBMWD) treats the collected sewage flows from CCWD. In addition to primary/secondary sewage treatment, CBMWD also operates and maintains all interceptor systems and water reclamation plants to reuse/dispose of this wastewater. The expansion or the construction of new regional facilities is also the responsibility of the CMBWD. Figure II-10 shows the location of existing and proposed sewer lines.

Presently, CMBWD has three regional wastewater plants. Regional Plant No. 1 (RP 1) in Ontario services the Industrial Area. RP 1 has a 26 mgd capacity and is currently operating at 19 mgd capacity level. Plans are being considered to expand this treatment plant capacity to 37 mgd by the late 1980's.

The CBMWD also operates a nonreclaimable wastewater line (NRWL) which is used to export industrial and other nonreclaimable wastes from the Industrial Area. The NRWL runs along 8th Street to Etiwanda Avenue on the east and to the Los Angeles County Sanitation system (LACSD) on the west. From the Los Angeles County facility, the sewage is transferred to Harbor Island for treatment and ocean disposal. CBMWD has a capacity contract with LACSD for 16 mgd total capacity. (See Table II-5 for available capacity information).

This nonreclaimable wastewater system was designed to serve the future industries within CBMWD. However, domestic waste flows currently are being accommodated in this system until additional treatment capacity can be provided at RP 1.

TABLE II-6  
AVAILABLE CAPACITY IN THE NRWL SYSTEM - 1979<sub>1</sub>

<u>Subsystem</u>	<u>Actual Discharge (cfs)</u>	<u>Capacity Sold (cfs)</u>	<u>Capacity (cfs)</u>	<u>Available Capacity (cfs)</u>
East Edison	4.94	7.1	5	.06
Cucamonga Intertie	5.66	---	8.5	2.84
"G" Street	7.47 <sub>2</sub>	4.4	10	2.53
Joint Center Trunk	13.13 <sub>3</sub>	11.5	15	1.87
Joint Trunk	15.72	13.7	22.9	7.18

<sub>1</sub>Table includes only those lines within the City of Rancho Cucamonga.

<sub>2</sub>Includes domestic discharge from Turner of 3.09 cfs.

<sub>3</sub>Includes a surcharge.

Source: Personal communication, Chino Basin Municipal Water District.

#### Solid Waste Disposal

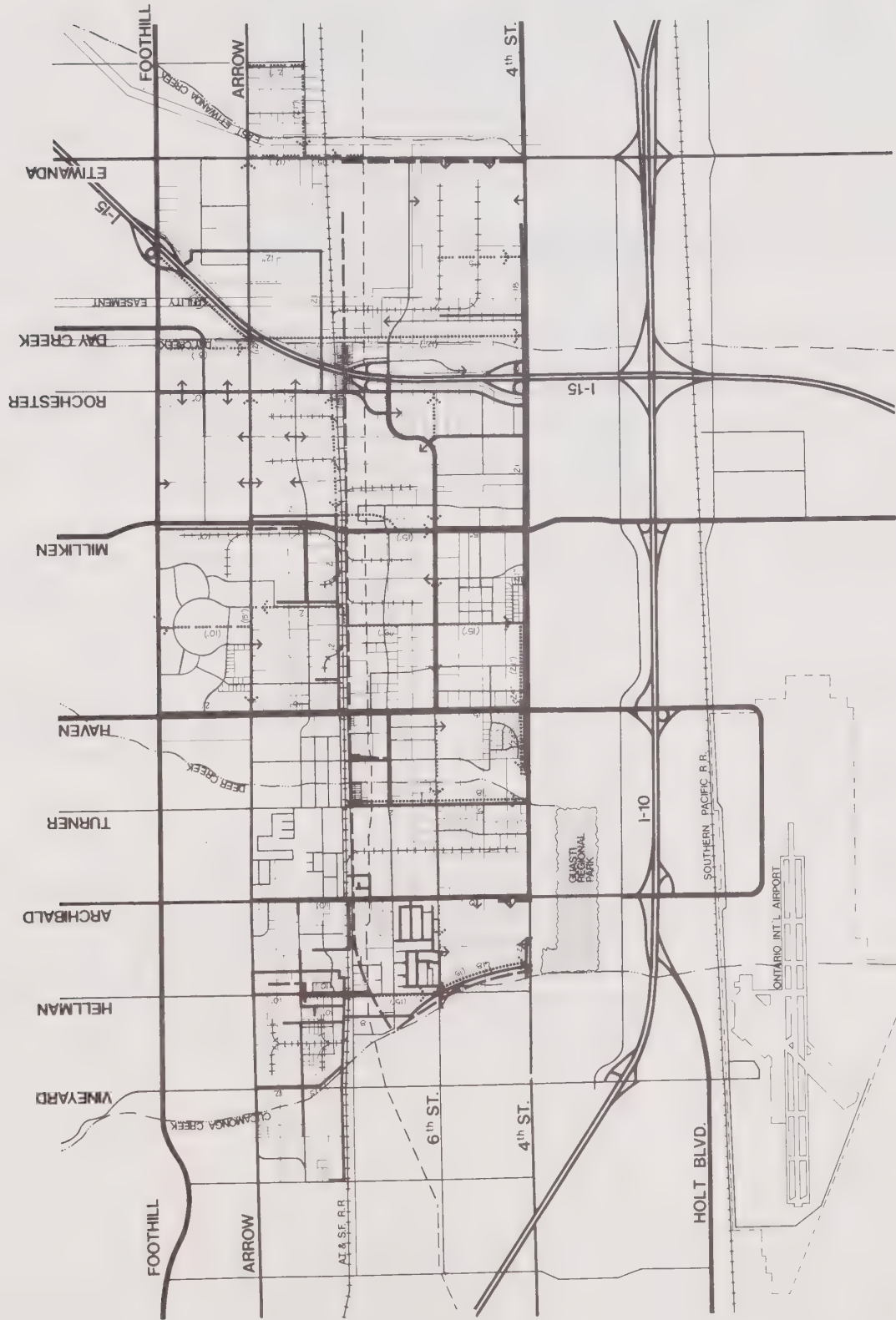
Solid waste collection and disposal services are provided to the Industrial Area by privately-owned companies. The solid waste is trucked to the county-owned site on Milliken Avenue, south of the Ontario Center. Beginning in July 1980, the 106-acre site will be expanded by 75 acres. This expansion will give the site an anticipated life expectancy of 15-20 years.

#### Law Enforcement

Law enforcement services for the Industrial Area are provided by the City of Rancho Cucamonga Police Department which acts under contract with the County Sheriff's Department. The Sheriff Station is located at the intersection of Hellman Avenue on 9th Street (See Figure II-1).



**Figure II-9  
SEWER SYSTEM**



EXISTING:

— SEWERS

- - - NON RECLAIMABLE  
WASTE LINE

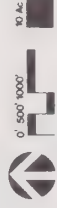
PROPOSED:

— PRIMARY TRUNK SEWERS

- - - TRUNK SEWERS

→ DIRECTION OF FLOW

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### Fire Protection

The San Bernardino County Foothill Fire District provides fire protection services to the Industrial Area. The district operates three fire stations within the City located:

- o One-fourth mile east of the intersection at Baseline and Rochester Avenue.
- o At 9612 San Bernardino Road
- o At 19th and Amethyst

Locations for two additional stations are currently proposed near the intersections of Rochester and Arrow and at 8th, east of Haven. Figure II-1 shows these locations to be located in the general proximity of future expansion. The latter location provides better access as Milliken Avenue (to be constructed in 1982) will be grade-separated from the Atchison Topeka and Santa Fe railroad tracks and has 120 feet right-of-way as will 6th Street, which is proposed for construction as part of the Assessment District Construction Program.

The Industrial Area also lies within the boundaries of the district's mutual aid agreement with the City of Ontario. This agreement calls for a minimum of 13 personnel, 4 fire engines, and a chief officer to be ready to respond to a building fire in the area.

Response time for the City (the time required to respond to a call) is considered average for the type and expanse of the district. Most calls can be handled within 2 to 7 minutes.

Communities are rated by private insurance companies for their ability to respond and put out fires. Ratings range on a scale of 1 to 10 with diminished effectiveness as the scale increases. The district began services to the area in 1975 and received the rating that had applied to the services formerly offered to Alta Loma and Cucamonga. The Insurance Service office in Los Angeles resurveyed the fire protection services at the end of 1979, and classified the Industrial Area as Class 4.

## VII. HOUSING AND GROWTH MANAGEMENT

Through the Housing Element of the General Plan, the City has established a high priority to providing for housing to all economic segments of the Community. A determination of the Housing Element was that the industrial base would provide the major driving force for the housing demands within the City of Rancho Cucamonga and other surrounding communities. Through the Housing Element, the City established a goal of providing housing for 30% of the local employment in the Industrial Area.

The Housing Element provides for an array of various housing program options which the City and private sectors will pursue in order to meet the housing demands of the Community. The housing programs which relate specifically to the Industrial Specific Plan include:

- o The City will implement a development monitoring program which reviews progress towards housing goals. The implementation of the development monitoring program requires industrial employers to provide the City with information on the number of future employees, employee job classification, wage and salary range, and those employees with secondary income. This information will be required at the earliest possible date of the development review process in order for the City and private sectors to adequately assess future housing needs.
- o The City will develop a housing information program which will provide information to those newly employed, or wishing to find housing within the Community, regarding potential programs and housing opportunities available along with other housing information. An example of this program would be to require developers to provide announcements to local employers of housing units for sale or rent thirty (30) days prior to offering the units to the general public. These announcements would be posted in places of local employment.





## **PART III**

# **DEVELOPMENT STANDARDS AND GUIDELINES**

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## PART III DEVELOPMENT STANDARDS AND GUIDELINES

### I. INTRODUCTION

The development standards and guidelines for the Industrial Area Specific Plan of Rancho Cucamonga consists of three components:

- Land Use Types
- Urban Design Standards and Guidelines
- Development Standards

#### A. Purpose

The purpose of this section is to set up the specific standards and guidelines which will be used for the development in the Industrial area. Standards and guidelines are designed to be compatible with the five Land Use categories. The standards vary depending on location and land use category. These variations are applied through special conditions within the seventeen subareas of the Industrial plan, as shown on Figure III -1. These subareas represent specific Land Use characteristics and development constraints which can be best dealt with on a subarea basis rather than through the application of broadly applied development standards.

#### B. Interpretation

The provisions of this Specific Plan are not intended to interfere with or void any easements, covenants, or other existing agreements which are more restrictive than the provisions of this Specific Plan. If ambiguity or conflict arises concerning the appropriate classification of a particular use within the meaning and intent of this title, or if ambiguity exists with respect to matters of development standards and guidelines or land use boundaries as set forth herein, it shall be resolved in accordance with the procedures as set forth in the Development Code.

C. Use Determination

In order to ensure that the Specific Plan regulations will permit all similar uses in each district, the Planning Commission, upon its own initiative or upon written request shall determine whether a use not specifically listed as permitted, secondary, accessory or temporary use in any district shall be deemed a permitted use or conditional use in one or more districts on the basis of similarity (in function, operation) to uses specifically permitted, in accordance with the procedures as set forth in the Development Code. The procedures of this section shall not be substituted for the amendment procedure as a means of adding new uses to the list of permitted or conditional uses.

D. Relationship to Other Regulations

The Specific Plan will provide the user with most of the information needed to determine what City policies, standards, and regulations will guide the development of a particular parcel. However, areas not specifically covered by this Plan (i.e., construction standards, health regulations, variance and appeal procedures, subdivision procedures, etc.) will continue to be governed by existing City regulations, and no provision of this Plan is intended to repeal, abrogate, annul, impair, or interfere with any existing City ordinance except as is specifically repealed by adoption of this Plan.



## II. LAND USE TYPES

There are 51 possible land use types within the Industrial Plan area. Each subarea has a certain set of land use types based upon its land use category and surrounding conditions. Table III-1 lists the land use types by subarea. Definitions of these land uses are in Table III-2.

### Permitted Uses

Permitted uses are those land uses allowed in a given subarea subject to the development regulations of the Plan. Conditional permitted uses, because of their unusual site development requirements or unique operating characteristics, are subject to the granting of a Conditional approval by the Planning Commission.

### Conditional Uses

Projects requiring a Conditional Use Permit shall be required to comply with the regulations of Section 17.04.030 of the Development Code. The Planning Commission shall make the following findings before granting a Conditional Use Permit:

1. That a proposed use is in accordance with the General Plan, the Specific Plan, and the purposes of the Land Use category in which the project is located.
2. That the proposed use, together with conditions applicable thereto, will not be detrimental to the public health, safety and welfare, or materially injurious to properties or improvements in the vicinity.
3. That the proposed use will comply with each of the applicable provisions of the Development Code and Specific Plan.

### Interim Uses

The following Interim uses shall be permitted, provided that such uses and their locations shall not preclude full development in accordance with the development regulations of the Plan and no permanent buildings are constructed: Agricultural uses, including roadside stands, and private parks and picnic areas are permitted uses prior to development. Parking and storage areas, park-and-ride lots, and other uses similar in character shall be permitted subject to a Conditional Use Permit and to standards contained in Section IV.H of Part III.

A Conditional Use Permit shall be approved for a two (2) year period. Extensions may be granted up to an additional 36 months for a maximum time limit not to extend more than five (5) years beyond the original date of approval.

As a condition of approval, an agreement between the City and applicant shall be completed stipulating timing, installation of permanent improvements and buildings, and/or restoration of the site to its original condition. At the end of five (5) years, the use shall be removed of the site developed in accordance with full development regulations of the Plan.



**TABLE III-1**  
**SUMMARY OF LAND USE TYPE BY SUBAREA**

● PERMITTED USE    + CONDITIONALLY PERMITTED USE    NOTE: Non-marked Uses Not Permitted

USE TYPES	Land Use*	IP	GI	GI	GI	GI	GI	IP	IP	GI	MI/	GI	GI	IP	GI	GI	HI	IP	IP
	Subareas	HO	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17
<b>MANUFACTURING</b>																			
Custom			●	●	●	●	●	●	●	●		●	●	●	●	●		●	●
Light			●	●	●	●	●	●	●	●		●	●	●	●	●		●	●
Medium			+	+	+		●			+	●	●	●		●	●	●		
Heavy																	●		
Minimum Impact Heavy											●						●		
<b>OFFICE PROFESSIONAL, DESIGN &amp; RESEARCH</b>																			
Administrative & Office		●	+		+	+		●	●	+			+	●	+	+		●	●
Professional/Design Services		●	+		+	+		●	●	+			+	●	+	+		●	●
Research Services			●	●	●	●	●	●	●	●		●	●	●	●	●		●	●
<b>WHOLESALE, STORAGE &amp; DISTRIBUTION</b>																			
Public Storage			+	+	●	+	+			●	+	+	+			+	●		
Light			●	●	●	●	●	●	●	●	●	●	●	●	●	●	●	●	●
Medium			+	●	●	+	●			●	●	●	●			●	●		
Heavy							+				●	+					●		
<b>COMMERCIAL</b>																			
Agricultural/Nursery Supplies & Services			●	●	●		●			●		●				●			
Animal Care			+		+					+		+		+		+			
Automotive Fleet Storage			+		+					+	+	+							
Automotive Rental/Leasing			●		●	●		+	+	+			●	+				+	+
Automotive /Light Truck Repair-Minor			●	+	●	+	+			●			●		●	●			
Automotive / Truck Repair-Major			●	+	●		+			●			●		+	+			
Automotive Sales								+	+	+				+					+
Automotive Service Station			+		+	+		+	+	+			+	+	+	+		+	
Building Contractor's Office & Yards			●	+	●	+	+			●			●		●	●	●		
Building Contractor's Storage Yard																	●		
Building Maintenance Services			●		●	●		●	●	●			●	●	●	●	●	●	●
Building & Lighting Equipment Supplies & Sales			●	+	●	+	+			●	●	+	●		+	+	●		
Business Supply Retail & Services	●	●	●	●	●	●		●	●	●	●		●	●	●	●	●	●	●
Business Support Services	●	●	+	●	●	+	+	●	●	●	+	+	●	●	●	●	+	●	●
Communication Services	●	●	●	●	●	●		●	●	●	+		●	●	●	●	+	●	●
Convenience Sales & Services	+	+	+	+	+	+	+	+	+	+	+	+	+	+	+	+	+	+	+
Eating & Drinking Establishments	●	●	●	●	●	●	●	●	●	●	●	●	●	●	●	●	●	●	●
Entertainment	+					+		+	+	+				+	+			+	+
Extensive Impact Commercial																	+		
Fast Food Sales	+			+	+			+	+	+			+	+		+		+	+
Financial, Insurance & Real Estate Services	●	+		+	●			●	●	+			+	●				●	●
Food & Beverage Sales	+	+	+	+	+	+	+	+	+	+	+	+	+	+	+	+	+	+	+
Funeral & Crematory Services					+			+										+	
Heavy Equipment Sales & Rentals			+	+	+		+			+	+	+	+				●		
Hotel/Motel	+							●	●					●				●	●
Laundry Services			●		●	●				●			●		●	●			
Medical/Health Care Service	●	+		+	+			+	+	+			+	+	+	+		●	+
Personal Services	+	+		+	+			+	+				+	+	+	+		+	+
Petroleum Products Storage						+				+	+	+	+				+		
Recreational Facilities	+	+	+	+	+	+	+	+	+	●	+	+	+	●		●		●	●
Repair Services			●	●	+	●	●			●	●	●	●				+		
Scrap Operation																	+		
<b>CIVIC</b>																			
Administrative Civic Services		●	●		●	●		●	●	●			●	●	●	●		●	●
Cultural		●				+		+	+					+	+	+		+	+
Extensive Impact Utility Facilities			+				+				+	+	+				+		
Flood Control/Utility Corridor			●	●	●	●	●	●	●	●	●	●	●	●	●	●			
Public Assembly	+	+		+	+			+	+	+			+	+	+	+		+	+
Public Safety & Utility Services	+	+	+	+	+	+	+	+	+	+	+	+	+	+	+	+	+	+	+
Religious Assembly	+	+		+	+			+	+	+			+	+	+	+		+	+

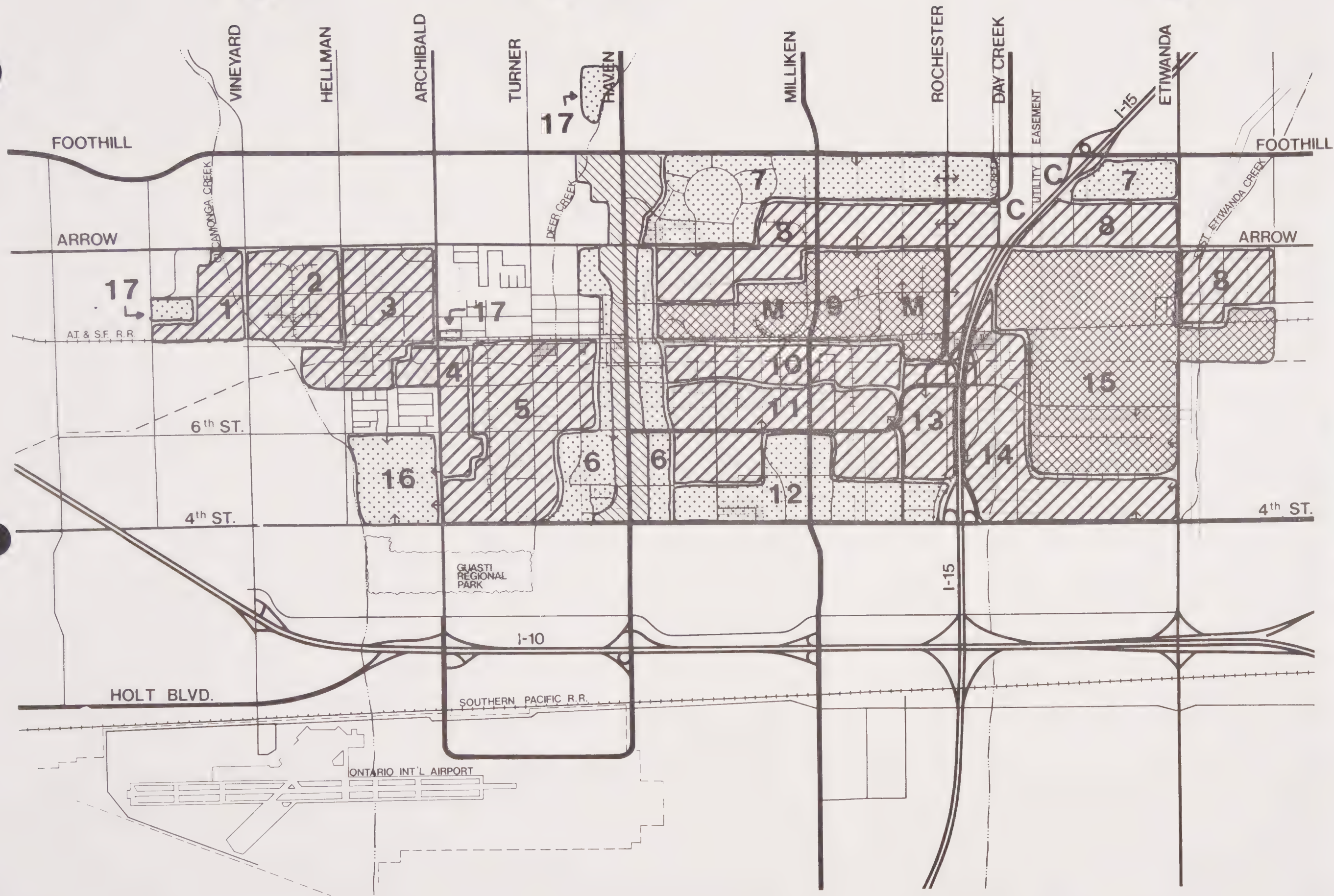
\* IP-Industrial Park  
HO-Haven Ave. Overlay District

GI-General Industrial


MI/Hi-Minimum Impact Heavy Industrial  
HI-Heavy Industrial







**Figure III-1  
SUBAREAS**

- C** COMMERCIAL
-  HAVEN OVERLAY DISTRICT
-  INDUSTRIAL PARK
-  GENERAL INDUSTRIAL
-  MINIMUM IMPACT
-  HEAVY INDUSTRIAL

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TABLE III-2

LAND USE TYPE DEFINITIONS

Uses shown in Table III-1 are defined as follows:

A. MANUFACTURING USE TYPES

Custom Manufacturing: Activities typically include, but not limited to; manufacturing, processing, assembling, packaging, treatment, or fabrication of custom made products. These types of business establishments do not utilize raw materials for their finished products, but rather, may utilize "semi-finished" type of manufactured materials for their custom made-to-order products. The finished products from these business establishments are ready for use or consumption and may include on-site wholesale and retail of the goods produced. Such uses may include, but are not limited to: jewelry; household furniture; art objects; apparel products; small instruments (musical, electrical or photographic); stationary, and related products: signs and advertising displays; stained glass products; leather products; and assembly of bicycle parts. The uses do not produce odors, noise, vibration, or particulates which would adversely affect uses in the same structure or on a same site. Where 24 hour on-site surveillance is necessary, a caretaker residence may be permitted when approved by a Conditional Use Permit.

Light Manufacturing: Activities typically include, but not limited to, labor intensive manufacturing, assembly, fabrication or repair processes which do not involve large container truck traffic or the transport of large scale bulky products, but may include limited rail traffic. The new product may be finished in the sense that it is ready for use or consumption or it may be semi-finished to become a component for further assembly and packaging. These types of business establishments are customarily directed to the wholesale market, inter-plant transfer rather than the direct sale to the consumer. Such uses may include, but are not limited to: electronic microchip assembly; printing, publishing and allied industries; candy and other confectionery products; bottle, canned soft drinks, and carbonated water; apparel and other finished products; paper board containers and boxes; drugs; small fabricated metal products such as hand

tools, general hardware, architectural and ornamental metal work; toys, amusement, sports, and athletic goods. The activities do not produce odors, noise, vibration, (hazardous waste material) or particulates which would adversely affect other uses in the structure or on the same site. Where 24 hour on-site surveillance is necessary, a caretaker's residence may be permitted when approved by a Conditional Use Permit.

Medium Manufacturing: Activities typically include, but not limited to, manufacturing, compounding of materials, processing, assembly, packaging, treatment or fabrication of materials and products which require frequent large container truck traffic or rail traffic, or the transport of heavy, bulky items. The new products are semi-finished to be a component for further manufacturing, fabrication and assembly. These types of business establishments are customarily directed to inter-plant transfer, or to order from industrial uses, rather than for direct sale to the domestic consumer. Such uses may include, but not limited to: canned food; textile products; furniture and fixtures; converted paper and paper board products; plastic products made from purchased rubber, plastic or resin; fabricated metal products made from sheet metals; electrical and electronic machinery, equipments and supplies; office, computing and accounting machines. Activities may produce noise, odors, vibrations, illumination or particulates that affect the persons residing in or conducting business in the vicinity. Where 24 hour on-site surveillance is necessary, a caretaker's residence may be permitted when approved by a Conditional Use Permit.

Minimum Impact Heavy Manufacturing: Activities typically include but not limited to: manufacturing, compounding of material, processing, assembly, packaging, treatment, or fabrication, activities which may have frequent rail or truck traffic or the transportation of heavy large scale products. Activities in this area may generate noise, odor, vibration, illumination, or particulates which may be obnoxious or offensive to persons residing or conducting business in the vicinity. Uses typically use raw materials to fabricate semi-finished products which include, but are not limited to: forge shops, metal fabricating facilities, open welding shops, lumber woodworking facilities, heavy machine shops, chemical storage and distributing, plastic plants, and light or vacuum casting facilities. Not permitted within this



area are uses which require massive structures outside of buildings such as cranes or conveyor systems or unscreened open air storage of large quantities of raw, semi-refined, or finished products. Where 24 hours on-site surveillance is necessary, a caretaker's residence may be permitted when approved by a Conditional Use Permit.

Heavy Manufacturing: Activities typically include but not limited to: manufacturing, compounding of material, processing, assembly, packaging, treatment, or fabrication. Activities in this area may have frequent rail or truck traffic and the transportation of heavy large scale products. Activities in this area may generate noise, odor, vibration, illumination, or particulates which may be obnoxious or offensive to persons residing in or conducting business in the vicinity. Characteristics of use activities permitted within this area may include massive structures outside of buildings such as cranes, conveyor systems, cooling towers or unscreened open air storage of large quantities of raw, semi-refined or finished products. Uses typically include but are not limited to: vehicular assembly plants, power plants, concrete product manufacturing activities, batch plants, scrap yards, air melting foundries, and aggregate or asphalt yards. Where 24 hours on-site surveillance is necessary, a caretaker's residence may be permitted when approved by a Conditional Use Permit.

B. PROFESSIONAL, DESIGN AND RESEARCH OFFICE USE TYPES

Administrative and Office: Activities typically include, but are not limited to: executive management, administrative, or clerical uses of private, profit orientated firms other than public utility firms. Uses typically include, but are not limited to: corporate headquarters, branch offices, and data storage centers.

Professional/Design Services: Activities typically include, but are not limited to: the provision of advice, design, information, or consultation of a professional nature. Uses typically include, but are not limited to: architects, engineers, lawyers, and accountant offices, and a planning or educational research service. Uses may include advertising offices and work activities.

Research Services: Activities typically include, but are not limited to: research, design, analysis and development, and/or testing of a product; activities typically include, but are not limited to: testing laboratories, acoustical chambers, wind tunnels, and main frame computer services. Such uses do not promote odors, noise, vibration, or particulates which would adversely affect uses in the same structure or on the same site.

C. WHOLESALE, STORAGE AND DISTRIBUTION USE TYPES

Public Storage: Activities include mini-warehouse or recreation vehicle storage facilities for the rental or lease of small scale enclosed storage units or parking spaces primarily to individuals rather than firms or organizations. Activities to store household items other than storage operations are not allowed on the premises. Where 24 hour on-site surveillance is necessary a caretaker's residence may be permitted when approved by a Conditional Use Permit.

Light Wholesale, Storage, and Distribution: Activities typically include, but are not limited to: wholesaling, storage, and warehousing services and storage and wholesale to retailers from the premises of finished goods and food products. Activities under this classification shall be conducted in enclosed buildings and occupy 50,000 square feet or less of building space. Retail sales from the premises may occur when approved as a Conditional Use. Where 24 hours on-site surveillance is necessary, a caretaker's residence may be permitted when approved by a Conditional Use Permit.

Medium Wholesale, Storage and Distribution: Activities typically include, but are not limited to: wholesale, storage and warehousing services, moving and storage services, storage and wholesaling to retailers from the premises of finished goods and food products, and distribution facilities for large scale retail firms. Activities under this classification shall be conducted in enclosed buildings and occupy greater than 50,000 square feet of building space. Included are multi-tenant or speculative buildings with over 50,000 square feet of warehouse space. Where 24 ours on-site surveillance is necessary, a caretaker's residence may be permitted when approved by a Conditional Use Permit.

Heavy Wholesale, Storage and Distribution: Activities typically include, but are not limited to: warehousing, storage, freight handling, shipping, trucking services and terminals; storage and wholesaling from the premises of unfinished, raw, or semi-refined products requiring further processing, fabrication or manufacturing. Typically uses include, but are not limited to: trucking firms, automotive storage or impound yards, and the wholesaling of metals, minerals and agricultural products. Outdoor storage is permitted. Where 24 hours on-site surveillance is necessary, a caretaker's residence may be permitted when approved by a Conditional Use Permit.

D. COMMERCIAL USE TYPES

Agricultural/Nursery Supplies and Services: Activities typically include, but are not limited to: the retail sale from the premises of feed and grain, fertilizers, pesticides, herbicides, and similar goods. Uses typically include, but are not limited to: feed and grain stores, well drilling, tree service firms and nurseries.

Animal Care: Activities typically include, but are not limited to: the provision of animal care treatment, and boarding services of large and small animals. Uses typically include, but are not limited to: animal clinics, large and small animal hospitals, and kennels.

Automotive Fleet Storage: Activities typically include, but are not limited to: the storage of vehicles used regularly in business operations and not available for sale on site. Such uses typically include, but not limited to: overnight storage of service vehicles, mobile catering trucks, and taxi cabs.

Automotive Rental/Leasing: Activities typically include, but are not limited to: the rental or leasing from the premises of motor vehicles, with provision of incidental maintenance services. Uses typically include, but are not limited to, car rental agencies and leasing services. On-site storage or keeping of vehicle for rental or lease shall not occupy more than 25% of the required parking for the subject building suite, unless approved with a Conditional Use Permit.



Automotive Sales: Activities typically include but not limited to the display, retail sale, or lease new and used automobiles, minor automotive repair, automotive body work, and installation of accessories.

Automotive and Light Truck Repair-Minor: Activities include, but are not limited to: automotive and light truck repair, the retail sale of goods and services for automotive vehicles and light trucks (less than 6000 lb), and the cleaning and washing of automotive vehicles. Uses typically include, but are not limited to: brake, muffler and tire shops and automotive drive-through car washes. Heavier automobile repair such as transmission and engine repair are not included. Disabled vehicles shall be screened from public view.

Automotive and Truck Repair-Major: Activities typically include, but are not limited to: heavy automobile and truck repair such as transmission and engine repair, the painting of automotive vehicles, automotive body work, and the installation of major accessories. Disabled vehicles shall be screened from public view.

Automotive Service Station: Activities typically include, but are not limited to: the sale from the premises of goods and the provision of service normally required in the day-to-day operation of motor vehicles, including the principal sale of petroleum products, the incidental sale of tires, batteries, replacement items, and lubricating services, and the performance of minor repairs, such as tune-up, tire change and brake work.

Building Contractor's Offices and Yards: Activities typically include offices and storage of equipment, materials, and vehicles for contractors who are in the trades involving construction activities which include, but are not limited to, plumbing, painting, electrical, roofing, carpentry, and other services. Screening of outdoor storage is required as per Section IV, A.6. (Storage Area/Screening).

Building Contractor's Storage Yard: Activities include the maintenance and outdoor storage of large construction equipment such as earthmoving equipment, cranes, and outdoor storage of building materials in an unscreened manner.



Building Maintenance Services: Activities typically include, but are not limited to: maintenance and custodial services, window cleaning services, disinfecting and exterminating services, and janitorial services.

Building and Light Equipment Supplies and Sales: Activities typically include, but are not limited to: the retail sale or rental from the premises of goods and equipment, including paint, glass, hardware, fixtures, electrical supplies, roto-tillers, small trailers, lumber and hardware and may have outdoor storage where allowed.

Business Supply Retail and Services: Activities typically include, but are not limited to: retail sales, rental or repair from the premises of office equipment, office supplies and similar office goods primarily to firms and other organizations utilizing the goods rather than to individuals. They exclude the sale of materials used in construction of buildings or other structures.

Business Support Services: Activities typically include, but are not limited to: services which support the activity of firms, such as, clerical, employment, protective, or minor processing, including blueprint services, and multi-copying of pamphlets and small reports for another firm. Activities not included in this category are the printing of books and services of a personal nature.

Communication Services: Activities typically include, but are not limited to: broadcasting and other information relay services accomplished primarily through use of electronic and telephonic mechanisms. Uses typically include, but are not limited to: television and radio studios and telegraph offices.

Convenience Sales and Services: Activities typically include, but are not limited to the retail sales from the premises of frequently needed small personal convenience items and professional services which are used frequently. Uses typically include, but are not limited to: drug stores, stores selling toiletries, tobacco, and magazines, beauty and barber shops, and apparel laundering and dry cleaning agencies.

Eating and Drinking Establishments: Activities typically include, but are not limited to: the retail sale from the premises of unpackaged food or beverages generally prepared for immediate on-premises consumption. Uses typically include, but are not limited to: restaurants and bars; delicatessens; excluding fast food type services.

Entertainment: Activities typically include, but are not limited to: cultural, educational, and entertainment services within an enclosed building to assembled groups of spectators of participants, as well as activities typically performed at private and non-profit clubs and lodges. Uses typically include, but are not limited to: dance halls, theaters and meeting halls.

Extensive Impact Commercial: Activities typically include, but are not limited to: those which produce or may produce a substantial impact upon the surrounding area. Uses typically include, but are not limited to amusement parks, drive-in movie theaters, flea markets, outdoor auction sales, or swap meet activities.

Fast Food Sales: Activities shall include the retail sale from the premises of quickly prepared foods and beverages such as hot or cold sandwiches, chicken, tacos, pizza, donuts, etc. served with dispensable (paper, plastic) plates and utensils for on- or off-site consumption. Table service is generally limited to delivery of counter ordered meals and busing. Service to persons in vehicles can be a function of fast food establishments.

Financial, Insurance, and Real Estate Services: Activities typically include, but are not limited to: financial, insurance, real estate and securities brokerage services and banks.

Food and Beverage Sales: Activities include, but are not limited to the retail sale from the premises of food and beverages for off-premises consumption. Uses typically include, but are not limited to: mini-markets, liquor stores and retail bakeries; catering businesses excluding chain type grocery stores.

Funeral and Crematory Services: Activities typically include, but are not limited to services involving the care, preparation, and disposition of human dead other than in cemeteries. Uses typically include, but are not limited to: funeral homes, crematories, and mausoleums.

Hotel/Motel: Activities typically include, but are not limited to lodging services to: transient guests on a less-than-monthly basis, other than in the case of uses classified as residential uses. Uses typically include, but are not limited to: hotels, motels, boarding houses and resorts.

Heavy Equipment Sales and Rentals: Activities typically include, but are not limited to: the sale or rental from the premises of heavy construction equipment, farm equipment, trucks and aircraft together with maintenance. Uses typically include, but are not limited to: aircraft, farm equipment, heavy truck, large boat (over 25'), and heavy construction equipment dealers.

Laundry Services: Activities typically include, but are not limited to: institutional or commercial linen supply and laundry services, dry cleaning plants, rug cleaning, and diaper service laundries.

Medical/Health Care Services: Activities typically include, but are not limited to therapeutic, preventative, or correctional personal treatment by physicians, dentists, and other practitioners, as well as the provision of medical testing and analysis services. Health care uses typically include those performed by:

- Medical Clinics
- Family Planning Clinics
- In-Patient Health Care Facilities,  
including hospitals and convalescent homes.

Personal Services: Activities typically include, but are not limited to: information, instruction and similar services of a personal nature. Uses typically include, but are not limited to: computer training, driving schools, day care facilities, travel bureaus, and photography studios.



Petroleum Products Storage: Activities typically include, but are not limited to: bulk storage, sale, and distribution of gasoline, liquified petroleum gas, and other petroleum products.

Recreational Facilities: Activities typically include, but are not limited to: sports performed either indoors or outdoors which require a facility for conducting the recreational activity. Uses typically include, but are not limited to: health clubs, exercise studios or classes, swimming centers, skating rinks, bowling alleys, tennis courts, sports fields, and golf courses.

Repair Services: Activities typically include, but are not limited to repair services involving articles such as upholstery, furniture, and large electrical appliance repair services.

Scrap Operation: Activities typically include, but are not limited to the storage and sale from the premises and/or dismantling of used, recyclable, or waste materials except when such activities are part of a manufacturing operation.

#### E. CIVIC USE TYPES

Administrative Civic Services: Activities typically include, but are not limited to: management, administrative or clerical services performed by public, quasi-public, and public utility administrative offices.

Cultural: Activities typically include, but are not limited to those performed by the following institutions:

- Public and private non-profit museums and art galleries;
- Public and private non-profit libraries and observatories.

Extensive Impact Utility Facilities: Activities typically include, but are not limited to those performed by public agencies or which are strongly vested in the public interest, and which produce or may produce a substantial impact upon the surrounding area. Uses typically include, but are not limited to the following institutions and installations:



- Electric, gas, and oil transmission facilities
- Garbage or refuse disposal facilities
- Major mail-processing centers
- Radio and television transmission facilities, including but not limited to booster or relay stations
- Railroad and bus terminals
- Railroad rights-of-way, railroad yards and bus storage areas
- Public utility corporation or truck yards
- Reservoirs, water tanks, and water treatment facilities
- Sewage treatment facilities and truck lines exclusive of individual septic tanks
- Steam, fossil, or nuclear power plants
- Truck terminals operated by a public agency

Flood Control/Utility Corridor: Activities typically include flood control channels and major utility corridors, such as high power electrical transmission lines and towers. Other activities which are compatible with both the utility function and surrounding existing or future land uses may be permitted in utility corridors, as provided for by the regulations contained in Chapter 17.16 (Open Space Districts) of the City's Development Code.

Public Assembly: Activities typically include, but are not limited to those performed by, or at, the following institutions or installations:

- Parks, botanical gardens, and open space areas of a passive use character;
- Public and semi-public playgrounds and playing fields, and open space areas of an active use character;
- Public and semi-public meeting halls.

Public Safety and Utility Services: Activities typically include, but are not limited to, the maintenance and operation of the following installations:

- Communications equipment installations and exchanges, except telephone exchange and switching facilities.
- Electrical substations;
- Gas substations;
- Ambulance services;
- Police stations and fire stations;
- Post offices, but excluding major mail-processing centers; and,
- Publicly operated off-street parking lots and garages available to the general public either without charge or on a fee basis.

Religious Assembly: Activities typically include, but are not limited to those performed at churches, temples, synagogues, and other places of worship.

#### F. GENERAL USE TYPES

Accessory/Ancillary Use: The use customarily incidental and/or accessory to the principal use of the land, building site, business activity, or to a building or other structure located on the same building site as the principal use. An accessory use which is customarily subordinate in function and in area requirements to the principal use or building site may also serve an ancillary function as an integral essential, and inseparable part of the principal use, building site, structure or portion thereof.

Principal Use: A use which fulfills a primary function of an establishment, business, institution, or other entity. A principle use occupies the largest single segment of the activity functions or area requirements of a development.

#### G. RESEARCH AND DEVELOPMENT USES

The Specific Plan provides for activities of Research and Development projects by authorizing specific activities/uses typically associated with that type of operation. Those activities are listed and defined under the Professional Design and Research Office land use type. Research and Development projects are

appropriate for those subareas in which the combination of the three individual office uses may be authorized. For Research and Development projects on Haven Avenue, refer to Haven Avenue Overlay District provisions for Professional Service uses and accessory use allowances.

### III. URBAN DESIGN STANDARDS AND GUIDELINES

The establishment of Urban Design Standards and Guidelines provides the City with the ability to ensure that all development in the Industrial Area has compatible architecture and enhanced design quality. The guidelines are general and through the Design Review process are intended to encourage the individual creativity of project designers as well as provide for the needs of the individual owner and user.

#### A. Architecture

The purpose of Architecture and Design standards and guidelines, is to ensure that the built environment within the Industrial area is compatible with the nature and use of each Land Use category. The standards and guidelines are intended to result in a well designed environment which is safe and pleasing to work within. The standards and guidelines shall apply on an area wide basis and where noted within certain Land Use categories.

- A.1. The design of buildings and site plans should be compatible with surrounding land use and architecture, and should recognize the climate, the physical setting, and the best architectural traditions of Southern California. Building orientation should include considerations of wind protection of site activities.
- A.2. All exterior wall elevations of buildings and screen walls shall have architectural treatment. Articulation of elevation surfaces is encouraged through the use of openings and recesses which create texture and shadow patterns and provide variety to a building plane or surface.
- A.3. At ground level, expanses of blank building walls shall be minimized with creative use of materials, textures, color and/or building form.
- A.4. Colors, materials, and finishes shall be coordinated in all exterior elevations of the buildings to achieve a total continuity of design.



- A.5. Building materials, colors, and textures shall be compatible with those of adjacent or nearby buildings.

Metal Buildings

- A.6. The use of prefab, all metal steel for sheathing of buildings is prohibited from the Industrial Park and General Industrial categories. This is not to preclude the use of metal detail within architecturally designed buildings such as "Cortin" steel. Where used, metal buildings shall be architecturally designed to be compatible with surrounding land use and architecture.

Height and Bulk

- A.7. The height and bulk of buildings shall not unduly block views and the solar access of adjacent and other nearby buildings.
- A.8. Maximum building or structure height shall not exceed four stories or 75' whichever is greater, unless approved as a Conditional Use Permit.

B. Site Planning

The purpose of Site Planning standards and guidelines is to ensure for a functional, safe and visually pleasing environment for those who may pass through or work in the area. The following standards and guidelines shall apply throughout the Industrial area.

- B.1. Building placement shall be designed in such a way to create opportunities for plazas or other landscaped open spaces and encourage defined and spaciouly enclosed open space on the same site or adjoining sites. Building orientation should include consideration of wind protection for site activities. The City Planner may require a detailed wind analysis of designs in question of adequacy (in wind protection) as a part of development application.
- B.2. The access and circulation of a development should be designed to provide a safe and efficient system, both on and off the site. Points of access shall be designed in conformance with the City access regulations. The circulation system shall be designed to reduce conflicts between vehicular and pedestrian traffic, minimize impacts on adjacent properties, combine circulation and access areas

where possible, and provide adequate maneuvering areas. Points of access shall not conflict with other planned or existing access points.

B.3. Parking areas should not be the dominant element in the overall design of a project and should be designed to minimize visual disruption. Parking areas should be screened from streets through combinations of mounding, landscaping, low profile walls and grade separations. The design of parking areas should also minimize vehicle noise, lights and glare, and ambient air temperature. This can be accomplished through the use of sound walls, general location, use of well designed lights, and landscaping throughout the parking lot.

B.4. Landscaping and open spaces should be designed as an integral part of the overall site plan design, and be designed to create visual interest and variety to the streetscape, enhance building architecture, screen utilities and buffer views of automobiles, pavement and service areas, and to define and to distinguish the pedestrian environment from vehicular spaces. Consideration should be given to wind protection of building and site activities by the buffer planting of wind tolerant trees and shrubs.

#### Pedestrian Orientation

B.5. Colonnades or loggias and other covered walkways or structures that provide shade to pedestrian spaces shall be utilized whenever possible. Employee outdoor eating areas shall be provided as an integral part of site designs.

B.6. At street level, the use of building material, and building details, that relate to human activity shall be required where appropriate.

B.7. Convenient pedestrian circulation shall be provided throughout all projects to connect parking areas and public transit facilities with buildings and pedestrian open spaces.

B.8. Open spaces shall be integrated with pedestrian walks and defined by landscaping and other elements to create a sense of place.

B.9. Where possible, open spaces shall be accessible to the public.

Street Furniture

B.10. Benches, light standards, kiosks, drinking fountains, trash receptacles, and other street furniture in on-site open spaces shall be designed in a coordinated fashion to enhance the appearance and function of the site and open space.

B.11. Pedestrian areas shall be highly visible and well lit.

#### IV. DEVELOPMENT STANDARDS

The Development Standards within the Industrial area address seven factors which include:

- A. - General Provisions
- B. - Master Plan Requirements
- C. - Minimum Parcel Size
- D. - Setback Requirements
- E. - Landscape Requirements
- F. - Parking & Loading Requirements
- G. - Rail Service Standards
- H. - Performance Standards
- I. - Interim Uses

Table III-3 summarizes the application of development standards which may apply on a subarea basis, including minimum parcel size, landscape requirements, performance requirements, and rail service standards. The setback requirements are determined in accordance with the street classification.

TABLE III-3 DEVELOPMENT STANDARDS SUMMARY

STANDARDS	HO	1	2	3	4	5	6*	7*	8	9	10	11	12	13	14	15	16	17
Minimum Parcel Size (Acres)	2	1/2	1	1/2	1/2	1	2/1	2/1	1/2	5/2	1	1/2	2/1	1/2	1/2	5	1	1/2
Minimum % % of Landscape Area (% of Net Lot Area)	25/30	12	10	12	12	7	15	15	12	10/5	10	12	15	12	12	10/5	15	15
Performance Schedule	A	B	B	B	B	B	A	A	B	B/C	B	B	A	B	B	C	A	A
Rail Service**		X	X	X		X	X		X	X	X				X	X		

\*May be subject to Haven Avenue Overlay District regulations.

\*\*Properties adjacent to rail spurs subject to rail service development standards



A. General Provisions

The purpose of standards within General Provisions is to establish minimum standards regulating specific details in the development of any project within the Industrial Area. The standards set forth in this section shall apply either within the entire Industrial area or where stated by the Land Use category.

Condition of Uses

- A.1. All business and manufacturing operations shall be conducted within an enclosed building unless specifically permitted and adequately screened from public view pursuant to this Section.

Signs

- A.2 Signs shall be used for the purpose of identification and direction. The design of permitted signs shall be architecturally integrated with the building design. Submittal of preliminary design concepts during the development review process is encouraged. The design of signs including location, materials, colors, copy, size, and construction details are all set forth in the City Sign Ordinance (Chapter 14 of the Municipal Code).

- A.3. A coordinated Uniform Sign Program may be required for any development, including wall and monument signs, to encourage design compatibility.

Lighting

- A.4. Lighting shall be used for the purpose of providing illumination for the security and safety of on-site areas such as parking, loading, shipping and receiving, pathways, and working areas. The following standards shall apply in all areas.
- a. The design of light fixtures and its structural support shall be architecturally compatible with the surrounding buildings. Free standing light standards shall not exceed 25' or the height of the shortest on-site building.
  - b. Security lighting fixtures are not to project above the fascia or roof line of the building.

- c. All lighting is to be shielded to confine light spread within the site boundaries. Particular concern shall be for lighting adjacent to residential areas.

#### Equipment Screening

A.5. The purpose of Equipment Screening standards shall be to allow for the use of equipment while preserving the architectural character and integrity of the surrounding environment. Equipment is deemed to include exterior mechanical or electrical equipment, such as A/C units, fans, ductwork, cyclone blowers, cranes, storage tanks, and satellite dish antennas. The following standards shall apply according to Land Use category:

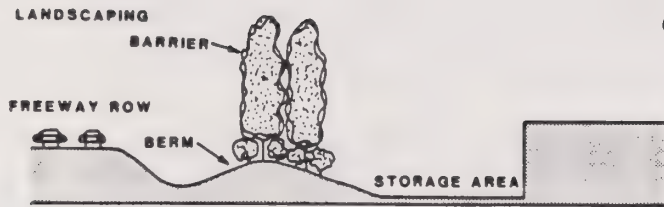
- a. All roof, wall and ground mounted equipment shall be screened from all sides within all land use categories except Minimum Impact Heavy Industrial and Heavy Industrial.
- b. Wherever possible, all roof, wall and ground mounted equipment shall be screened on all sides within the Minimum Impact Heavy Industrial and heavy industrial categories.
- c. All screening shall be architecturally integrated with the building design and where possible a roof parapet wall shall be used to screen roof or wall mounted equipment. Where roof-mounted mechanical equipment and/or ductwork projects vertically more than one and one-half (1-1/2) feet above the roof or roof parapet it shall be screened by an architecturally designed enclosure which exhibits a permanent nature with the building design and is detailed consistent with building. Where roof-mounted mechanical equipment and/or ductwork projects one and one-half (1-1/2) feet or less above the roof or roof parapet it shall be painted consistent with the color scheme of the building in all cases.

#### Storage Area/Screening

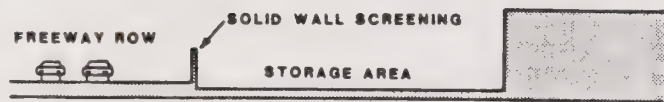
A.6. The purpose of storage area regulations is to allow for on-site storage which is screened from view from the public right-of-way and from land used in common (public view) and is architecturally compatible with the surrounding environment. The following standards shall apply according to Land Use category:

a. Industrial Park category - No outdoor storage shall be permitted except for fleet vehicles and light trucks (not exceeding 6,000 lbs.). Outdoor storage tanks may be permitted at a height not to exceed 8' from highest finish grade when screened from public view by concrete, masonry or other similar materials.

b. General Industrial or General Industrial/Rail Served categories - All materials, supplies, equipment and operating trucks shall be stored within an enclosed building or a screened area from public view.



c. Minimum Impact Heavy Industrial and Heavy Industrial categories - All materials, supplies, equipment and operating trucks shall be stored within an enclosed building or a storage area. Such storage areas within 120' of a street frontage shall be screened.



d. Within 600' of the Devore Freeway right-of-way all outdoor storage shall be screened from public view from the Devore Freeway. Screening the Devore Freeway may consider the use or combination of block or masonry walls, berming, dense landscaping, or building mass.

e. Within all Land Use categories except Heavy Industrial, all storage area screening shall be architecturally integrated with surrounding buildings by the use of concrete, masonry or other similar materials not to exceed a height of 8' from highest finish grade.

f. Within the Heavy Industrial category storage area screening may include masonry or concrete, wood, metal, or chain link with wood slats.

g. Storage of materials or equipment shall not exceed screen height within 100' of street fronting screens.





Security Fences  
and Walls

- h. The City Planner may waive screening requirements where future building expansion would screen abutting storage area.

A.7. The purpose of Security Fencing and Wall Standards is to provide for the safe environment for businesses within the Industrial area.

- a. Site planning, including building configuration and placement, is encouraged to create defined areas that may be adequately secured.
- b. Any wall or fence along a street frontage over three feet (3') in height is subject to the streetscape setback requirements established in Section D (Setback Requirements) of this chapter.
- c. Within all Land Use categories except Heavy Industrial, all fencing or walls shall be wrought iron, concrete, masonry or other similar materials not to exceed a height of 8' from highest finish grade. The use of barbed wire or similar materials is prohibited from these Land Use categories. Chain link may be used in areas not visible in front setback area.
- d. Within the Heavy Industrial category security fencing may include wrought iron, masonry or concrete, wood, metal, or chain link with wood slats. Barbed wire may be permitted atop fencing.
- e. Security gates are subject to review and approval by the Fire and Sheriff Department to ensure adequate emergency access.

Utilities

A.8. The purpose of Utility Service standards is for the efficient distribution of utilities designed to be compatible with the surrounding environment. The following requirements shall apply within the Industrial Area.

- a. All existing and new utilities 12KV and less within the project and along adjacent major arterials shall be installed underground.



- b. All ground mounted utility appurtenants such as transformers shall be located out of public view, preferably in the side yard, and adequately screened through the use or combination of concrete or masonry walls, berming, and landscape materials.

#### Maintenance

A.9. Property owners are responsible for the maintenance of all buildings, structures, yards, landscaping, signs, parking areas and other improvements in a manner which does not detract from the appearance of the surrounding area. The following conditions are prohibited:

- a. Delapidated, deteriorating, or unrepaired structures, such as: roofs, walls, windows, doors, fences, signs or similar items.
- b. Scrap lumber, junk, trash or debris.
- c. Abandoned, discarded or unused objects or equipment, such as vehicles, machine parts, pallets, steel drums, boxes, scrap metal, waste materials, or similar items.
- d. Stagnant water.
- e. Any device, decoration, design, structure or vegetation which is unsightly by reason of its height, condition, or its inappropriate location.
- f. All landscaped areas shall be kept free from weeds and debris and maintained in a healthy, growing condition, and shall receive regular pruning, fertilizing, mowing and trimming. Any damaged, dead, diseased or decaying plant material shall be replaced within thirty (30) days from the date of damage.

#### B. Master Plan Requirements

The purpose of Master Plan requirements is to provide for integrated development throughout the Industrial Area at the earliest possible time in the review process. Master planning of defined areas will avoid development in a manner which would prevent or preclude future development of surrounding land on the same or

adjoining parcels in the best way possible. It is not the intention of the master planning process to "cast future development patterns in stone". The intent is to recognize and solve problems before they occur and take advantage of opportunities while they exist.

- B.1. A conceptual Master Plan shall be required for City Planner and/or Planning Commission review in conjunction with development proposals whenever necessary to assure integrated development, enhance harmonious and orderly development, mitigate site constraints on the same or adjoining parcels, and maximize land potential.
- B.2. The area of Master Plans shall not be confined by individual lot lines, but determined by logical planning boundaries and site conditions as required by the City Planner.
- B.3. At a minimum, Master Plans shall address harmonious site plan relationships and the potential for shared access and reciprocal parking by illustrating conceptual building pad locations, circulation and parking lot configurations, and conceptual grading and drainage.
- B.4. Future development may be allowed to vary from the Master Plan, provided the proposed project is compatible with the intent of the Master Plan and satisfies all development requirements within the Land Use category.

C. Minimum Parcel Size

The purpose of a Minimum Parcel Size is to establish minimum lot size standards which will provide for the development and use of land within the industrial area according to the Land Use category and development standards contained within this Plan. The Minimum Parcel Size is determined by a Land Use category with the exception of portions of subareas 6, 7, and 12.

Lot Size

C.1. The Minimum Parcel Size shall be set forth as follows:

Land Use Category	Minimum Parcel Size (Acres)
a. Industrial Park	1 or 2*
b. General Industrial	$\frac{1}{2}$
c. General Industrial Rail-Served	1
d. Minimum Impact Heavy Industrial	5 or 2**
e. Heavy Industrial	5

\* 2 acres minimum parcel size in subareas 6, 7, and 12 on parcels adjacent to 4th Street, Haven Avenue, and Foothill Boulevard. Also, special standards for Master Planning and urban centers apply within the Haven Avenue Overlay District.

\*\* 2 acres minimum parcel size in subarea 9 between Arrow Highway and 1000' south.

Lot Width

C.2. Minimum Parcel Width shall be not less than 100', and not less than 300' on parcels abutting Special Boulevards shown on Figure III-2.

Condominium Lots

C.3. Condominium lots and lots within an approved Master Planned Development are exempt from required minimum parcel size and dimensions requirements.

C.4. All lots of record are allowed to develop according to development standards as set forth in this Plan.

D. Setback Requirements

The purpose of Setback Requirements is to provide open space for building separation, landscape treatment, and the encouragement of architectural and site planning design diversity. The following standards shall apply in all areas of the Plan:

## Definition

D.1. Streetscape setbacks include the minimum building setback, minimum parking setback and the average depth of landscaping along public and private street frontages.

D.2. All streetscape setbacks are determined from ultimate face of curb.

## Streetscape Setback

D.3. Streetscape setback requirements shall be established according to street classification as shown in Table III-4 and as shown on Figure III-3 streetscape setback schedule, except as provided for in sections D.5 through D.10 below.

TABLE III-4 STREETSCAPE SETBACK REQUIREMENTS  
(Determined from Ultimate Face of Curb)

	Average Depth of Landscape*	Building Setback**	Parking Setback***
Major Arterial and Special Boulevard	45'	45'	25'
Secondary	35'	35'	20'
Local	25'	25'	15'

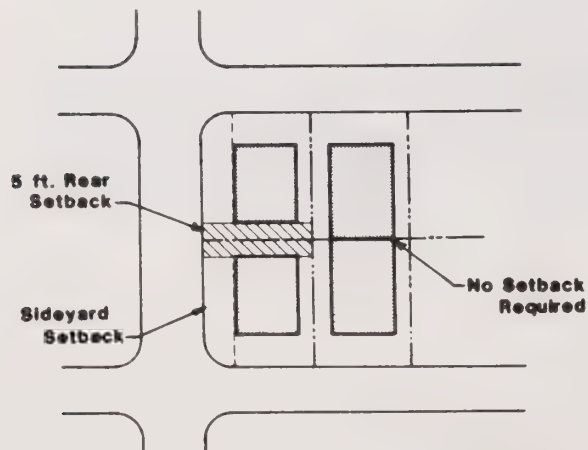
\* The average depth shall be uninterrupted from the face of curb, except for sidewalks, Pedestrian hardscape, plazas and courtyards, and monument signs.

\*\* Street frontage walls and fences over three feet (3') in height are subject to building setbacks.

\*\*\* Average depth of landscaping must still be provided, see D.5.

## Building Setback

D.4. Building setbacks shall be as follows (except when modified as set forth in items D.5 through D.10).

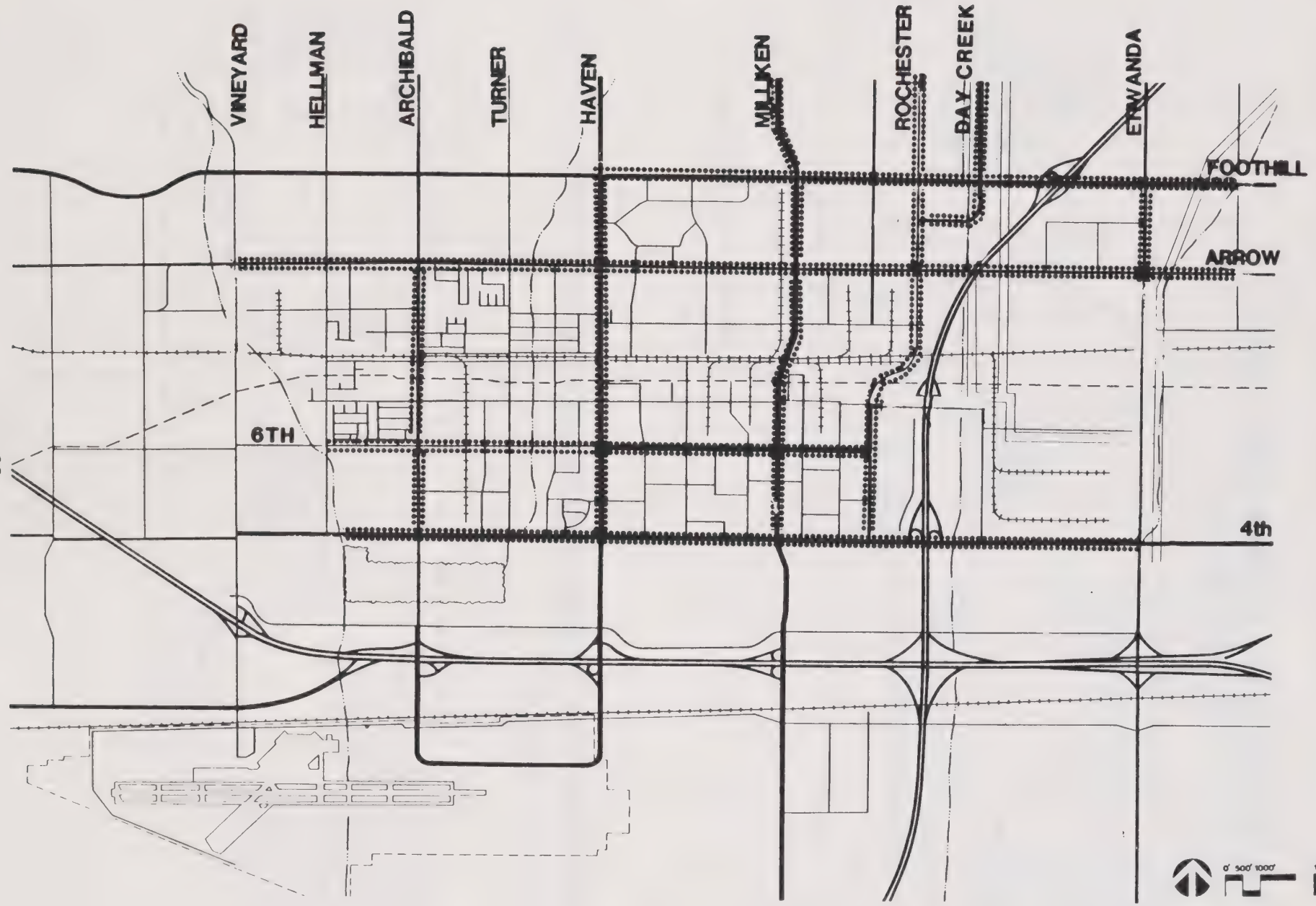


a. Front: As shown per street classification in Table III-4.

b. Interior Rear: None, except when rear lot area abuts a side street the setback shall be 5' minimum.



III-33

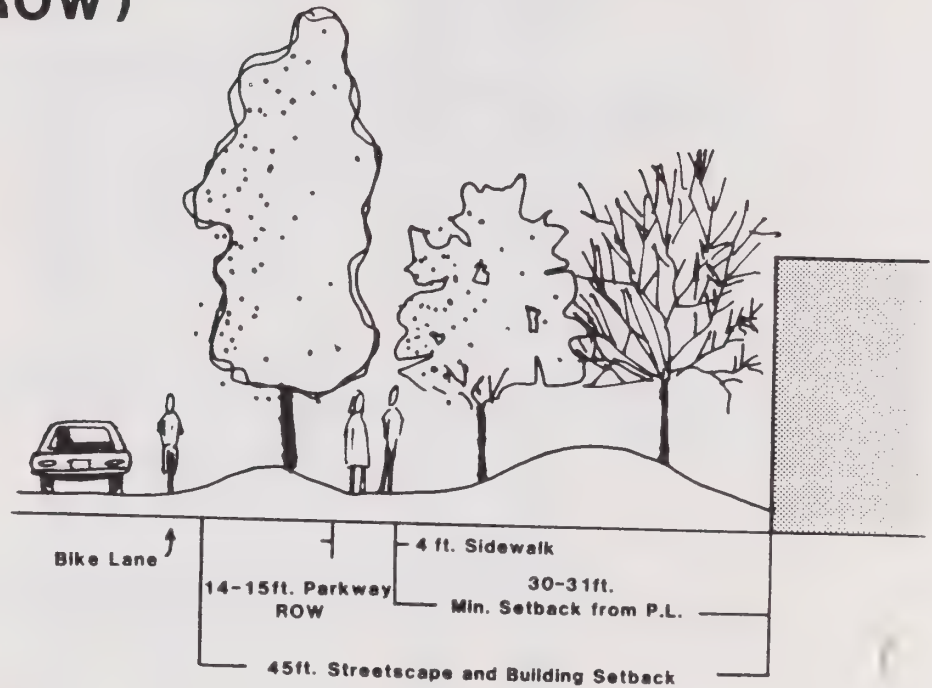


SPECIAL BOULEVARD CORRIDORS  
FIGURE III - 2

# FIG.III-3 STREETSCAPE SETBACK SCHEDULE

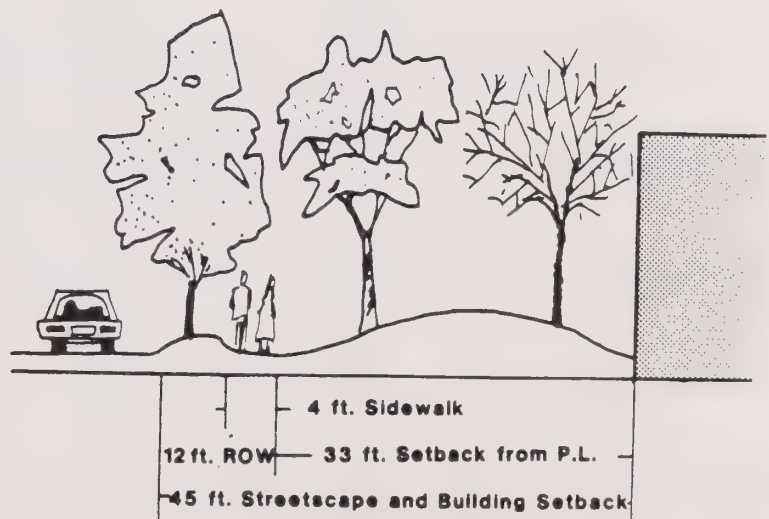
## MAJOR ARTERIAL/SPECIAL BLVD. (100\*and 120 ft. ROW)

- o Day Creek Blvd.
- o Foothill
- o Haven
- o Milliken
- o 4th
- o 6th between Haven & Devore
- o Archibald\*
- o Arrow\*
- o New Rochester\*
- o Etiwanda\*



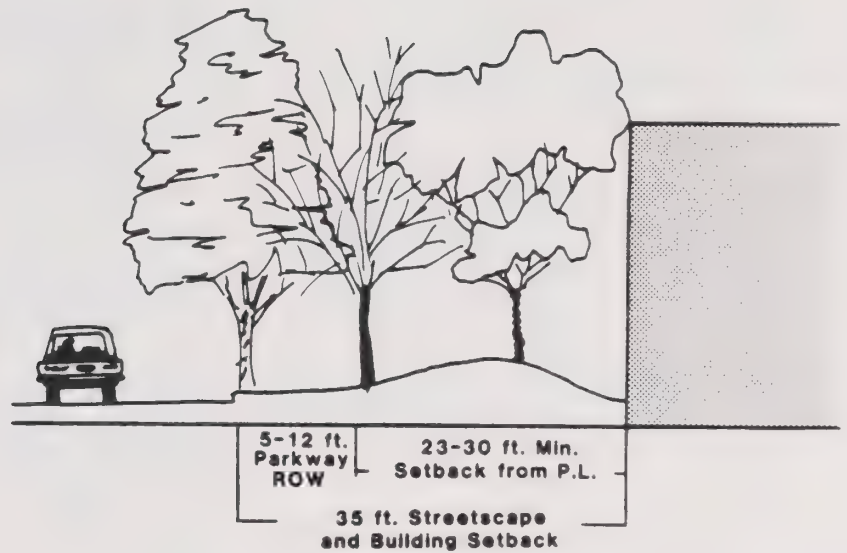
## SECONDARY/SPECIAL BLVD. (88 ft. ROW)

- o 6th (West of Haven)



## SECONDARY (88 ft. ROW)

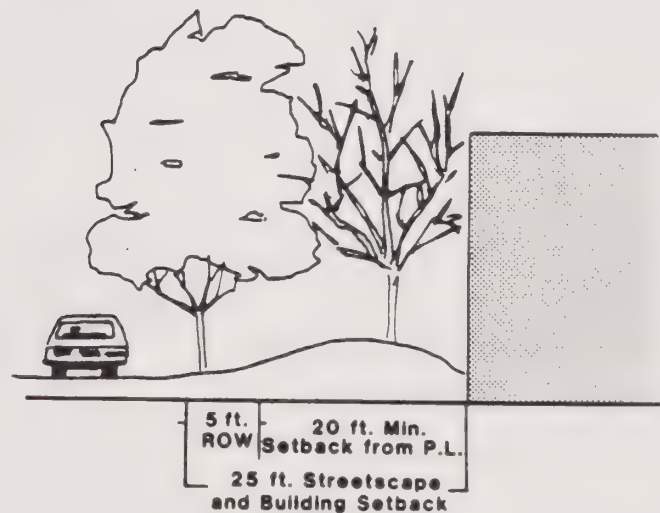
- o Vineyard
- o Hellman
- o Cleveland
- o 6th<sup>1</sup> East of Devore Freeway
- o Turner
- o Jersey



<sup>1</sup>See Subarea 15, Special Consideration

## LOCAL INDUSTRIAL (66 ft. ROW)

- o 8th
- o 9th
- o Center
- o Santa Anita
- o (Old) Rochester
- o 7th West of Haven



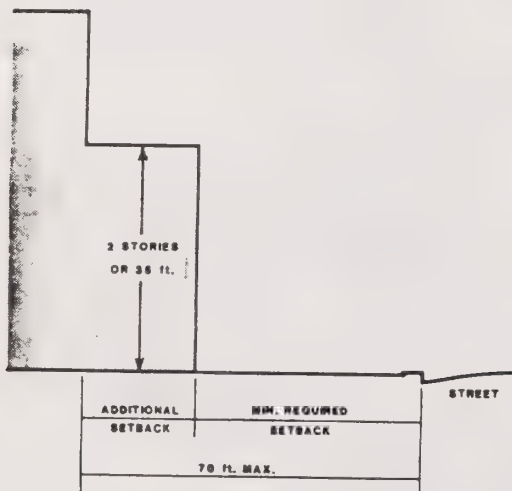
- c. Rear Abutting Street: As shown per street classification.
- d. Interior Side: 5' minimum. Minimum side yard setback may be waived or modified by the Planning Commission subject to Master Plan application and approval.
- e. Street Side: As shown per street classification in Table III-4.

#### Parking Setbacks

- D.5. Parking Setbacks as setforth in Table III-4 may be included within the streetscape setback provided that the average landscape setback is maintained according to the street classification.
- D.6. Properties adjacent to residential areas shall have a minimum of 45' building setback from property line on interior lot line and 45' from ultimate face of curbs on abutting streets.
- D.7. Areas of other industrial land use categories abutting an Industrial Park category shall have a minimum of 45' building setback to the abutting property line on interior lot line and 45' from ultimate face of curb on abutting streets.

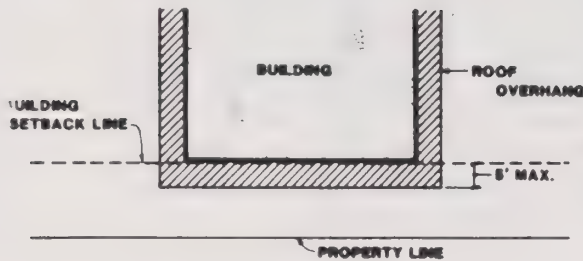
#### Building Height

- D.8. For buildings exceeding two (2) stories or 35' as measured from highest finish grade, whichever is more restrictive, an additional 1' setback measured from ultimate face of curb shall be required for each additional 1' height. This setback need not exceed 70'.





## Projections



D.9. Eaves, roof projections, awnings, stair landings, and similar architectural features may project into the building setback a maximum distance of 5', provided such appendages are supported only at, or behind, the building setback line.

D.10. Parcels less than 225' deep from ultimate curb face on Special Boulevards need not provide an average depth of landscaping or building setback greater than 25' or 20% of the depth of property (as measured from ultimate face of curb), whichever is greater, except on Haven Avenue or within Master Planned Developments. Buildings over two (2) stories or 35' are still subject to Section D.8 above.

## E. Landscape Requirements

The purpose of landscaping is to provide the Industrial area greater aesthetic quality, a unified design theme, and to promote energy conservation. The "primary landscape design concepts" listed in the Urban Design Concept Section of Part II shall apply to all landscape plans under these requirements. The following requirements shall be applied throughout the Industrial area.

### Minimum Landscape Coverage

E.1. The minimum landscape coverage of net lot area (net lot area includes the area under property ownership excluding all right-of-way dedications and private streets) is as follows:

Subarea	H0**	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17
Landscape Coverage	25/30	12	10	12	12	7	15	15	12	10*/5	10	12	15	12	12	10*/5	15	15

\* Within Subareas 9 and 15, the minimum landscape coverage shall be 10% of net lot area between the area extending from Arrow Highway, 1000' south. All remaining area shall have a 5% minimum landscaped coverage.

\*\* Haven Avenue Overlay District

E.2. The landscaped coverage requirement may be modified by the Planning Commission when it is determined that the project is designed to the highest aesthetic quality compatible with the land use category and consistent with the surrounding land use. (i.e., within a Master Planned project area, variation of landscape coverage requirement may be allowed.)

#### Berming

E.3. Bermed landscaping shall be incorporated wherever possible within the landscape setback and landscape areas surrounding parking and loading areas.

E.4. On Special Boulevards, all parking areas shall be screened with berms an average height of three feet (max. slope not to exceed 3 1/2:1).

E.5. The design of the berms shall be undulating to provide interest and visual access to buildings.

#### Trees

E.6. All required trees will be a minimum of 15 gallon size, except within the Industrial Park category 30% of the required trees shall be 24" box size or larger.

E.7. Within parking lots, trees will be planted at a rate of 1 tree for every 3 parking stalls provided in the planters.

E.8. Trees shall be planted in areas of public view adjacent to structures at a rate of 1 tree per 30 linear feet of building dimensions, particularly to interrupt expansive horizontal and vertical surfaces. Tree clusters may be used to satisfy specific design objectives.

E.9. Along property boundaries, trees will be planted at a rate of 1 tree per 30 linear feet of interior property line. Tree clusters may be used to satisfy specific design objectives.

#### Expansion Areas

E.10. Undeveloped areas proposed for future expansion shall be kept in a weed free condition and with appropriate ground cover may be required. Graded pad sites may require temporary seeding with appropriate ground cover and served by an automated irrigation system for erosion control and to mitigate visual impact.

## Irrigation System

E.11. All landscaped areas shall be served by an automatic underground irrigation system.

## Water Conservation

E.12. A combination of water conserving landscape and irrigation techniques are required such as, but not limited to, drought tolerant plant species and hardscape (non-irrigated) surfaces, and special irrigation systems such as drip emitters, low volume stream rotors, deep watering of trees and shrubs, tensiometers to measure soil moisture, and automatic timers.

E.13. Property owners will be responsible for the development and maintenance of landscaping their on-site landscaped area and for the contiguous planted right-of-way. Any damage to the landscaping and irrigation systems shall be planted or replaced within thirty (30) days from date of damage.

## Screening

E.14. The use or combination of berming, landscape materials, low level wall, and building mass shall be used to screen parking and loading areas, and refuse collection areas from the public view.




E.15. The following is a list of acceptable landscaping materials for low level screening:

BOTANICAL NAME	COMMON NAME	Size at 3 Years Height x Width	Minimum Spacing
Buxus japonicum	Japanese Boxwood	5' x 4'	3.0' o.c.
Carisa grandiflora	Natal Plum	5' x 4'	3.5' o.c.
Coprosma baueri	Copromsa	6' x 5'	3.5' o.c.
Hakea suaveolens	Sweet Hakea	6' x 5'	4.0' o.c.
Hibiscus rosa-sinensis	Chinese Hibiscus	5' x 5'	5.0' o.c.
Ligustrum texanum	Japanese Privet	6' x 4'	3.0' o.c.
Myoporum laetum	Myoporum	6' x 6'	5.0' o.c.
Nerium oleander	Oleander	6' x 6'	4.0' o.c.
Pittosporum tobira	Tobira	3' x 4'	4.0' o.c.
Viburnum japonicum	Viburnum	6' x 5'	4.0' o.c.
Xylosma congestum	Xylosma	5' x 5'	4.0' o.c.





# TABLE III-5

## LANDSCAPING MATERIALS CONCEPT

LOCATION	CHARACTER	TREE TYPES	PLANTING PROVISIONS AND TREATMENT	REPRESENTATIVE SPECIES
1. Roadways designated for special boulevard treatment.	Trees along primary circulation routes play a major role in defining the City's image, especially at gateways to the City. Primary corridors separate the industrial area from residential areas. In some cases trees can define edges and provide buffers.	Columnar evergreens, or deciduous trees, 60' - 100' tall. 	Curbside and median planting strips 5' - 14' wide. Trees planted 20' - 30' on center (10' less than the mature diameter" of the trees) on the average either formally or in less structured linear groves.  *Mature diameter is defined as the diameter after 20 years.	<u>Eucalyptus robusta</u> , Swamp Mahogany <u>E. sideroxylon rosea</u> , Red Ironbark <u>E. camaldulensis</u> , Red Gum
2. Major arterials and, secondary streets.	Trees along secondary circulation routes and primary routes in the residential areas can provide a transition to a more human scale and to a more structured community form. Canopies can provide both a definition of scale and shade.	Columnar to round-headed, fan-shaped deciduous or evergreen trees, 50' - 80' tall. 	Curbside and median planting 5' - 14' wide. Trees planted 20' - 50' on center (10' less than the mature diameter of the trees) on the average.	<u>Platanus acerifolia</u> , London Plane <u>P. racemosa</u> , California Sycamore <u>Fraxinus holotricha</u> , "Moraine" Ash <u>Celtis sinensis</u> , Chinese Hackberry <u>Acacia melanoxylon</u> , Black Acacia <u>Ginkgo biloba</u> , Maidenhair Tree
3. Collector streets, local streets and setbacks on special boulevards in industrial areas.	Industrial areas are less formal than commercial or residential. The role of trees is to give the expansive industrial area a sense of scale and to provide shade.	Columnar to round-headed, fan-shaped deciduous or evergreen trees, 40' - 70' tall. 	Curbside planting 5' - 10' wide, trees planted 20' - 25' on center (10' less than the mature diameter of the trees) on the average. Setback planting 13' wide, trees planted informally 20' on center.	<u>Platanus acerifolia</u> , London Plane <u>P. racemosa</u> , California Sycamore <u>Fraxinus holotricha</u> , "Moraine" Ash <u>Celtis sinensis</u> , Chinese Hackberry <u>Acacia melanoxylon</u> , Black Acacia <u>Ginkgo biloba</u> , Maidenhair Tree <u>Ceratonia siliqua</u> , Carob <u>Cinnamomum camphora</u> , Camphor <u>Liquid amber styraciflua</u> , American Sweet Gum



# LANDSCAPING MATERIALS CONCEPT

LOCATION	CHARACTER	TREE TYPES	PLANTING PROVISIONS AND TREATMENT	REPRESENTATIVE SPECIES
4. Streets at commercial centers and bus stops.	<p>When a commercial center occurs along a residential or commercial street or major arterial, trees can indicate the change in character. Broad canopy trees can reflect the shifts from linear movement to activity center and the change in scale from automobile to pedestrian.</p> <p>A tree's ability to provide shade and to resist heat is essential for its use as a parking lot tree.</p> <p>Trees used to designate the entry to a parking lot should be different from the trees lining the street.</p>	<p>Round-headed, deciduous and evergreen trees, 35' - 50' tall in formal or informal clusters.</p> 	<p>Planters set into sidewalk paving, variable spacing.</p>	<p><u>Celtis sinensis</u>, Chinese Hackberry  <u>Pistacia chinensis</u>, Chinese pistache  <u>Platanus acerifolia</u>, London Plane Tree  <u>Schinus terebinthifolius</u>, Brazilian Pepper  <u>Cinnamomum camphora</u>, Camphor  <u>Magnolia grandiflora</u>, Southern Magnolia</p>
5. Parking lots of commercial centers.	<p>A tree's ability to provide shade and to resist heat is essential for its use as a parking lot tree.</p> <p>Trees used to designate the entry to a parking lot should be different from the trees lining the street.</p>	<p>Round-headed, spreading evergreen trees, 35' - 50' tall.</p> <p>Trees at entries may be deciduous and/or columnar.</p> 	<p>Strips between rows of parking stalls; planters should extend into a parking stall occasionally to break up the straight rows. Trees planted a distance of 10' less than the tree's diameter at maturity on center.</p> <p>A sufficient number of trees shall be planted such that when they are matured they will shade 50 percent of the parking area at solar noon on August 21 (1 p.m. Daylight Savings Time). This entails, at a minimum, planting trees at the required spacing in all strips between parking stalls.</p>	<p><u>Prunus</u> Species Flowering Cherries  <u>Ceratonia siliqua</u>, Carob  <u>Cinnamomum camphora</u>, Camphor  <u>Ficus rubiginosa</u>, Rusty Leaf Fig  <u>Schinus terebinthifolius</u>, Brazilian Pepper  <u>Ulmus parvifolia</u>, Chinese Elm</p>

E.16. In order to achieve a uniform landscape theme, the areas within the street medians, park strips, and streetscape setbacks shall have an established landscape materials pallet consistent with the City's landscaping theme. Landscaping materials should be selected for their longevity, drought tolerance, low maintenance, and heat and wind tolerance in addition to their aesthetic beauty and functionality. Table III-5 describes the landscaping materials and characteristics per street classification.

F. Parking & Loading Requirements

In order to prevent traffic congestion, promote business and enhance public safety; off street parking and loading facilities shall be provided as set forth herein. The facilities required by this section for parking and maneuvering of motor vehicles are assumed to be the minimum standard necessary for such use. The following shall apply for the Industrial area:

F.1. Parking regulations within the Industrial area shall be those set forth in the Development Code, Chapter 17.12 (Parking Regulations), except for any special standards established herein.

F.2. Required parking shall be located on the same site with the main use of the building, or on premises contiguous thereto, or in a location in accordance with an approved development plan.

Parking Spaces Required

F.3. The total parking space count shall be determined by the summation of individual use parking demands based upon the following rates:

a. Warehousing or building for storage: 1 space per 1,000 square feet for the first 20,000 square feet; 1 space per 2,000 square feet for the second 20,000 square feet; 1 space per 4,000 square feet for all space in excess of the first 40,000 square feet.

b. Industrial/Manufacturing: 1 space per 500 square feet.

- c. Research and Development: 1 space per 350 square feet (research services only ).
- d. Office and Administration: 1 space per 250 square feet.
- e. Multi-use tenant buildings where office use does not exceed 35% of building area: 1 space per 400 square feet.
- f. Following interior building areas can be deducted from the overall parking requirements: electrical/mechanical rooms, elevator shafts, stairwells, and multi-story lobbies.

#### Bicycle and Other Two Wheel Vehicular Facilities

F.4. Bicycle storage facilities shall be provided within all development and relate to planned and existing bicycle routes.

F.5. Required on-site parking may be reduced at a rate of one automobile parking space per 4 spaces of bicycle or other two wheel vehicular parking up to three automobile parking spaces or 5% of total required on-site parking, whichever is less.

#### Compact Spaces

F.6. 20% to 35% of all required parking stalls shall be devoted to compact car use. Minimum stall dimension shall be 8' in width and 16' in length and marked for compact cars.

F.7. All Parking areas shall be screened from public view through the use of berms, landscaping material and low walls.

#### Loading Facilities

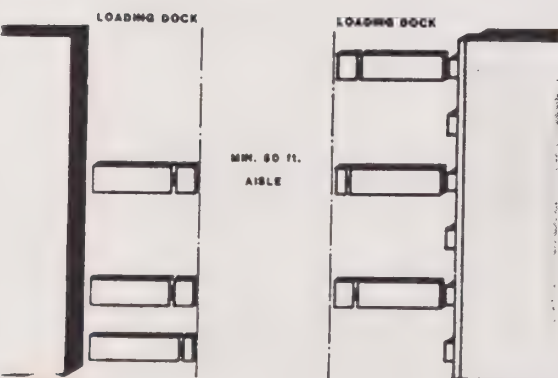
F.8. All loading facilities and maneuvering areas must be on site with the use.

F.9. All loading facilities shall be permitted only in the rear and interior side yard areas except within the Heavy Industrial category and rail served buildings.

F.10. Aisle width to loading docks shall be a minimum of 50' width plus additional width for truck parking (typically 40 to 50 feet).

F.11. Loading docks shall be set back a minimum of 70' from street property line.

F.12. Parking stalls for trailers shall be 50' x 14' and provided at ratio of 1 stall per truck loading dock door.





F.13. Loading facilities shall be adequately screened from the public view except within the Heavy Industrial category and Rail Served buildings.

F.14. Minimum aisle width adjacent to loading areas, without dock high doors, shall be 16' one way and 28' for two way.

G. Rail Service Standards

The purpose of Rail Service Standards is to provide for the use of Rail Service to those properties which adjoin existing or proposed lead or spur lines as indicated on the subarea maps. Uses with dependency on Rail Service shall be encouraged to locate on properties (except those in Subarea 6) which adjoin such lines. Those uses which are not dependent on rail service are discouraged from locating on properties adjacent to lead or spur lines. The following standards are required for all developments which adjoin lead and spur railroad lines.

G.1. Rail service standards, as set forth in Appendix D are suggested guidelines for developing rail access. The railroad and the PUC may consider modification of track standards, therefore the project designer should consult with the railroad at the time of project design.

G.2. Lot division and building layouts for properties which adjoin existing or proposed lead and spur lines shall be done in a manner to ensure full potential of future rail access and use and should not preclude rail access to other properties adjacent to such rail lines. Subdivision which could reduce a property's ability to accommodate potential rail served developments may not be authorized.

G.3. Easements shall be provided for lead/spur lines as indicated on the subarea maps in conjunction with development proposals or lot subdivisions.

G.4. Building design shall include rail service features to ensure the potential use of available spur lines. Finish floor elevations and dock height door (per Appendix D) or "kick out" wall panels shall be provided in all properties abutting rail lines.



- G.5. Within portions of subareas 6 and 11, rail access is permitted under condition that adequate screening is provided for non-rail service areas. Refer to Part IV Subareas 6 and 11 of this Plan for further detail.

H. Interim Use Standards

The purpose of Interim Use Standards is to establish minimum standards for setbacks, landscaping, screening, and parking which meet the intent of the Plan. Unless specifically modified through the Conditional Use Permit, all other development standards of the City of Rancho Cucamonga shall apply, including, but not limited to, grading and drainage, and street improvements. The following standards shall apply in all areas of the Plan:

- H.1. The minimum streetscape and parking setback requirement shall be contiguous with the ultimate right-of-way line, but in no case less than ten (10) feet.
- H.2. There shall be no minimum landscape coverage requirements, except that which is necessary for screening purposes as determined by the City Planner.
- H.3. All parking and storage areas shall be paved with slag, crushed aggregate, asphaltic concrete or concrete. The location, number and design of said parking shall be in accordance with the Plan.
- H.4. All parking and storage areas, and other interim uses which require screening as determined by the City Planner, shall be screened from public view through a combination of landscaping and fencing. Fencing may include a 6' chain link fence with slats, masonry or concrete, wood, or decorative metal. Screening must be maintained in good condition at all times.
- H.5. Landscaping required for screening purposes shall include 15-gallon trees and 5-gallon shrubs to provide a dense landscape buffer to afford maximum screening from the public view, satisfactory to the City Planner.

## I. Performance Standards

The purpose of Performance Standards is to establish Industrial Use Standard characteristics which allow the uses to operate consistent with the overall characteristics of the Land Use category. This section bases its determination on land use characteristics of each category. The intent of the Performance Standards is to provide for a healthy, safe, and pleasing environment in keeping with the nature and level of surrounding Industrial activity. The Performance Standards contained with Table III-6, are applied on a subarea basis as follows:

Class A - 6, 7, 12, 16, 17

Class B - 1, 2, 3, 4, 5, 8, 10, 11, 13, 14

Class C - 9, 15

CLASS A Performance Standards is the most restrictive of the Performance Standards. It is utilized for the Industrial Park Category.

CLASS B Performance Standards are employed for the General Industrial Categories. These standards are intended to provide for the broadest range of Industrial activity while assuring for a basic level environmental protection.

CLASS C Performance Standards provide for a level of industrial activity associated with the Heavy Industrial use while providing safeguards.

## TABLE III-6 INDUSTRIAL PERFORMANCE STANDARDS

### CLASS A

CLASS A. It is the intent of the standards of this section to ensure a high quality working environment and available sites for industrial and business firms whose functional and economic needs require protection from the adverse affects of noise, odors, vibration, glare or high-intensity illumination, and other nuisances.

**Noise:** The maximum allowable exterior noise level of any use shall not exceed 65  $L_{dn}$  as measured by any location on the lot occupied by such uses. Where a structure is occupied by more than one use, the noise level shall not be in excess of 60  $L_{dn}$  as measured within the interior space of the neighboring establishment. Noise caused by motor vehicles are exempted from this standard.

**Vibration:** All uses shall be so operated as not to generate vibration discernible without instruments by the average person while on or beyond the lot upon which the source is located or within an adjoining enclosed space if more than one establishment occupies a structure. Vibration caused by motor vehicles, trains, and temporary construction or demolition work is exempted from this standard.

**Particulate Matter and Air Contaminants:** In addition to compliance with the Air Quality Maintenance District (AQMD) standards, all uses shall be operated so as not to emit particulate matter or air contaminants which are readily detectable without instruments by the average person while on the lot containing such uses.

**Odor:** All uses shall be operated so as not to emit matter causing unpleasant odors which are perceptible to the average person while within or beyond the lot containing such uses.

**Humidity, Heat, and Glare:** All uses shall be operated so as not to produce humidity, heat, glare, or high-intensity illumination which is perceptible without instruments by the average person while on or beyond the lot containing the use.

**Definition  $L_{dn}$ :** Day-night average sound level. The day-night sound level is a measure of the cumulative noise exposure in the community. It results from the summation of an average noise level determined over a 24 hour time period with a weighting factor applied during the night time period (10 p.m. to 7 a.m.).

### CLASS B

CLASS B. It is the intent of the standards of this section to provide for uses whose operational needs may produce noise, vibration, particulate matter and air contaminants, odors, or humidity, heat, and glare which cannot be mitigated sufficiently to meet the Class A standards. The standards are so designed to protect uses on adjoining sites from effects which could adversely affect their functional and economic viability.

**Noise:** The maximum allowable noise level of any use shall not exceed 75  $L_{dn}$  as measured at the lot line of the lot containing the use. Where a use occupies a lot abutting residentially zoned land, the noise level shall not exceed 65  $L_{dn}$  as measured at the common lot line. Noise caused by motor vehicles and trains are exempted from this standard.

**Vibration:** All uses shall be operated so as not to generate vibration discernible without instruments by the average persons beyond the lot upon which the source is located. Vibration caused motor vehicles, trains, and temporary construction or demolition is exempted from this standard.

**Particulate Matter and Air Contaminants:** In addition to compliance with the AQMD standards, all uses shall be operated so as not to emit particulate matter or air contaminants which are readily detectable without instruments by the average person beyond any lot line of the lot containing such uses.

**Odor:** All uses shall be operated so as not to emit matter causing unpleasant odors which are perceptible to the average person beyond any lot line of the lot containing such uses.

**Humidity, Heat and Glare:** All uses shall be operated so as not to produce humidity, heat, glare or high-intensity illumination which is perceptible without instruments by the average person beyond the lot line of any lot containing such uses.

### CLASS C

CLASS C. It is the intent of the standards of this section to make allowances for industrial uses whose associated processes produce noise, particulate matter and air contaminants, vibration, odor, humidity, heat, glare or high-intensity which would adversely affect the functional and economic viability of other uses. The standards, when combined with standards imposed by other governmental agencies, serve to provide basic health and safety protection for persons employed within or visiting the area.

**Noise:** The maximum allowable noise level of any use shall not exceed 85  $L_{dn}$  as measured at the lot line of the lot containing the use. Where a use is within 200 feet of residentially zoned land, the noise level shall not exceed 65  $L_{dn}$ . Where a use occupies a lot abutting or separated by a street from a lot within the designated Class A or B performance standard or residential property, the performance standard of the abutting property shall apply at the common or facing lot line.

**Vibration:** All uses shall be operated so as not to generate vibration discernible without instruments by the average person beyond 600' from where the source is located. Vibration caused by motor vehicles, trains, and temporary construction and demolition is exempt from this standard.

**Particulate Matter and Air Contaminants:** In addition to compliance with the AQMD standards, all uses shall be operated so as not to emit particulate matter or air contaminants which: (a) are injurious to the health of either persons engaged in or related to the use of the lot, or persons residing, working, visiting, or recreating in neighboring areas; (b) substantially and adversely affect the maintenance of property in nearby areas; (c) are disruptive of industrial processes carried on in other parts of the industrial area. Where a use occupies a lot abutting or separated by a street lot with designated Class A or B performance standard for particulate matter and air contaminants shall apply at the common or facing lot line.

**Odor:** All uses shall be operated so as not to emit matter causing unpleasant odors which are perceptible to the average person beyond any lot line of the lot containing such uses.

**Humidity, Heat, and Glare:** All uses shall be operated so as not to produce humidity, heat, glare, or high-intensity illumination which is perceptible without instruments by the average person while on any lot zoned for residential purposes or any industrial property with a Class A or B performance standard designation.





# **PART IV**

## **OVERLAY DISTRICTS AND SUBAREA DEVELOPMENT STANDARDS**

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<b>Overlay Districts and Subarea Development Standards</b>	<b>Page</b>
<b>Introduction</b>	<b>IV- 1</b>
<b>Haven Avenue Overlay District</b>	<b>IV- 3</b>
<b>Subarea 1</b>	<b>IV- 18</b>
<b>Subarea 2</b>	<b>IV- 23</b>
<b>Subarea 3</b>	<b>IV- 27</b>
<b>Subarea 4</b>	<b>IV- 33</b>
<b>Subarea 5</b>	<b>IV- 37</b>
<b>Subarea 6</b>	<b>IV- 43</b>
<b>Subarea 7</b>	<b>IV- 48</b>
<b>Subarea 8</b>	<b>IV- 54</b>
<b>Subarea 9</b>	<b>IV- 59</b>
<b>Subarea 10</b>	<b>IV- 65</b>
<b>Subarea 11</b>	<b>IV- 69</b>
<b>Subarea 12</b>	<b>IV- 73</b>
<b>Subarea 13</b>	<b>IV- 79</b>
<b>Subarea 14</b>	<b>IV- 83</b>
<b>Subarea 15</b>	<b>IV- 89</b>
<b>Subarea 16</b>	<b>IV- 94</b>
<b>Subarea 17</b>	<b>IV-100</b>



## PART IV OVERLAY DISTRICTS & SUBAREA DEVELOPMENT STANDARDS

### INTRODUCTION

This part provides development standards for the individual subareas of the I.S.P. and for specific sections of Haven Avenue which crosses through the industrial area. Because of the City-wide significance of Haven Avenue as the major entry into the City, the Haven Overlay District provisions have been placed at the beginning of this part followed by those of the subarea's.

The purpose of preparing subarea development standards is to identify the most prominent development standards applied within each of the 17 subareas in the industrial area. For each subarea the following categories have been identified and discussed:

- Land Use Designation
- Primary Function
- Permitted Uses
- Conditional Uses
- Access and Circulation
- Minimum Parcel Size
- Setback Requirements
- Landscape Area Requirements
- Performance Standards
- Special Considerations

The list described above does not intend to cover every specific development requirement which may be utilized in developing within the industrial area, but does provide a quick overview of the major development considerations within each subarea. Any person interested in developing within the boundaries of the Specific Plan should refer to both Parts II and III of this plan for an indepth discussion of all the development constraints, opportunities, and standards. Any special conditions which may provide exception or modification to development requirement are discussed within the "Special Consideration" topic of each subarea.



FIG. IV-1

# HAVEN AVENUE OVERLAY DISTRICT



Urban Center

## CIRCULATION

- 120' R.O.W.
- 100' R.O.W.
- 88' or less R.O.W.

## RAIL SERVICE

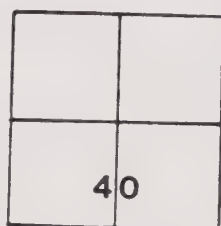
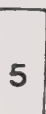
- Existing
- Proposed

## TRAILS/ROUTES

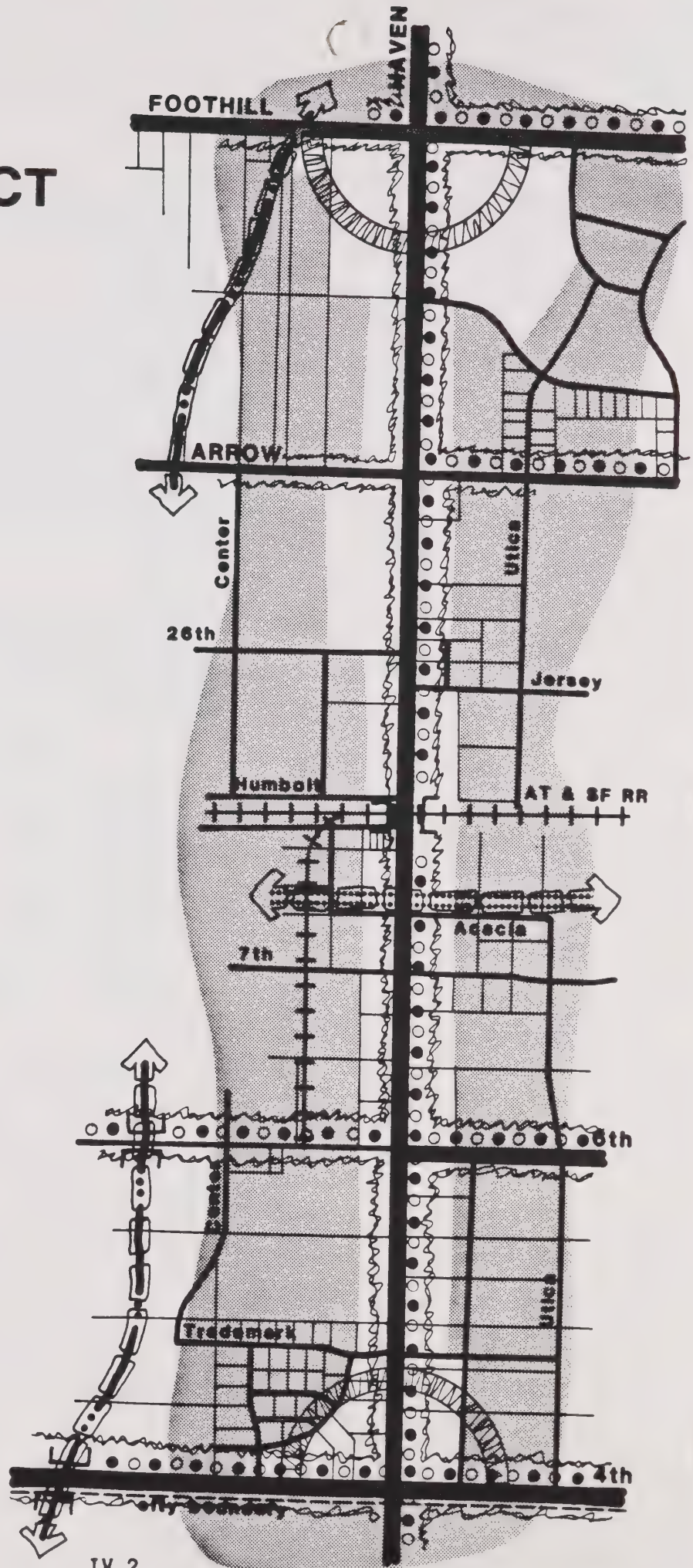
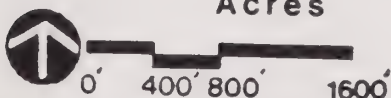
- Pedestrian
- Bicycle
- Regional Multi-Use
- Special Streetscape/Landscaping
- Power Line/Utility Easement
- Creeks & Channels
- Bridge
- Access Points
- Park



Fire Station



Acres





## HAVEN AVENUE OVERLAY DISTRICT

### Purpose

The purpose of the Overlay District is to establish development standards which address the unique setting and character of the Haven Avenue corridor. The Haven Avenue Overlay District is located on both the east and west sides of Haven Avenue extending from Foothill Boulevard south to 4th Street, as shown in Figure V-1.

### Applicability

The Overlay District is to be applied in conjunction with the Specific Plan for Subareas 6 and 7 and provides more specific and/or restrictive development policies, design standards and land use regulations for both Subareas 6 and 7. Topics not specifically covered by the Haven Avenue Overlay District will continue to be governed by the regulations of the Specific Plan. Development adjacent to the Haven Avenue Overlay District shall consider compatibility and integration with the Overlay District through the Master Plan process.

### Setting

Haven Avenue is located near the geographic center of the City and will be the most significant gateway into Rancho Cucamonga. Another important aspect is the distinctiveness of the Haven Avenue corridor in relation to other major arterials designated as "Special Boulevards". Haven Avenue is the major travel route for the City and has the potential for high end office development with a unique combination of direct access to the airport and the Interstate 10 freeway. This provides the City a rare opportunity to enhance its image by encouraging intensive, high quality office and professional development along the Haven Avenue corridor.

### Relation to General Plan

The General Plan affirms that travel routes are predominate elements of the community's image and encourages the distinctiveness of individual districts and roadway corridors. In addition, the General Plan states that a consistent design theme is necessary to reinforce the image or perception of a route. The Specific Plan states that the Haven Avenue corridor and the Industrial

Park category should be designed to project a "campus like image for firms seeking an attractive and pleasant working environment with high prestige value".

#### A. Goal Statement

Goals are statements that define the community's aspiration and intentions. The goal for the Haven Avenue corridor has evolved from sessions with the industrial and business communities and with the City's elected and appointed officials. This goal represents the current conception of and future aspirations for the best interest of the City for the Haven Avenue corridor.

- A.1. Encourage long-range master planned development along the Haven Avenue corridor which enhances Rancho Cucamonga's image by providing an intensive, high quality gateway into the City and by promoting a distinctive, attractive, and pleasant office park atmosphere in a campus like setting with high prestige identity.

#### B. Land Use Types

The intent of the following policies is to encourage land uses and development consistent with the design goal for the Haven Avenue corridor as an intensive, high quality gateway into the community.

- B.1. The primary land use function along Haven Avenue is intended to be of an administrative/professional and office nature. However, on the parcels directly adjacent to the railroad, the Planning Commission may consider additional uses permitted in Subarea 6, subject to the issuance of a Conditional Use Permit, and a finding that such a use will be consistent with the stated design goal for Haven Avenue and all other provisions of the Overlay District.

B.2. Select ancillary research services, commercial and business support service uses shall not exceed 20% of the floor area in any Master Planned development. Concentration of such uses in any building or along the street frontage is not permitted.

B.3. The following land use types are permitted or conditionally permitted within the Haven Avenue Overlay District. All other uses shall be prohibited.

Permitted Uses

Administrative and Office  
Financial, Insurance & Real Estate  
Services  
Communication Services  
Medical/Health Care Services  
Professional/Design Services  
Administrative Civic Services  
Cultural  
Business Supply Retail Sales & Services\*  
Business Support Services\*  
Eating and Drinking Establishments

Conditional Uses

Convenience Sales & Services\*  
Entertainment  
Food and Beverage Sales\*  
Fast Food Sales\*  
Hotel/Motel  
Personal Services\*  
Recreation Facilities  
Public Assembly  
Public Safety & Utility Services  
Religious Assembly

\*Ancillary Uses Limited to 20% of the floor area per B.2.

B.4. Fast food services are specifically excluded as a primary use. This would preclude the development of typical free standing fast food restaurants, most of which require drive-through facilities, in the Overlay District. However, fast food could be permitted as an ancillary or secondary use, subject to a Conditional Use Permit, as a part of a larger project, provided, however, such use not be located directly adjacent to Haven Avenue.



#### B.5. Accessory/Ancillary Use Restrictions

The purpose of the section is to set maximum development provisions for accessory/ancillary uses which are not normally permitted in the Haven Overlay District. The following provisions shall apply in the Haven Overlay District:

B.5.1. The total of all accessory/ancillary uses not listed as permitted or conditionally permitted uses\* are limited to 20% of the total building and business area. The ancillary commercial and business support service uses listed in Section B.2. may exceed the 20% business area limitation.

B.5.2. Accessory/Ancillary uses must be located within main buildings housing permitted principal uses. No outdoor accessory/ancillary uses are permitted.

B.5.3. No accessory/ancillary manufacturing uses are permitted in the Haven Overlay District.

\* Only uses defined and listed in Table III-2, Land Use Type Definitions can be considered as accessory or ancillary uses.

#### C. Master Planned Development

The intent of this section is to provide for integrated development at the earliest possible time in the review process. Through the Master Plan process there is opportunity to coordinate the efforts of single or multiple property owners and discourage piecemeal development. Finally, master planning of defined areas will avoid development of single parcels of land in a manner which would prevent or preclude future development of adjacent parcels in the best way feasible. It is not the intention of the master planning process to cast future development patterns in stone. Rather, it is an attempt to discover problems before they develop, to deal with issues while they can be solved, and to take advantage of opportunities while they exist. The standards and guidelines which follow are intended to apply to all projects and should not be constrained by parcel lines or specific site boundaries.



- C.1. A conceptual Master Plan shall be submitted for Planning Commission approval, together with any development proposal, including subdivision or parcel map applications. Such Master Plan shall address relationships to other parcels within the Master Plan area.
- C.2. At minimum, Master Plans shall indicate conceptual building locations, overall circulation, points of ingress and egress to both public and private streets, parking lot layouts, conceptual grading and drainage, areas to be used for landscaping and plazas, pedestrian circulation, and common signing. Areas intended for common use, such as shared access, reciprocal parking or pedestrian plazas shall also be identified. In addition, a statement of architectural intent and/or conceptual elevations shall be submitted to indicate how the architectural concepts including style, form, bulk, height, orientation, and materials relate to other buildings or projects within the planning area as well as to the overall design goal for Haven Avenue.
- C.3. The Master Plan boundaries indicated in Figure V-2 are logical planning boundaries based upon physical constraints and property ownership. These boundaries may be modified when it is determined that the Master Plan is consistent with the intent and purpose of the Haven Avenue Overlay District. The City Planner may require master planning of property outside the Overlay District, adjacent to a project proposal, where necessary to assure integrated development and promote the goal of the Haven Avenue Overlay District.
- C.4. No Parcel Map or Subdivision Map shall be accepted or approved without concurrent submittal and approval of a Master Plan to assure integrated development consistent with the goal of the Haven Avenue Overlay District.

FIG. IV-2

# MASTER PLAN AREAS



MASTER PLAN AREAS

## CIRCULATION

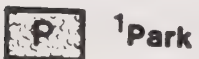
- 120' R.O.W.
- 100' R.O.W.
- 88' or less R.O.W.

## RAIL SERVICE

- Existing
- Proposed

## TRAILS/ROUTES

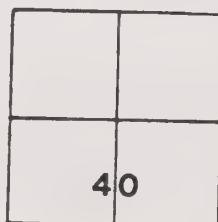
- Pedestrian
- Bicycle
- Regional Multi-Use
- Special Streetscape/Landscaping
- Power Line/Utility Easement
- Creeks & Channels
- Bridge
- Access Points



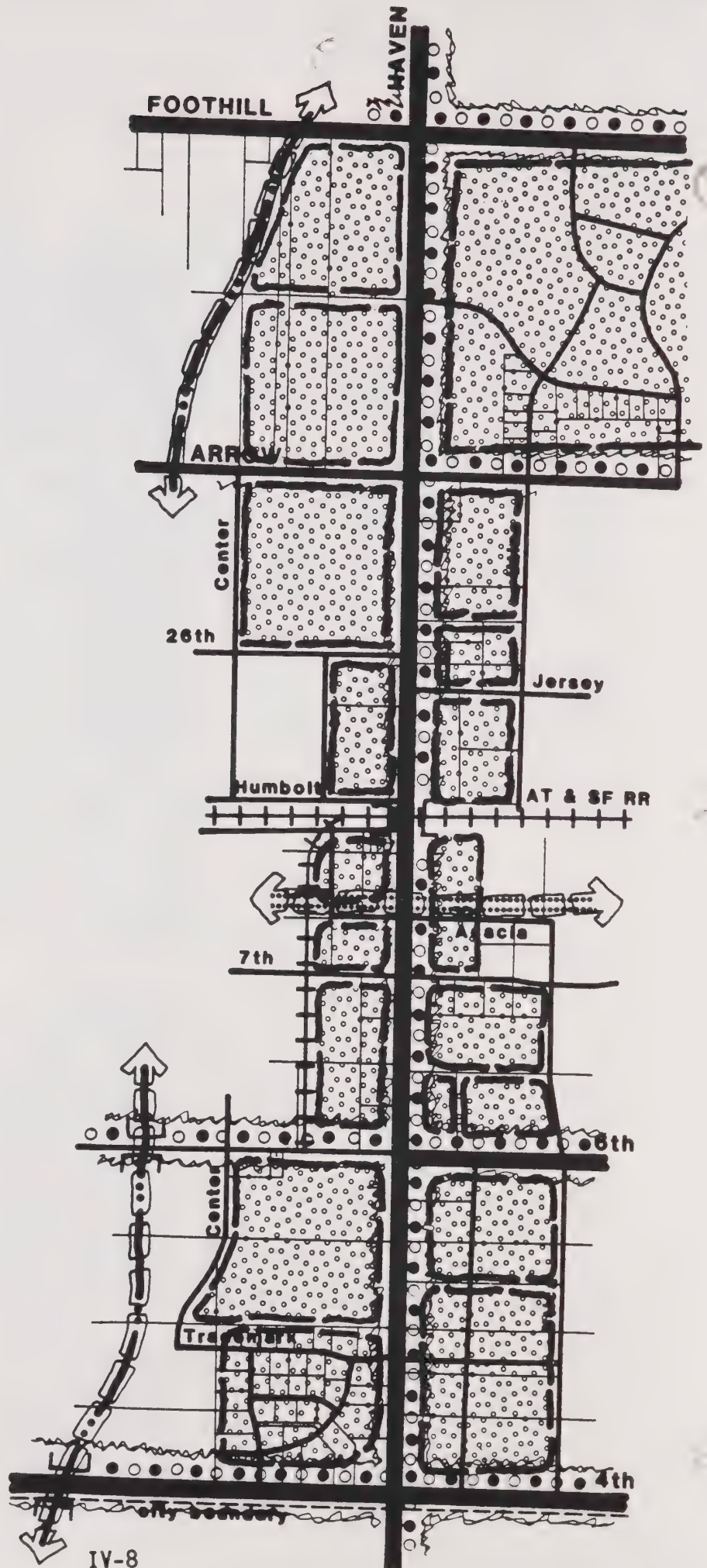
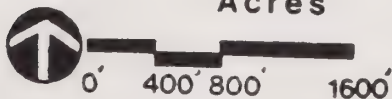
<sup>1</sup>Park



Fire Station



Acres





C.5. Architecture within Master Planned development shall have a compatible design style with variation in the building style, form, and materials in accordance with the architectural standards of the Overlay District.

#### Lot Size

C.6. Minimum parcel size shall be two (2) acres with a minimum parcel depth of 225 feet within a Master Plan development. A 300-foot minimum lot width shall also be required, consistent with the access control policies. The Planning Commission may waive these requirements when it is determined that the parcel is part of a Master Plan which is consistent with the intent and purpose of the Overlay District. All lots of record are allowed to develop according to the requirements of the Haven Avenue Overlay District.

#### Access

C.7. Vehicular access onto Haven Avenue shall be discouraged wherever suitable alternative access may be developed from other streets as determined through the Master Plan process. If vehicular access onto Haven Avenue is granted, said access shall be shared with adjacent parcels. The minimum distance between drive approaches shall be 300 feet with 100 feet the minimum distance between a drive approach and the curb return of any intersection along Haven Avenue. Also, reciprocal parking and access easements shall be required, where appropriate, with any development proposal.

C.8. Public transit facilities shall be considered within all Master Plans. Convenient pedestrian access shall be provided to designated transit facilities, such as bus stops.

C.9. On-site circulation for both pedestrians and vehicles shall consider existing or planned circulation patterns on adjacent properties. Connections shall be made where appropriate to foster more integrated development and enhance pedestrian movement.

#### D. Site Orientation

The following standards are intended to promote integrated, pedestrian oriented, office park development in a campus like setting:

##### Pedestrian Orientation

D.1. Site planning, including building configuration and placement, must create opportunities for courtyards and plazas and other landscaped open spaces and promote safe and convenient pedestrian movement with continuous landscaped pathways between buildings.

D.2. Parking areas and circulation aisles along the Haven Avenue street frontage are discouraged and may only be approved through the Master Plan process when it is determined appropriate and necessary to implement the design goal of the Haven Avenue Overlay District. Their visual impact shall be fully mitigated through dispersed parking areas and extensive landscaping and berming.

D.3. Vehicular circulation around the rear and side portions of a site is encouraged. Circulation aisles which fragment or disrupt the connection of pedestrian spaces throughout a project shall be avoided to the extent possible.

##### Parking

D.4. The use of dispersed parking areas which provide convenient access to buildings without interrupting interior pedestrian spaces is encouraged to reduce the need for large parking lots. Where large parking lots are necessary, they shall be located in areas less visible from Haven Avenue and their visual impact fully mitigated with building placement and extensive landscaping and berming.

D.5. On-street parking along Haven Avenue shall be prohibited.



## Setbacks

- D.6. Building placement at or near the streetscape building setback is strongly encouraged within all developments, particularly on corner lots with high visibility. Multiple building placement should provide variable streetscape setbacks to reduce streetscape monotony.
- D.7. A 45-foot average landscape setback and a minimum 45-foot building setback shall be required along Haven Avenue, as measured from the ultimate face of curb, including existing lots of record and condominium lots or lots within a center when designed as an integral part of a Master Planned development.
- D.8. Ancillary service and loading areas shall be designed and located where least visible from public view and adjacent properties designated "Industrial park".
- D.9. All existing and new utilities, including electrical services less than 34.5 KV, within the project shall be installed underground.

## E. Landscaping Requirements

The intent of the following standards is to enhance the visual quality of the streetscape and provide an attractive and pleasant working environment in a campus like setting.

## Minimum Landscape Coverage

- E.1. A minimum twenty-five (25) percent of net lot area (excludes right-of-way dedications and private streets) shall be landscaped areas and pedestrian hardscape plazas and courtyards.
- E.2. The landscape/hardscape coverage requirement may be modified for individual parcels within master planned developments when it is determined that the master plan as a whole meets the required coverage and the project is consistent with the intent and purposes of the design goal for Haven Avenue.

E.3. A maximum five (5) percent credit toward the required landscape/hardscape coverage may be permitted where appropriate public art is to be displayed in a setting which enhances pedestrian spaces and building architecture.

E.4. A consistent streetscape design theme shall be developed along the Haven Avenue streetscape which incorporated intensified landscaping with specimen size trees, alluvial rockscape, mounding, meandering sidewalks, and appropriate street furniture. Landscaping materials shall be selected which provide an historic link to the City's past, such as Eucalyptus windrows, citrus trees, and grape vines. In addition, a program of street name monument signs shall be developed to include low profile, natural alluvial rock monument signs with individual letters formed in a sandblasted concrete face.

#### Berming

E.5. Landscaping and berming shall be designed to create visual interest and variety to the streetscape, enhance building architecture, screen utilities and buffer views of automobiles, pavement and service areas, and to define and to distinguish the pedestrian environment from vehicular spaces.

#### Water Conservation

E.6. A combination of water conserving landscape and irrigation techniques are required such as the use of drought tolerant plant species and hardscape (non-irrigated) surfaces, and special irrigation systems such as drip emitters, low volume stream rotors, deep watering of trees and shrubs, tensiometers to measure soil moisture, and automatic timers.

E.7. Landscape materials shall be selected for their low maintenance, drought tolerance, and heat and wind tolerance.

#### F. Open Space and Pedestrian Environment

The intent of this section is to promote the functional design and location of pedestrian spaces, and provide convenient pedestrian circulation on- and off-site and promote a campus like setting.

F.1. The development of a pedestrian node or focal point, such as a plaza or courtyard is required within all projects.

F.2. The location of plazas and courtyards should encourage maximum pedestrian use and be separated and/or buffered from vehicular parking and circulation. Such locations may be near a prominent building entrance or along a centralized pedestrian path.

#### Pedestrian Facilities

F.3. Pedestrian plazas or courtyards shall be designed to create an attractive, comfortable, and functional setting with a "sense of place". A combination of the following design elements are encouraged, but are not limited to: pavement or surface texture; elevation/grade changes; use of landscape materials and structures to provide shade and define enclosed spaces; seating (eg - benches, steps, or raised planters); and outdoor eating areas. In addition, the use of water features, covered walkways, kiosks and public art are encouraged.

F.4. Trash receptacles, drinking fountains, light standards and other street furniture shall be designed to enhance the appearance and function of open space area.

F.5. Convenient pedestrian circulation shall be provided throughout all projects to connect parking areas and public transit facilities with buildings and pedestrian open spaces.

#### G. Architecture

The following standards are intended to promote a high quality office park image with high prestige identity.



## Urban Style

- G.1. Desirable architecture along Haven Avenue shall project a high quality progressive, sophisticated, and urban style of development. While the use of a variety of exterior materials may be permitted to achieve this image; Spanish, Mediterranean, or traditional architecture styles are not generally considered appropriate for office buildings.
- G.2. Variations in architectural styles, construction methods, and materials for certain ancillary uses, such as restaurants and banks, may be permitted where the architecture is exemplary and consistent with the high quality image required for Haven Avenue and where a particular design is necessary and more appropriate considering the intended use of the building.

## Multiple Story

- G.3. Multiple story buildings of sufficient mass are encouraged that reflect the scale and proportion of the Haven Avenue right-of-way and streetscape setbacks. Low linear buildings are discouraged.
- G.4. Building design elements that are considered inappropriate for an office park atmosphere include retail type storefront elevations (linear configurations, continuous ground floor glazing, multiple doorways), and numerous overhead roll-up doors which promote concentrations of ancillary commercial and business support services, and are therefore prohibited.
- G.5. Service and loading areas shall be screened from public view and adjoining properties, wherever possible, to reduce site design constraints on future adjacent development. The design of loading doors shall be consistent with office architectural design features. All building equipment must be housed within the building or below finished grade. All equipment screening must be architecturally integrated into the overall office building design.



## Architectural Variety

- G.6. Within the Overlay District, all building features must exhibit an office style architecture regardless of the type of accessory/ancillary uses or equipment contained within the building or on the site. Architectural planes shall have variation in depth and angle to create variety and interest in the basic form and silhouette of the building.
- G.7. Articulation of the elevation surfaces is encouraged through the use of openings and recesses which create texture and shadow patterns and provide variety in the building plane or surface.
- G.8. Building entrances shall be well articulated and project a formal entrance statement through variation of architectural planes, pavement surface treatment, and landscaped plazas.
- G.9. Accent treatment such as changes in exterior materials and texture is encouraged in conjunction with variation in the major form giving elements of a structure.

## Signs

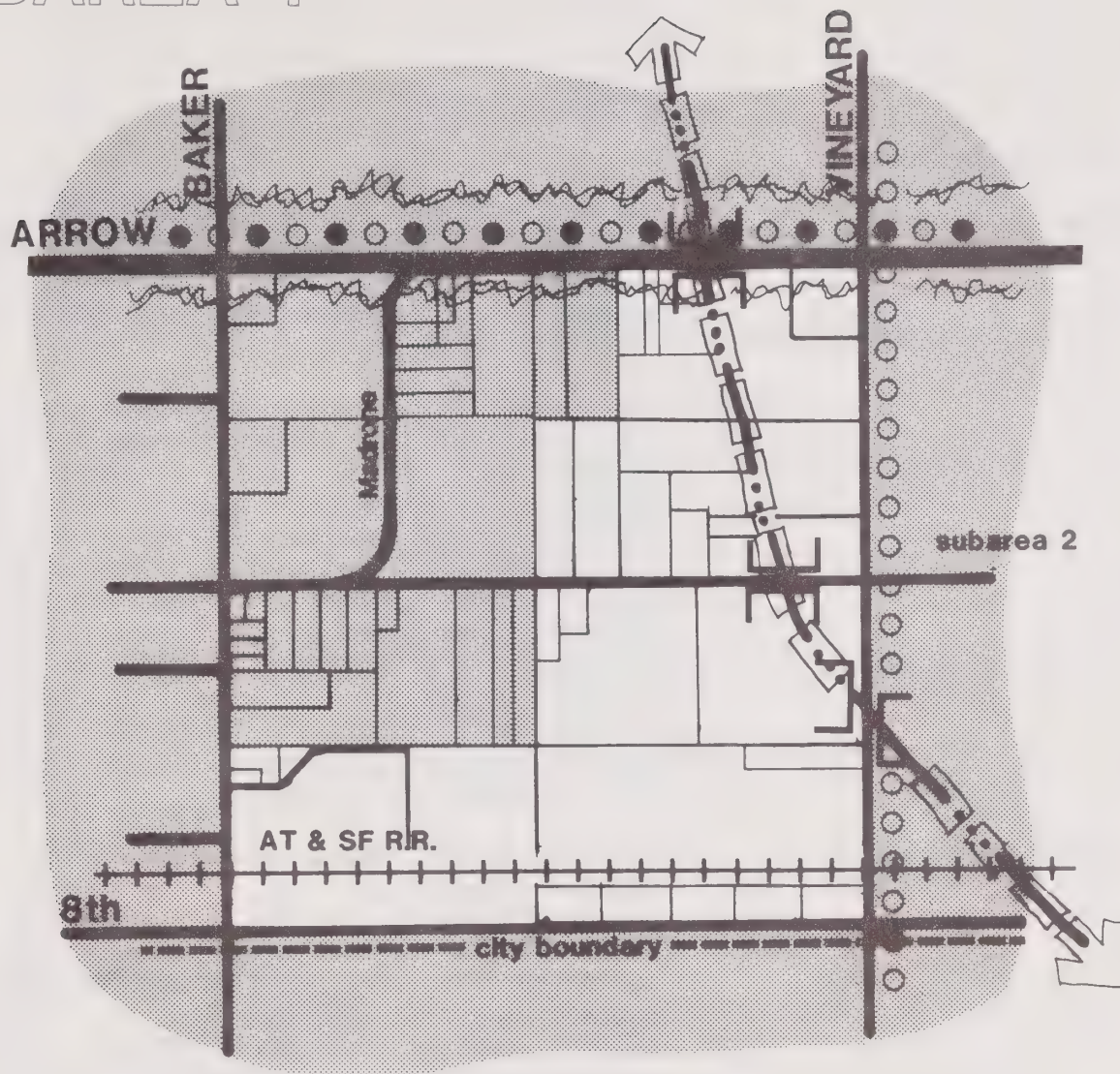
- G.10. A coordinated Uniform Sign Program shall be required for any development, including wall and monument signs. Building wall signs shall consist of individual letters and can signs are prohibited. The size, number, typical design and location of the signs, as permitted by the City's Sign Ordinance, shall be submitted with the development application and be reviewed concurrently.

## H. Urban Centers

The following standards are intended to promote the highest quality development and intensity to create community focal points or "urban centers" near the key intersections of 4th Street and Foothill Boulevard, as shown in Figure V-1.

- H.1. The applicability of these urban center standards for specific parcels shall be determined through the Master Plan review process.
- H.2. Multiple story buildings shall be of the highest design quality, particularly at the immediate corners of 4th Street and Foothill Boulevard. Desirable structural components include steel, and concrete in conjunction with curtain walls, spandrels and glass. Wood frame structures are discouraged.
- H.3. A minimum thirty (30) percent of net lot area (excludes right-of-way dedications and private streets) shall be landscaped areas and pedestrian hardscape plazas or courtyards.
- H.4. Special landscaping and streetscape design features shall be developed for the intersections at 4th Street and Foothill Boulevard to create an intensive and prestigious gateway entry into Rancho Cucamonga and the Haven Avenue Corridor.
- H.5. The use of parking structures is encouraged to promote intensified development and maximize the site area devoted to urban pedestrian plazas and courtyards. Parking structures shall be harmoniously designed with the main building and located around the rear or side portions of the site.
- H.6. The minimum parcel size shall be five (5) acres unless waived by the Planning Commission when it is determined that the parcel is designed as an integral part of a master planned development consistent with the intent and purpose of the Overlay District.

# SUBAREA 1



## CIRCULATION

	120' R.O.W.
	100' R.O.W.
	88' or less R.O.W.

## RAIL SERVICE

	Existing
	Proposed

## TRAILS/ROUTES

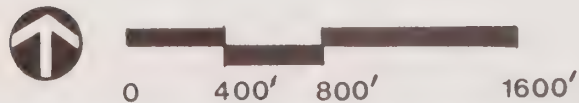
	Pedestrian
	Bicycle
	Regional Multi-Use

	Bridge
	Access Points

Creeks & Channels

Park<sup>1</sup>

Special Streetscape Landscaping



Note: Parcel lines and lot configurations are shown as approximation only.

<sup>1</sup>The sites shown may not be currently owned nor is the location site specific. The depiction of a site is an indication of a projected future need that may be adjusted over time as the City develops.



## SUBAREA 1

### Land Use Designation

General Industrial

### Primary Function

The long-range plan for this subarea is to maintain its function primarily as a community serving industrial area, consisting of light manufacturing, warehousing, and other General Industrial Group uses.

The subarea is located in the eastern portions of the Industrial Area, between Arrow Route and 8th Street and Baker and Vineyard Avenues. Surrounded by residential uses to the north, east and south, the subarea contains a few existing industrial uses, which occupy approximately 25 acres. The subarea is also transversed by Cucamonga Creek, which was recently improved by the Army Corp of Engineers. While the subarea is directly accessible to the rail and from Ontario International Airport via Vineyard, the uses that exist today are not particularly dependent on these transportation modes.

### Permitted Uses

Custom Manufacturing  
Light Manufacturing  
Research Services  
Light Wholesale, Storage, and Distribution  
Agricultural/Nursery Supplies and Service  
Automotive Rental/Leasing  
Automotive/Light Truck Repair-Minor  
Automotive/Truck Repair-Major  
Building Contractor's Offices and Yards  
Building Maintenance Services  
Building and Light Equipment Supplies and Sales  
Business Supply Retail Sales and Services  
Business Support Services  
Communication Services  
Eating and Drinking Establishments  
Laundry Services  
Repair Services  
Administrative Civic Services  
Flood Control/Utility Corridor

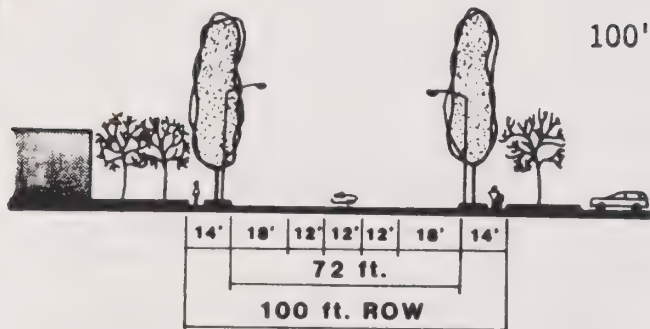


## SUBAREA 1 (Continued)

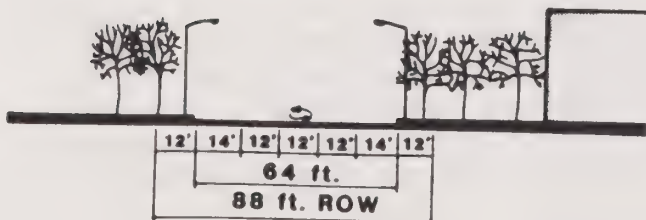
### Conditional Uses

Medium Manufacturing  
 Administrative and Office  
 Professional/Design Services  
 Public Storage  
 Medium Wholesale, Storage and Distribution  
 Animal Care  
 Automotive Fleet Storage  
 Automotive Service Station  
 Convenience Sales and Services  
 Financial, Insurance and Real Estate Services  
 Food and Beverage Sales  
 Heavy Equipment Sales and Rentals  
 Medical/Health Care Services  
 Personal Services  
 Recreation Facilities  
 Extensive Impact Utility Facilities  
 Public Assembly  
 Public Safety and Utility Services  
 Religious Assembly

### Access and Circulation

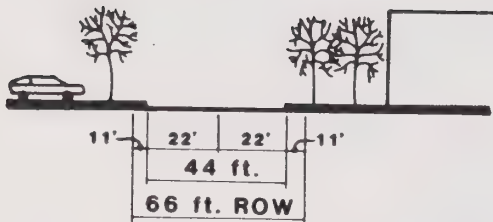


### 88' Right-of Way - Vineyard



## SUBAREA 1 (Continued)

### 66' Right-of-Way - Baker



Minimum Parcel Size

One-half (1/2) acre

Setback Requirements  
(Measured from  
ultimate face of curb)

	Average Landscaping	Parking	Building
Arrow	45'	25'	45'
Vineyard	35'	20'	35'
Baker	25'	15'	25'
9th St. and other local streets*	25'	15'	25'

Landscape Requirements

12% of net lot area

Performance Standards

Noise: The maximum allowable noise level of any use shall not exceed 75  $L_{dn}$  as measured at the lot line of the lot containing the use. Where a use occupies a lot abutting residentially zoned land, the noise level shall not exceed 65  $L_{dn}$  as measured at the common lot line. Noise caused by motor vehicles and trains are exempted from this standard.

Vibration: All uses shall be operated so as not to generate vibration discernible without instruments by the average persons beyond the lot upon which the source is located. Vibration caused by motor vehicles, trains, and temporary construction or demolition is exempted from this standard.

\*Note: See Special Consideration for 8th Street

## SUBAREA 1 (Continued)

Particulate Matter and Air Contaminants: In addition to compliance with the AQMD standards, all uses shall be operated so as not to emit particulate matter or air contaminants which are readily detectable without instruments by the average person beyond any lot line of the lot containing such uses.

Odor: All uses shall be operated so as not to emit matter causing unpleasant odors which are perceptible to the average person beyond any lot line of the lot containing such uses.

Humidity, Heat and Glare: All uses shall be operated so as not to produce humidity, heat, glare or high-intensity illumination by the average person beyond the lot line of any lot containing such uses.

## Special Consideration

Properties adjacent to residential areas shall have a minimum of 45' building setback from property line on interior lot lines abutting residential districts, and 45' from ultimate face of curbs on abutting streets abutting residential districts. Minimum front parking setback shall be 35' for such parcels.

In areas which abut residential uses, screening of industrial buildings, parking, storage and equipment areas is required through the use of walls, fences and/or berms. The screening, in conjunction with the streetscape setback and landscaping will soften the edge of the industrial development and provide an attractive design element. Screen wall/fence height shall not exceed 3 feet inside the 40' parking setback.

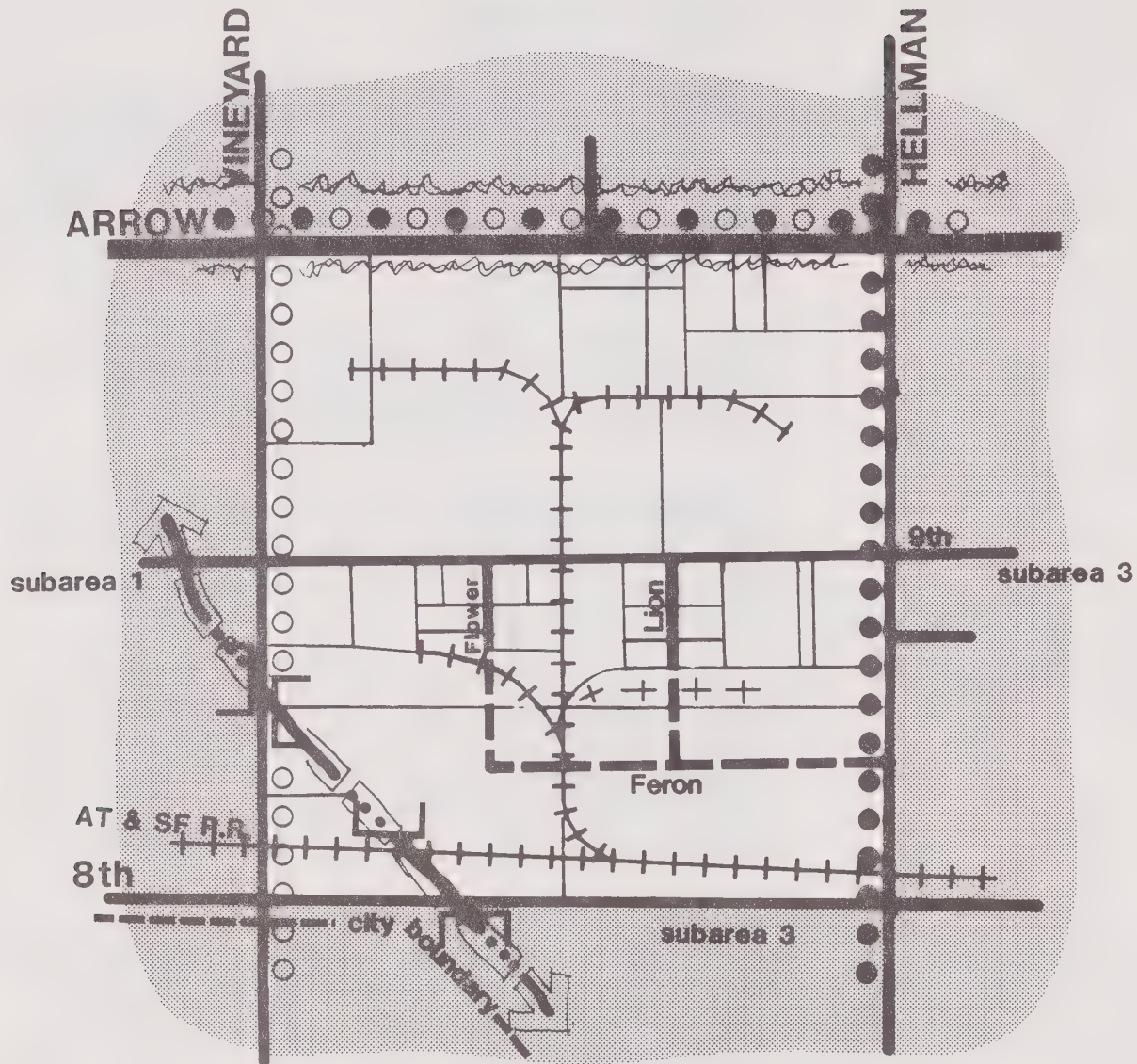
In areas abutting residential uses, access via internal circulation in place of residential streets is encouraged, when feasible, in order to minimize impact upon existing residential, vehicular and pedestrian circulation patterns.

Although the right-of-way on 8th Street west of Vineyard is greater than 54', the local street setbacks for buildings and parking shall apply. However, considering the shallow depth of property on the north side of 8th Street, the average depth of streetscape landscaping required shall be determined during the Technical/Design Review process.



FIG. IV-4

# SUBAREA 2



## CIRCULATION

	120' R.O.W.
	100' R.O.W.
	88' or less R.O.W.

## RAIL SERVICE

	Existing
	Proposed

## TRAILS/ROUTES

	Pedestrian
	Bicycle
	Regional Multi-Use



Bridge



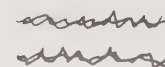
Access Points



Creeks & Channels



Park<sup>1</sup>



Special Streetscape/  
Landscaping



Note: Parcel lines and lot configurations are shown as approximation only.

<sup>1</sup>The sites shown may not be currently owned nor is the location site specific. The depiction of a site is an indication of a projected future need that may be adjusted over time as the City develops.



## SUBAREA 2

### Land Use Designation

General Industrial

### Primary Function

The long-range plan for this subarea is to strengthen its manufacturing and warehousing activities, complemented by rail access. Portions of this Subarea have lead/spur rail lines proposed to accommodate Rail Service needs of industrial uses. It is the City's policy to encourage the development of Rail Service facilities on parcels with strong rail development potential. Standards have been established as a part of this Specific Plan (Part III, Section IV.G) which require the Rail Service development of all parcels which adjoin existing or proposed lead/spur rail lines.

This subarea is bounded on the north by Arrow Route, on the east by Hellman Avenue, on the south by 8th Street, and on the west by Vineyard Avenue. Of the 155 acres in the subarea, approximately 60 acres are occupied by existing industrial uses, most of which are served by rail.

### Permitted Uses

Custom Manufacturing  
Light Manufacturing  
Research Services  
Light Wholesale, Storage and Distribution  
Medium Wholesale, Storage and Distribution  
Agricultural/Nursery Supplies and Services  
Eating and Drinking Establishments  
Repair Services  
Flood Control/Utility Corridor

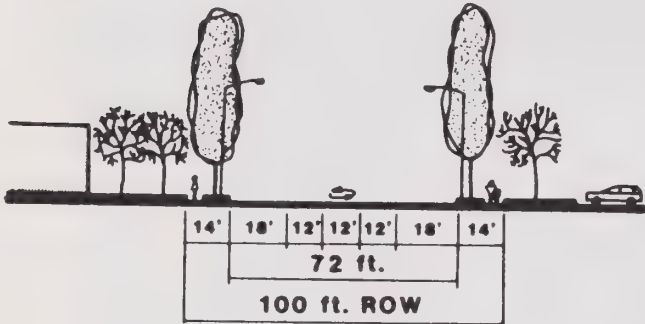
### Conditional Uses

Medium Manufacturing  
Public Storage  
Automotive/Light Truck Repair - Minor  
Automotive/Truck Repair - Major  
Building Contractor's Office & Yards  
Building/Light Equipment Supplies and Sales  
Business Support Services  
Convenience Sales and Services  
Food and Beverage Sales  
Heavy Equipment Sales and Rentals  
Recreation Facilities  
Public Safety and Utility Services

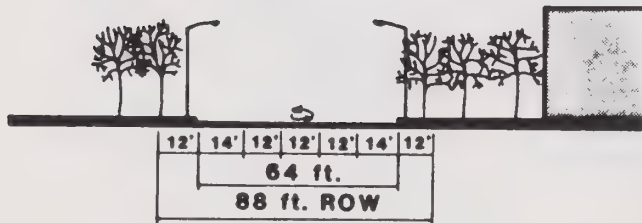
## SUBAREA 2 (Continued)

### Access and Circulation

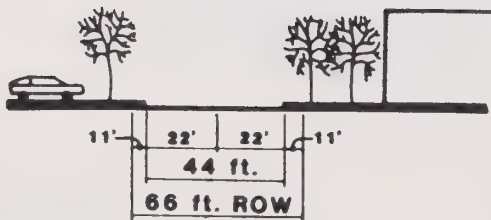
#### 100' Right-Of-Way - Arrow



#### 88' Right-Of-Way - Vineyard Hellman



#### 66' Right-Of-Way - 9th Street Other Local Streets



### Minimum Parcel Size

One (1) acre

### Setbacks

(Measured from Ultimate face of curb)

	Average Landscaping	Parking	Building
Arrow	45'	25'	45'
Vineyard	35'	20'	35'
Hellman	35'	20'	35'
9th Street and other local streets	25'	15'	25'

Landscaped area requirement 10% of net lot area

## SUBAREA 2 (Continued)

### Performance Standards

Noise: The maximum allowable noise level of any use shall not exceed 75  $L_{dn}$  as measured at the lot line of the lot containing the use. Where a use occupies a lot abutting residentially zoned land, the noise level shall not exceed 65  $L_{dn}$  as measured at the common lot line. Noise caused by motor vehicles and trains are exempted from this standard.

Vibration: All uses shall be operated so as not to generate vibration discernible without instruments by the average persons beyond the lot upon which vehicles, trains, and temporary construction or demolition is exempted from this standard.

Particulate Matter and Air Contaminants: In addition to compliance with the AQMD standards, all uses shall be operated so as not to emit particulate matter or air contaminants which are readily detectable without instruments by the average person beyond any lot line of the lot containing such uses.

Odor: All uses shall be operated so as not to emit matter causing unpleasant odors which are perceptible to the average person beyond any lot line of the lot containing such uses.

Humidity, Heat and Glare: All uses shall be operated as to not produce humidity, heat, glare or high-intensity illumination which is perceptible without instruments by the average person beyond the lot line of any lot containing such uses.

### Special Consideration

Parcels which adjoin existing or proposed lead/spur lines as indicated on the Subarea Map are required to be developed in accordance with Rail Service Standards (Part III, Section IV.G, Appendix D).

Properties adjacent to residential areas shall have a minimum of 45' building setback from property line on interior lot lines abutting residential districts and 45' from ultimate face of curbs on streets abutting residential districts. Minimum front parking setback shall be 35' for such parcels.

## SUBAREA 2 (Continued)

In areas which abut residential uses, screening of industrial buildings, parking, storage, and equipment areas is required through the use of walls, fences, and/or berms. The screening, in conjunction with the streetscape setback and landscaping, will soften the edge of the industrial development and provide an attractive design element. Screen wall/fence height shall not exceed 3 feet inside the 40' parking setback.

In areas abutting residential uses, access via internal circulation in place of residential street is encouraged, when feasible, in order to minimize impact upon existing residential, vehicular, and pedestrian circulation patterns.



### SUBAREA 3

#### Land Use Designation

#### General Industrial

#### Primary Function

The long-range plan for this subarea is to protect and enhance the viability of the existing industrial uses by eliminating conflicting land uses and improving land use transitions. Its primary function is to remain as general industrial.



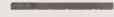
This subarea is bounded on the north by Arrow Route, on the east by Archibald Avenue, on the south by 7th Street, and on the west by Hellman Avenue (for the portion north of the 8th Street and by Cucamonga Creek (for the portion south of the 8th Street). Surrounded by residential uses to the north and the south, and by the North Town neighborhood and an elementary school on the east. Subarea 3 contains a mixture of land uses including industrial, commercial and residential. The existing industrial uses include some of the older establishments such as lumberyard, metal forging shops, etc., as well as more recent light manufacturing and office uses along Archibald and Arrow. Older residential uses still remain on the southern side of the AT & SF right-of-way. A small convenience store is located on Archibald near the railroad track. Of all the 14 subareas in the Rancho Cucamonga Industrial Area, this subarea is not densely developed; approximately half of its land area, however, is still undeveloped.

#### Permitted Uses


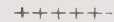
Custom Manufacturing  
Light Manufacturing  
Research Services  
Public Storage  
Light Wholesale, Storage, and Distribution  
Medium Wholesale, Storage and Distribution  
Agricultural/Nursery Supplies and Services  
Automotive Rental/Leasing  
Automotive/Light Truck Repair-Minor  
Automotive/Truck Repair-Major  
Building Contractor's Offices and Yards  
Building Maintenance Services  
Building and Light Equipment Supplies and Sales  
Business Supply Retail Sales and Services  
Business Support Services  
Communication Services  
Eating and Drinking Establishments  
Laundry Services  
Administrative Civic Services  
Flood Control/Utility Corridor

FIG. IV-5


CIRCULATION

-  120' R.O.W.
-  100' R.O.W.
-  88' or less R.O.W.

RAIL SERVICE

-  Existing
-  Proposed

TRAILS/ROUTES

-  Pedestrian


-  Bicycle

-  Regional Multi-Use

-  Special Streetscape/Landscaping


-  Power Line/Utility Easement

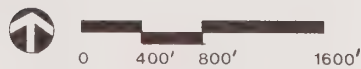
-  Creeks & Channels

-  Bridge

-  Access Points

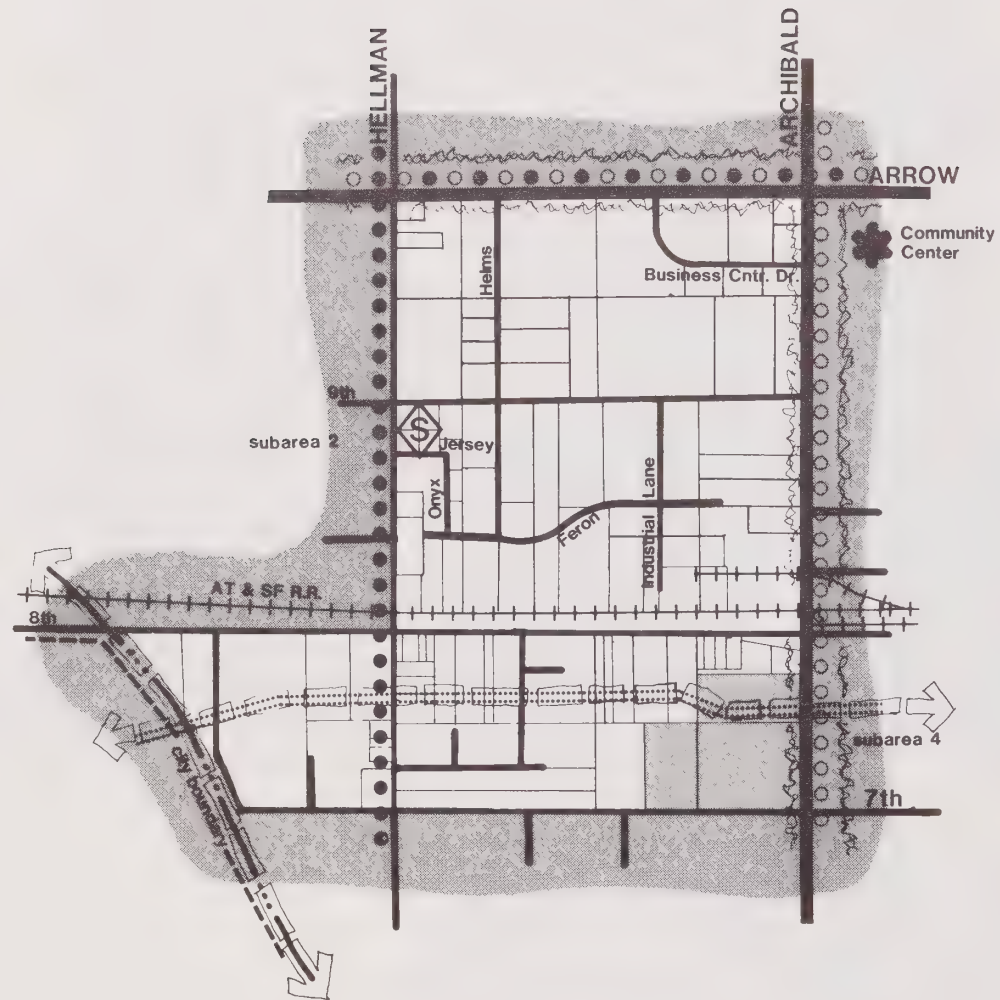
-  Fire Station<sup>1</sup>

-  Sherriff Station



Note: Parcel lines and lot configurations are shown as approximation only.

<sup>1</sup> The sites shown may not be currently owned nor is the location site specific. The depiction of a site is an indication of a projected future need that may be adjusted over time as the City develops.



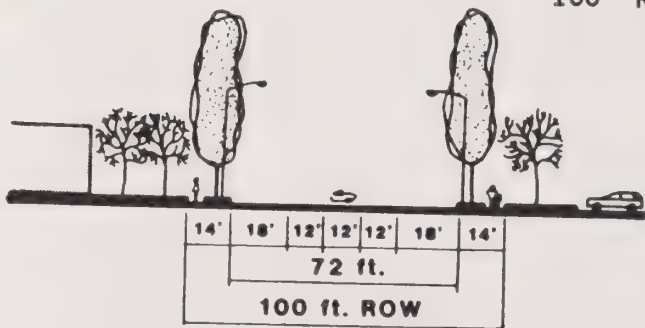
# SUBAREA 3 (Continued)

## Conditional Uses

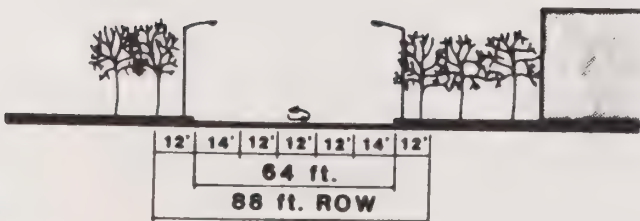
Medium Manufacturing  
 Administrative and Office  
 Professional/Design Services  
 Animal Care  
 Automotive Fleet Storage  
 Automotive Service Station  
 Convenience Sales and Services  
 Fast Food Sales  
 Financial, Insurance and Real Estate Services  
 Food and Beverage Sales  
 Heavy Equipment Sales and Rentals  
 Medical/Health Care Services  
 Personal Services  
 Recreation Facilities  
 Repair Services  
 Public Assembly  
 Public Safety and Utility Services  
 Religious Assembly

## Access and Circulation

100' Right-Of-Way - Archibald  
 Arrow

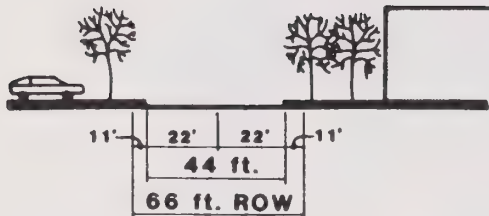


88' Right-of-Way - Hellman  
 7th



### SUBAREA 3 (Continued)

66' Right-Of-Way - 9th Street  
and other local roads



Minimum Parcel Size

One-half (1/2) acre

Setback Requirements  
(Measured from ultimate face of curb)

	Average Landscaping	Parking	Building
Archibald	45'	25'	45'
7th*	35'	20'	45'*
Hellman	35'	20'	35'
9th and all other	25'	15'	25'

\*Adjacent to residential area

Landscaped Area  
Requirements

12% of net lot area

Performance Standards

Noise: The maximum allowable noise level of any use shall not exceed 75  $L_{dn}$  as measured at the lot line of the lot containing the use. Where a use occupies a lot abutting residentially zoned land, the noise level shall not exceed 65  $L_{dn}$  as measured at the common lot line. Noise caused by motor vehicles and trains are exempted from this standard.

Vibration: All uses shall be operated so as not to generate vibration discernible without instruments by the average persons beyond the lot upon which the source is located. Vibration caused motor vehicles, trains, and temporary construction or demolition is exempted from this standard.



### SUBAREA 3 (Continued)

Particulate Matter and Air Contaminants: In addition to compliance with the AWMD standards, all uses shall be operated so as not to emit particulate matter or air contaminants which are readily detectable without instruments by the average person beyond any lot line of the lot containing such uses.

Odor: All uses shall be operated so as not to emit matter causing unpleasant odors which are perceptible to the average person beyond any lot line of the lot containing such uses.

Humidity, Heat and Glare: All uses shall be operated so as not to produce humidity, heat, glare or high-intensity illumination which is perceptible without instruments by the average person beyond the lot line of any lot containing such uses.

### Special Consideration

At the southwest corner of Archibald and Arrow, the Cucamonga Business Park has the characteristics of the Industrial Park category and development shall be permitted to occur according to the Industrial Park Land uses and standards, as shown in Subareas 6, 7, 12 and 16.

Properties adjacent to residential areas shall have a minimum of 45' building setback from property line on interior lot lines abutting residential districts and 45' from ultimate face of curbs on streets abutting residential districts. Minimum front parking setback shall be 35' for such parcels.




In areas which abut residential uses, screening of industrial buildings, parking, storage, and equipment areas is required through the use of walls, fences, and/or berms. The screening, in conjunction with the streetscape setback and landscaping will soften the edge of the industrial development and provide an attractive design element. Screen wall/fence height shall not exceed 3 feet inside the 40' parking setback.

In areas abutting residential uses, access via internal circulation in place of residential streets is encouraged, when feasible, in order to minimize impact upon existing residential, vehicular, and pedestrian circulation patterns.


FIG. IV-6

# SUBAREA 4

## CIRCULATION

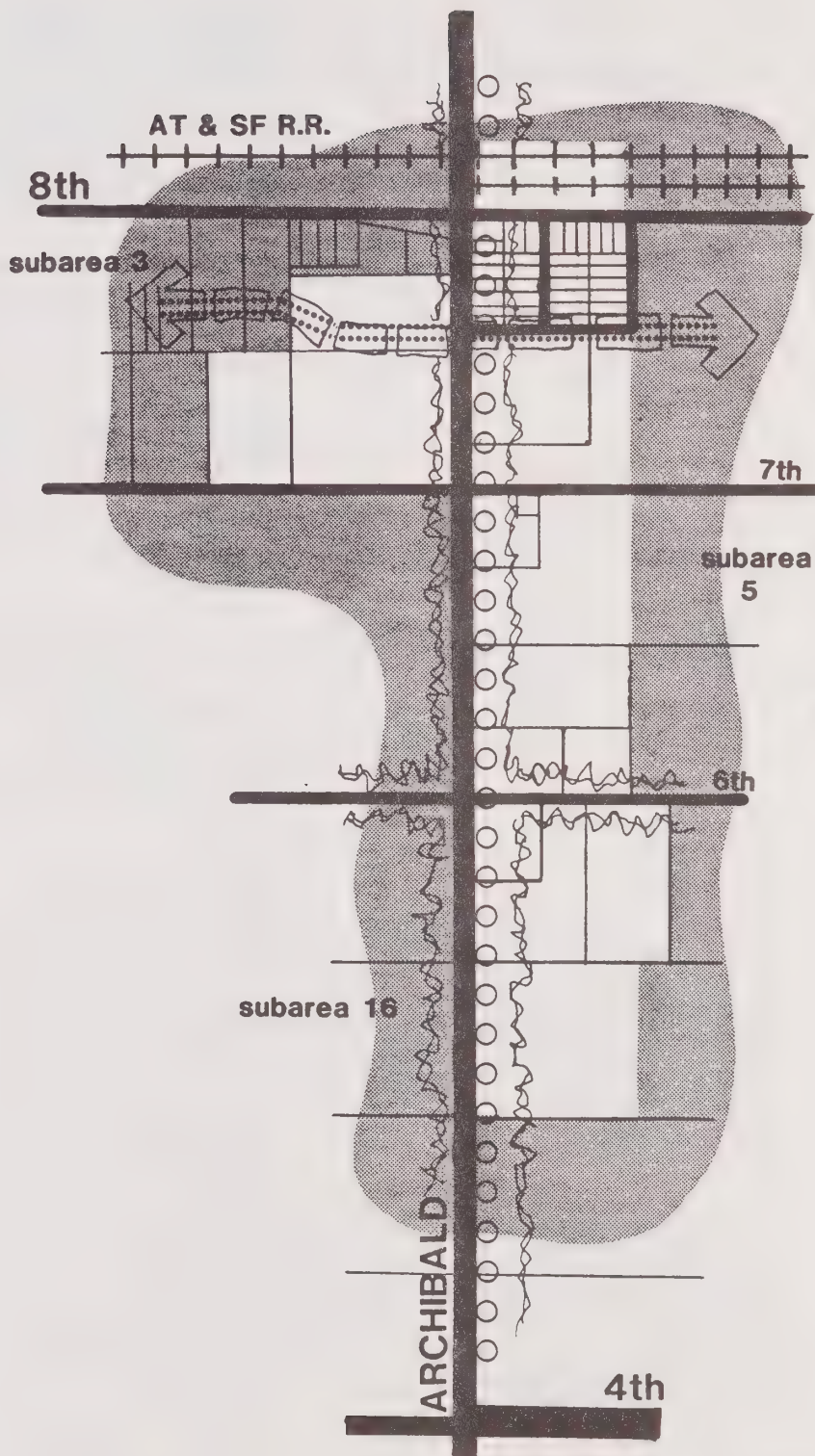
	120' R.O.W.
	100' R.O.W.
	88' or less R.O.W.

## RAIL SERVICE

	Existing
	Proposed

## TRAILS/ROUTES

	Pedestrian
	Bicycle
	Regional Multi-Use
 	Special Streetscape/ Landscaping
	Power Line/ Utility Easement
	Access Points



Note: Parcel lines and lot configurations are shown as approximation only.

#### SUBAREA 4

##### Land Use Designation

General Industrial

##### Primary Function

The function of this area should be to provide for General Industrial development and space for support commercial users to the industrial area. The location of the area provides good access to the industrial area west of Haven and to vehicular traffic between 1-10 and the existing residential areas. Subarea 4 is bounded on the north by AT & SF Railroad line and follows Archibald to about 600' south of 6th Street. The Subarea includes the smaller industrial units which front Archibald Street. These units are suitable for smaller commercial or industrial users.

##### Permitted Uses

Custom Manufacturing  
Light Manufacturing  
Research Services  
Light Wholesale, Storage and Distribution  
Automobile Rental/Leasing  
Building Maintenance Services  
Business Supply Retail Sales and Services  
Business Support Services  
Communication Services  
Eating and Drinking Establishments  
Financial, Insurance and Real Estate Services  
Laundry Services  
Repair Services  
Administrative Civic Services  
Flood Control/Utility Corridor

##### Conditional Uses

Administrative and Office  
Professional/Design Services  
Public Storage  
Medium Wholesale, Storage and Distribution  
Automotive/Light Truck Repair-Minor  
Automotive Service Station  
Building Contractor's Offices and Yards  
Building and Light Equipment Supplies and Sales  
Convenience Sales and Services  
Entertainment  
Fast Food Sales  
Food and Beverage Sales  
Funeral and Crematory Services  
Medical/Health Care  
Personal Services  
Recreation Facilities  
Cultural  
Public Assembly

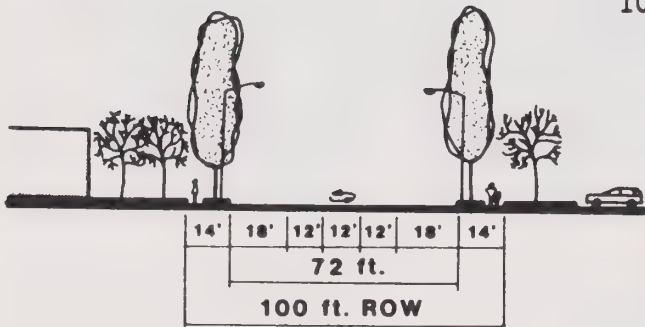


# SUBAREA 4 (Continued)

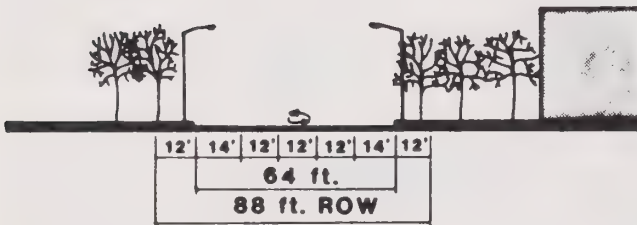
Public Safety and Utility Services  
Religious Assembly

## Access and Circulation

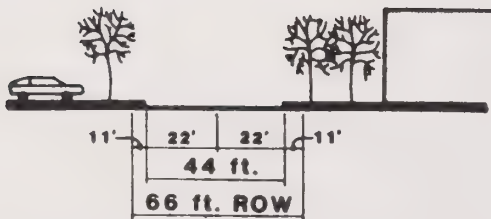
100' Right-Of-Way - Archibald



88' Right-Of-Way - 6th  
7th west of Archibald



66' Right-Of-Way - 7th (east of Archibald)  
and all other local streets



Minimum Parcel Size

One-half (1/2) acre



#### SUBAREA 4 (Continued)

Setback Requirements (Measured from ultimate face of curb)	Average Landscaping	Parking	Building
Archibald	45'	25'	45'
6th	"	"	"
7th and all other* local streets	25'	15'	25'
** 7th Street	35'	20'	45'

Landscaping Requirements      12% of net lot area

Notes: \* 7th Street, east of Archibald.

      \*\* 7th Street, west of Archibald adjacent to residential properties.

#### Performance Standards

Noise: The maximum allowable noise level of any use shall not exceed 75  $L_{dn}$  as measured at the lot line of the lot containing the use. Where a use occupies a lot abutting residentially zoned land, the noise level shall not exceed 65  $L_{dn}$  as measured at the common lot line. Noise caused by motor vehicles and trains are exempted from this standard.

Vibration: All uses shall be operated so as not to generate vibration discernible without instruments by the average persons beyond the lot upon which the source is located. Vibration caused motor vehicles, trains, and temporary construction or demolition is exempted from this standard.

Particulate Matter and Air Contaminants: In addition to compliance with the AAMD standards, all uses shall be operated so as not to emit particulate matter or air contaminants which are readily detectable without instruments by the average person beyond the lot line of the lot containing such uses.

Odor: All uses shall be operated so as not to emit matter causing unpleasant odors which are perceptible to the average person beyond any lot line of the lot containing such uses.

#### SUBAREA 4 (Continued)

Humidity, Heat and Glare: All uses shall be operated so as not to produce humidity, heat, glare or high-intensity illumination which is perceptible without instruments by the average person beyond the lot line of any lot containing such uses.

#### Special Considerations

If a commuter train service occurs along the AT & SF Railroad line, the existing station should be considered as a temporary location for a transit stop. This would provide the most direct access to the existing employment area. Any future permanent transit stop location should be studied as part of the Regional Transportation Plan.

Properties adjacent to residential areas shall have a minimum of 45' building setback from property line on interior lot lines abutting residential districts and 45' from ultimate face of curbs on streets abutting residential districts. Minimum front parking setback shall be 35' for such parcels.

In areas which abut residential uses, screening of industrial buildings, parking, storage, and equipment areas is required through the use of walls, fences and/or berms. The screening, in conjunction with the streetscape setback and landscaping will soften the edge of the industrial development and provide an attractive design element. Screen wall/fence height shall not exceed 3 feet inside the 40' parking setback.

In areas abutting residential uses, access via internal circulation in place of residential street is encouraged, when feasible, in order to minimize impact upon existing residential, vehicular, and pedestrian circulation patterns.

## SUBAREA 5

### Land Use Designation

General Industrial

### Primary Function

The area has railroad access and is substantially developed with many users having rail access. Where possible, future users should be functionally served by rail uses and primarily for manufacturing and warehousing activities. This subarea is located south of AT & SF Railroad to the southern City limit east of Archibald, west of Haven.

### Rail Service Function

It is the City's policy to encourage the development of Rail Service facilities on parcels with strong rail development potential. Standards have been established as a part of this Specific Plan (Part III, Section IV.G.) which require Rail Service development of all parcels which adjoin existing or proposed lead/spur rail lines.

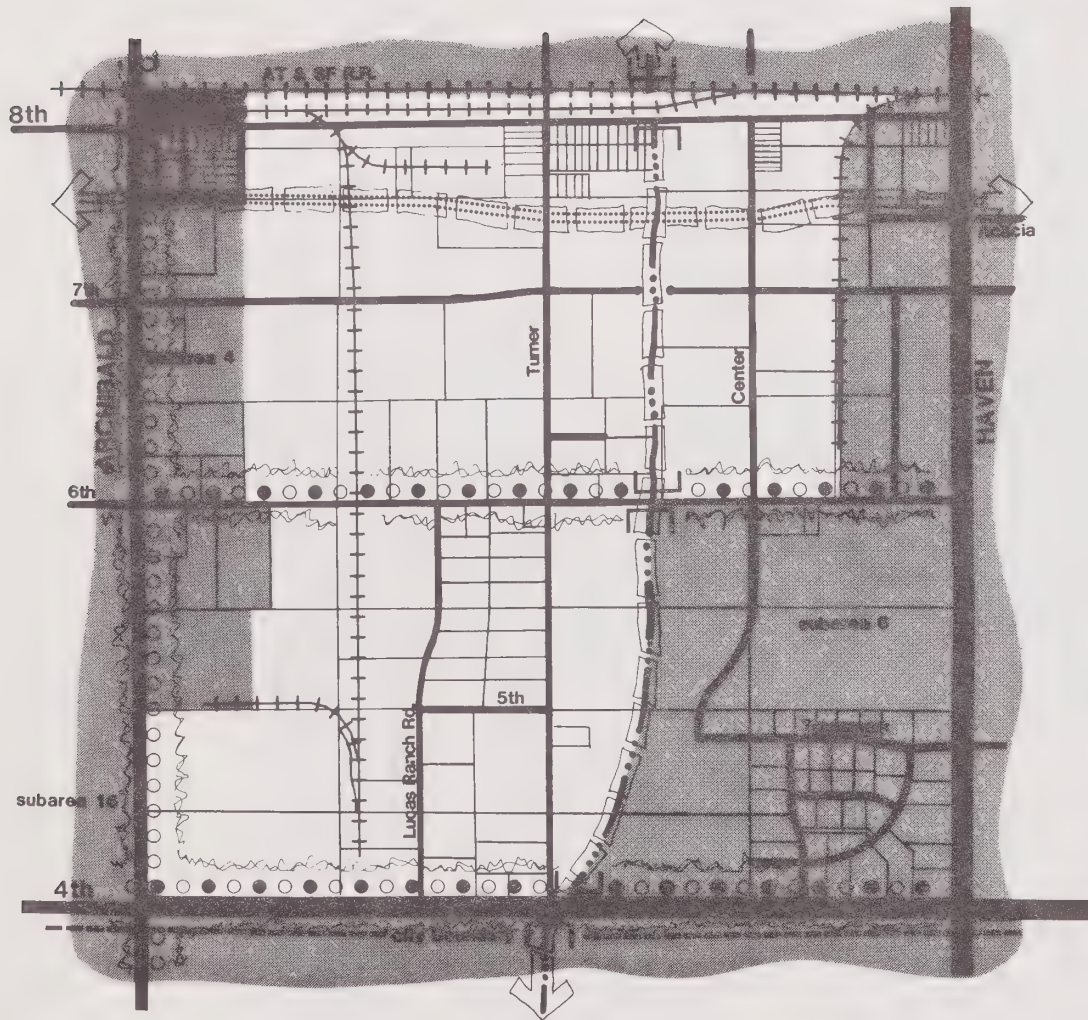
### Permitted Uses

Custom Manufacturing  
Light Manufacturing  
Medium Manufacturing  
Research Services  
Light Wholesale, Storage, and Distribution  
Medium Wholesale, Storage and Distribution  
Agricultural/Nursery Supplies and Services  
Eating and Drinking Establishments  
Repair Services

### Conditional Uses

Public Storage  
Heavy Wholesale, Storage, and Distribution  
Automobile/Light Truck Repair - Minor  
Automobile/Truck Repair - Major  
Building Contractor's Offices and Yards  
Building and Light Equipment Supplies and Sales  
Business Support Services  
Convenience Sales and Services  
Food and Beverage Sales  
Heavy Equipment Sales and Rentals  
Petroleum Products Storage  
Recreation Facilities  
Extensive Impact Utility Facilities  
Public Safety and Utility Services





**FIG. IV-7**  
**SUBAREA 5**

**CIRCULATION**

- 120' R.O.W.
- 100' R.O.W.
- 88' or less R.O.W.

**RAIL SERVICE**

- Existing
- Proposed

**TRAILS/ROUTES**

- Pedestrian
- Bicycle
- Regional Multi-Use
- Special Streetscape/Landscaping
- Power Line/Utility Easement
- Creeks & Channels
- Bridge
- Access Points
- Fire Station <sup>1</sup>
- Sheriff Station



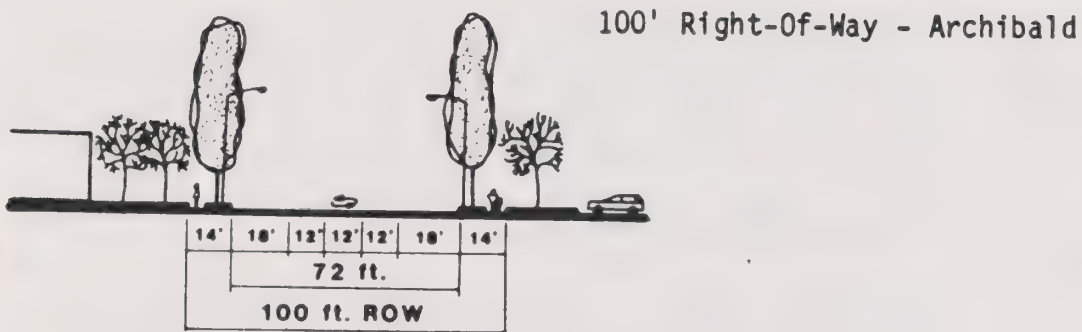
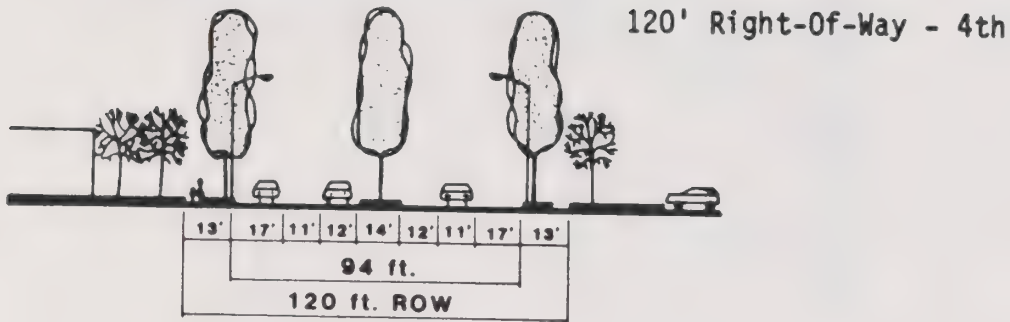
Note: Parcel lines and lot configurations are shown as approximation only.

<sup>1</sup> The sites shown may not be currently owned nor is the location site specific. The depiction of a site is an indication of a projected future need that may be adjusted over time as the City develops.

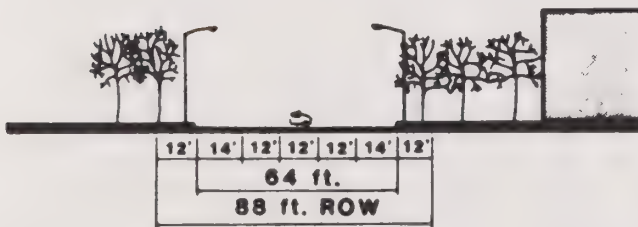


# SUBAREA 5 (Continued)

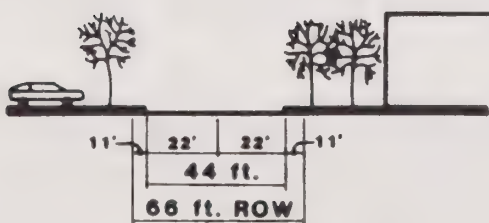
## Access and Circulation



## 88' Right-Of-Way - 6th, Turner



## 66' Right-Of-Way - 7th 8th Center and all other local streets



Minimum Parcel Size

One (1) acre

## SUBAREA 5 (Continued)

Setback Requirements  
(Measured from ultimate  
face of curb)

	Average Landscaping	Parking	Building
9th	45'	25'	45'
6th	"	"	"
Archibald	"	"	"
Turner	35'	20'	35'
7th	25'	15'	25'
8th	"	"	"
Center	"	"	"
All Other			
Local			
Streets	"	"	"

Landscaping Requirements

7% of net lot area

Performance Standards

Noise: The maximum allowable noise level of any use shall not exceed 75L<sub>dn</sub> as measured at the lot line of the lot containing the use. Where a use occupies a lot abutting residentially zoned land, the noise level shall not exceed 65L<sub>dn</sub> as measured at the common lot line. Noise caused by motor vehicles and trains are exempted from this standard.

Vibration: All uses shall be operated so as not to generate vibration discernible without instruments by the average persons beyond the lot upon which the source is located. Vibration caused motor vehicles, trains, and temporary construction or demolition is exempted from this standard.

Particulate Matter and Air Contaminants: In addition to compliance with the AQMD standards, all uses shall be operated so as not to emit particulate matter or air contaminants which are readily detectable without instruments by the average person beyond any lot line of the lot containing such uses.

Odor: All uses shall be operated so as not to emit matter causing unpleasant odors which are perceptible to the average person beyond any lot line of the lot containing such uses.

Humidity, Heat and Glare: All uses shall be operated so as not to produce humidity, heat, glare or high-intensity illumination which is perceptible without instruments by the average person beyond the lot line of any lot containing such uses.

## SUBAREA 5 (Continued)

### Special Consideration:

Parcels which adjoin existing or proposed lead/spur rail lines as indicated on the Subarea Map are required to be developed in accordance with Rail Service Standards (Part III, Section IV.G, Appendix L)

Properties adjacent to residential areas shall have a minimum of 45' building setback from property line on interior lot line and 45' from ultimate face of curbs on abutting streets. Minimum front parking setback shall be 35' for such parcels.

In areas which abut residential uses, screening of industrial buildings, parking, storage and equipment areas is required through the use of walls, fences, and/or berms. The screening, in conjunction with the streetscape setback and landscaping will soften the edge of the industrial development and provide an attractive design element. Screen wall/fence height shall not exceed 3 feet inside the 40' parking setback.


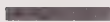

In areas abutting residential uses, access via internal circulation in place of residential streets is encouraged, when feasible, in order to minimize impact upon existing residential, vehicular, and pedestrian circulation patterns.



FIG. IV-8

# SUBAREA 6












## CIRCULATION

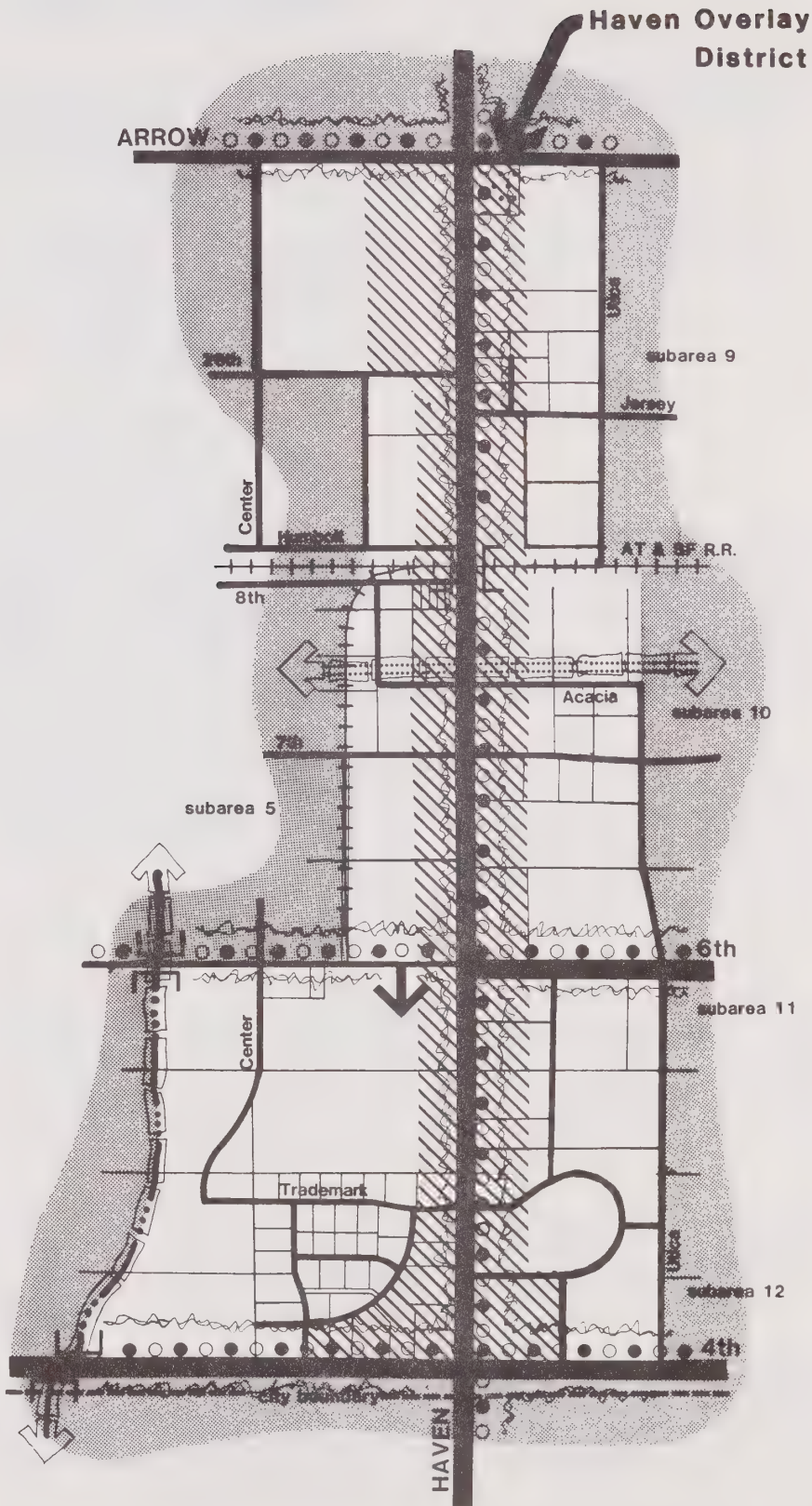
-  120' R.O.W.
-  100' R.O.W.
-  88' or less R.O.W.

## RAIL SERVICE

-  Existing
-  Proposed

## TRAILS/ROUTES

-  Pedestrian
-  Bicycle
-  Regional Multi-Use
-  Special Streetscape/Landscaping
-  Power Line/Utility Easement
-  Creeks & Channels
-  Bridge
-  Access Points
-  <sup>1</sup>Park
-  Fire Station
-  Haven Overlay District



Note: Parcel lines and lot configurations are shown as approximations only.

<sup>1</sup>The sites shown may not be currently owned nor is the location site specific. The depiction of a site is an indication of a projected future need that may be adjusted over time as the City develops.



## SUBAREA 6

### Land Use Designation

Industrial Park

### Primary Function

The function of this area is to provide for office development and support administrative services for industrial related activities. Development in this subarea will provide for the high quality character associated with "Office Park" type development. Subarea 6 is located on both east and west sides of Haven Avenue extending south from Arrow to 4th Street. While some existing manufacturing uses exist around the AT & SF track, the area is substantially undeveloped. In the future, Haven Avenue will serve as a major access and gateway to the City.

### Permitted Uses

Custom Manufacturing  
Light Manufacturing  
Administrative and Office  
Professional/Design Services  
Research Services  
Light Wholesale, Storage and Distribution  
Building Maintenance Services  
Business Supply Retail Sales and Services  
Business Support Services  
Communication Services  
Eating and Drinking Establishments  
Financial, Insurance and Real Estate Services  
Hotel/Motel  
Administrative Civic Services

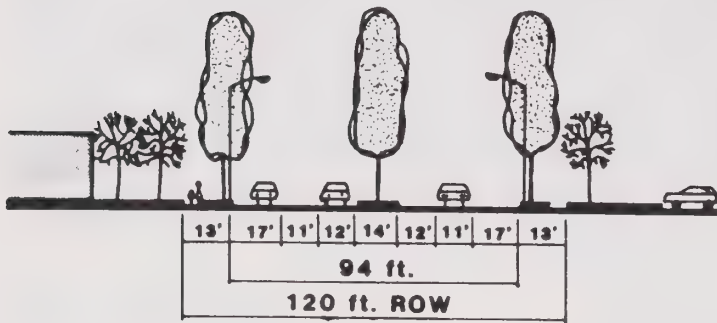
### Conditional Uses

Automotive Rental/Leasing  
Automotive Sales  
Automotive Service Station  
Convenience Sales and Services  
Entertainment  
Fast Food Sales  
Food and Beverage Sales  
Funeral and Crematory Services  
Medical/Health Care  
Personal Services  
Recreational Facilities  
Cultural  
Public Assembly  
Public Safety and Utility Services  
Religious Assembly

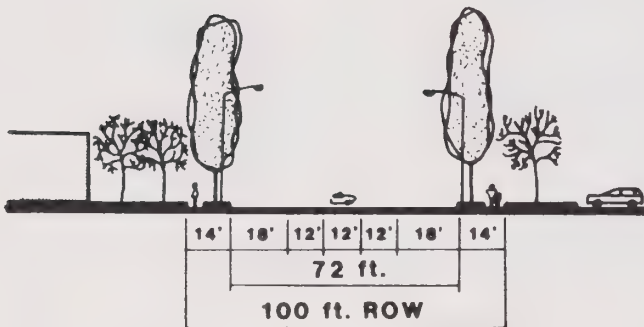
# SUBAREA 6 (Continued)

## Access and Circulation

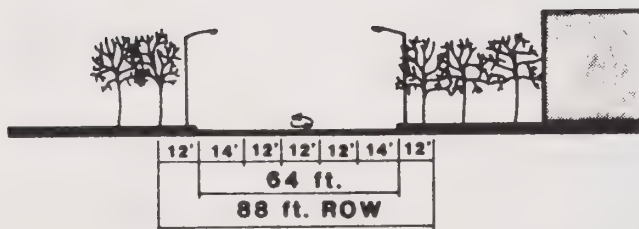
120' Right-Of-Way - Haven  
4th  
6th (East of Haven)



100' Right-Of-Way - Arrow



88' Right-Of-Way - 6th (West of Haven)  
Jersey



## SUBAREA 6 (Continued)

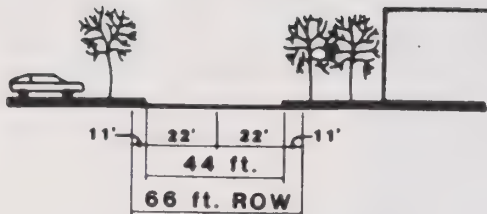
66' Right-Of-Way - 7th

8th

9th

Center

and all other local streets



A median will be constructed to limit left turn movements from Haven Avenue directly onto properties in Subarea 6.

### Minimum Parcel Size

One (1) acre and Two (2) acres on parcels adjacent to Haven Avenue.

### Setback Requirements (Measured from ultimate face of curb)

	Average Landscaping	Parking	Building
Haven	45'	25'	45'
4th	45'	25'	45'
6th	45'	25'	45'
Arrow	45'	25'	45'
Jersey	35'	20'	35'
7th	25'	15'	25'
8th	25'	15'	25'
9th	25'	15'	25'
Center	25'	15'	25'
All Other Local Streets	25'	15'	25'

### Landscape Requirements

15% of the net lot area

### Performance Standards

Noise: The maximum allowable exterior noise level of any use lot shall not exceed  $65_{Ldn}$  as measured by any location on the lot occupied by such uses. Where a structure is occupied by more than one use, the noise level shall not be in excess of  $60_{Ldn}$  as measured within the interior space of the neighboring establishment. Noise caused by motor vehicles are exempted from this standard.

## SUBAREA 6 (Continued)

Vibration: All uses shall be so operated as not to generate vibration discernible without instruments by the average person while on or beyond the lot upon which the source is located or within an adjoining enclosed space if more than one establishment occupies a structure. Vibration caused by motor vehicles, trains, and temporary construction or demolition work is exempted from this standard.

Particulate Matter and Air Contaminants: In addition to compliance with the Air Quality Maintenance District (AQMD) standards, all uses shall be operated so as not to emit particulate matter or air contaminants which are readily detectable without instruments by the average person while on the lot containing such uses.

Odor: All uses shall be operated so as not to emit matter causing unpleasant odors which are perceptible to the average person while within or beyond the lot containing such uses.

Humidity, Heat, and Glare: All uses shall be operated so as not to produce humidity, heat, glare, or high-intensity illumination which is perceptible without instruments by the average person while on or beyond the lot containing the use.

### Special Considerations

A potential future fire station location has been indicated in the general proximity east of Haven, south of AT&SF rail tracks.

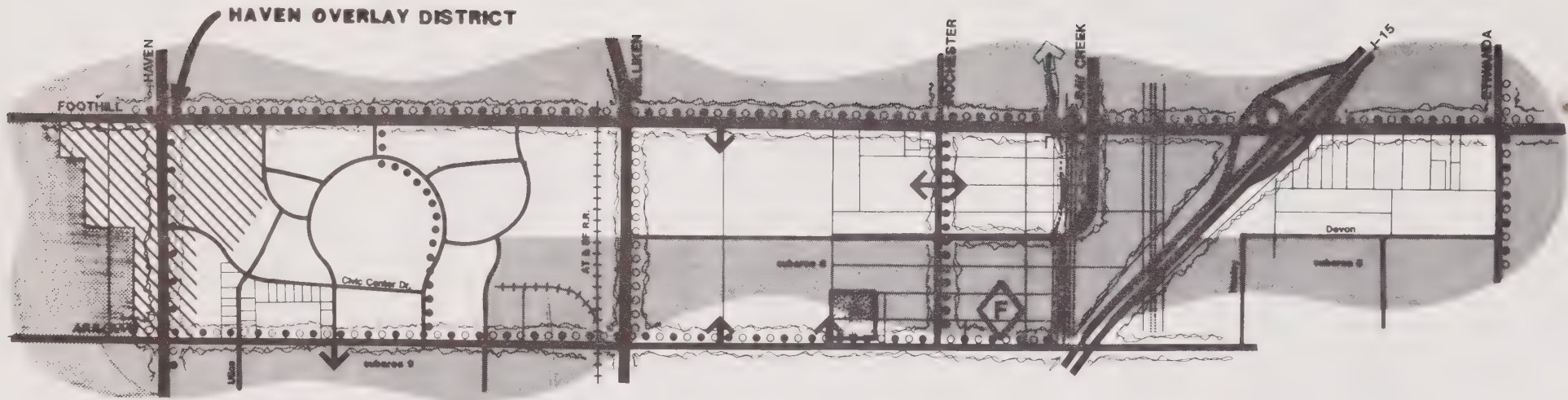
For property within the boundaries of the Haven Avenue Overlay District, refer to the Overlay District at the beginning Part IV for a modified list of permitted land uses and special development criteria.



FIG. IV-9

# SUBAREA 7

IV-47



## CIRCULATION

- 120' R.O.W.
- 100' R.O.W.
- 88' or less R.O.W.

## RAIL SERVICE

- Existing
- Proposed

## TRAILS/ROUTES

- Pedestrian
- Bicycle
- Regional Multi-Use
- Special Streetscape/Landscaping
- Power Line/Utility Easement

Creeks & Channels

Bridge

Access Points

Park<sup>1</sup>

Fire Station<sup>1</sup>

0 400' 800' 1600'

Note: Parcel lines and lot configurations are shown as approximation only.

<sup>1</sup>The sites shown may not be currently owned nor is the location site specific. The depiction of a site is an indication of a projected future need that may be adjusted over time as the City develops.

## SUBAREA 7

### Land Use Designation

Industrial Park

### Primary Function

Subarea 7 occupies an area directly south of Foothill Boulevard which represents an important land use edge between the City's Industrial Area and community oriented non-industrial area and is a gateway to the City. A major industrial, office, and commercial development on approximately 300 acres is currently undergoing phased construction. Within this area is a planned Civic Center which will include San Bernardino County and City offices.

### Permitted Uses

Custom Manufacturing  
Light Manufacturing  
Administrative and Office  
Professional/Design Services  
Research Services  
Light Wholesale, Storage, Distribution  
Building Maintenance Services  
Business Supply Retail Sales and Services  
Business Support Services  
Communication Services  
Eating and Drinking Establishments  
Financial, Insurance and Real Estate Services  
Hotel/Motel  
Administrative Civic Services  
Flood Control/Utility Corridor

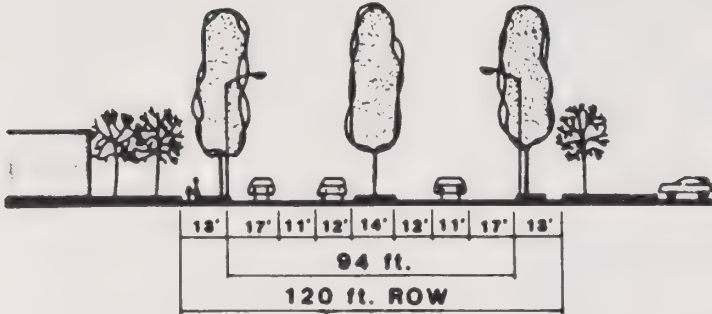
### Conditional Uses

Automotive Rental/Leasing  
Automotive Sales  
Automotive Service Station  
Convenience Sales and Services  
Entertainment  
Fast Food Sales  
Food and Beverage Sales  
Medical/Health Care Services  
Personal Services  
Recreation Facilities  
Cultural  
Public Assembly  
Public Safety and Utility Services  
Religious Assembly

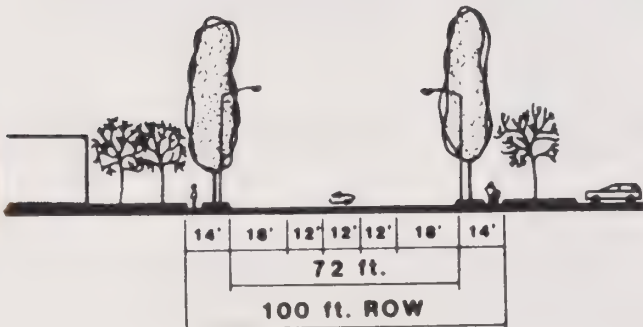
## SUBAREA 7 (Continued)

### Access and Circulation

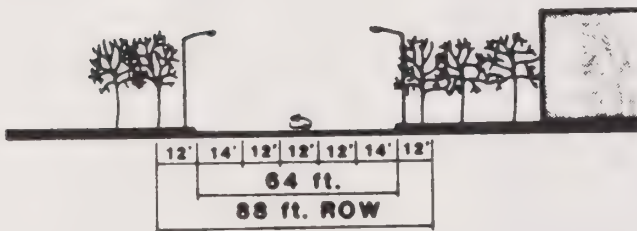
120' Right-Of-Way - Haven  
Foothill  
Milliken  
Day Creek



100' Right-Of-Way - Arrow (No median on Arrow)  
Rochester  
Etiwanda

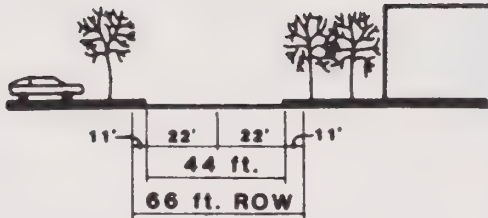


88' Right-Of-Way - Cleveland



## SUBAREA 7 (Continued)

### 66' Right-Of-Way - All Local Streets



A median will be constructed to limit left turn movement from Haven, Foothill, and Milliken directly onto properties in Subarea 7.

#### Minimum Parcel Size

One (1) acre and two (2) acres on parcels adjacent to Foothill and Haven

#### Setback Requirements (Measured from ultimate face of curb)

	Average Landscaping	Parking	Building
Haven	45'	25'	45'
Foothill	45'	25'	45'
Milliken	45'	25'	45'
Day Creek	45'	25'	45'
Arrow	45'	25'	45'
Rochester	45'	25'	45'
Etiwanda	45'	25'	45'
Cleveland	35'	20'	35'
All Other Local Streets	25'	15'	25'

#### Landscape Standards

15% of net lot area

#### Performance Requirements

Noise: The maximum allowable exterior noise level of any use shall not exceed 65  $L_{dn}$  as measured by any location on the lot occupied by such uses. Where a structure is occupied by more than one use, the noise level shall not be in excess of 60  $L_{dn}$  as measured within the interior space of the neighboring establishment. Noise caused by motor vehicles are exempted from this standard.



## SUBAREA 7 (Continued)

Vibration: All uses shall be so operated as not to generate vibration discernible without instruments by the average person while on or beyond the lot upon which the source is located or within an adjoining enclosed space if more than one establishment occupies a structure. Vibration caused by motor vehicles, trains, and temporary construction or demolition work is exempted from this standard.

Particulate Matter and Air Contaminants: In addition to compliance with the Air Quality Maintenance District (AQMD) standards, all uses shall be operated so as not to emit particulate matter or air contaminants which are readily detectable without instruments by the average person while on the lot containing such uses.

Odor: All uses shall be operated so as not to emit matter causing unpleasant odors which are perceptible to the average person while within or beyond the lot containing such use.

Humidity, Heat, and Glare: All uses shall be operated so as not to produce humidity, heat, glare, or high-intensity illumination which is perceptible without instruments by the average person while on or beyond the lot containing the use.

### Special Consideration

Foothill Boulevard is a major arterial which is projected to carry a significant volume of traffic. In order to limit vehicular access problems, the minimum parcel size requirement along Foothill is 2 acres and minimum parcel width is 200'.

For property within the boundaries of the Haven Avenue Overlay District, refer to the Overlay District for a modified list of permitted land uses and special development criteria.

The San Bernardino County Law and Justice Center near the corner of Foothill Boulevard and Haven Avenue may include a detention facility if municipal court facilities are provided.

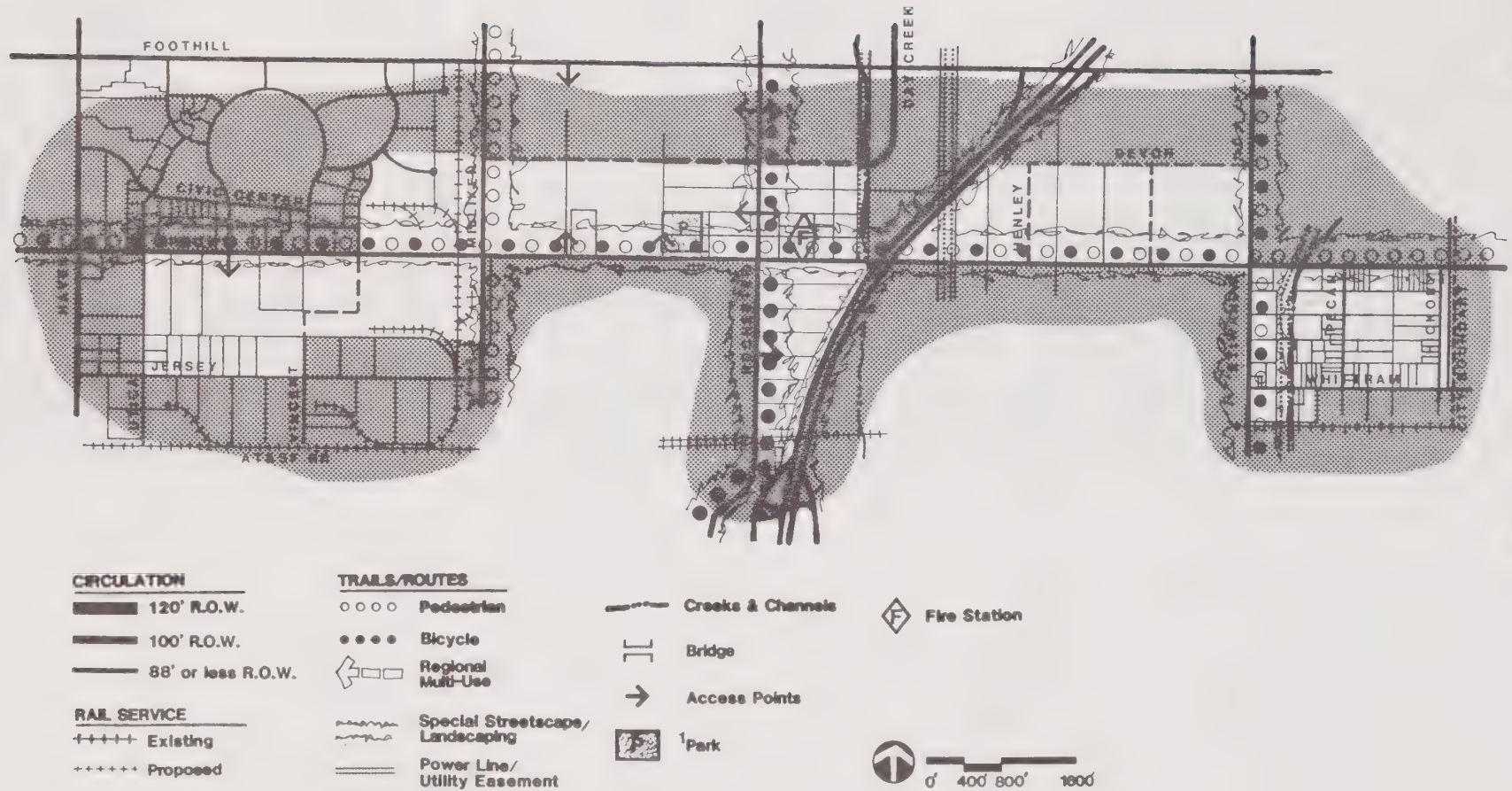
#### SUBAREA 7 (Continued)

On the east side of Haven Avenue, north of Arrow Highway, Development Code provisions for the General Commercial District shall apply to K-Mart and the adjoining northerly building. Development and use of satellite buildings in the K-Mart Center are subject to provisions of the Industrial Area Specific Plan.

To preserve and enhance the image of the community special considerations shall be given to the quality of site design, architecture, and landscaping of all properties adjacent to the I-15 Freeway. Attractive screening of outdoor work, loading, storage areas, roof and ground mounted equipment from significant freeway points of view shall be required.

FIG. IV-10

## SUBAREA 8



Note: Parcel lines and lot configurations are shown as approximation only.

<sup>1</sup>The sites shown may not be currently owned nor is the location site specific. The depiction of a site is an indication of a projected future need that may be adjusted over time as the City develops.



## SUBAREA 8

### Land Use Designation

General Industrial

### Primary Function

This area functions to provide for General Industrial activities and to assure for a transition area from the Heavy Industrial category located north of this subarea. North of Arrow and west of Milliken, the industrial uses should be allowed to continue and expand with all service according to the development standards of the plan. Subarea 8 extends north of Arrow approximately 1000' east of Cleveland to the east Plan boundary including a portion south of Arrow along the eastern plan boundary.

### Permitted Uses

Custom Manufacturing  
Light Manufacturing  
Research Services  
Public Storage  
Light Wholesale, Storage, and Distribution  
Medium Wholesale, Storage and Distribution  
Agricultural/Nursery Supplies and Services  
Automotive/Light Truck Repair-Minor  
Automotive/Truck Repair-Major  
Building Contractor's Offices and Yards  
Building Maintenance Services  
Building Supplies and Light Equipment Sales  
Business Supply Retail Sales and Services  
Business Support Services  
Communication Services  
Eating and Drinking Establishments  
Laundry Services  
Recreation Facilities  
Repair Services  
Administrative Civic Services  
Flood Control/Utility Corridor

### Conditional Uses

Medium Manufacturing  
Administrative and Office  
Professional/Design Services  
Animal Care  
Automotive Fleet Storage  
Automotive Rental/Leasing  
Automotive Service Station  
Convenience Sales and Services  
Entertainment  
Fast Food Sales  
Financial, Insurance and Real Estate Services  
Food and Beverage Sales  
Heavy Equipment Sales and Rentals  
Medical/Health Care Services  
Personal Services

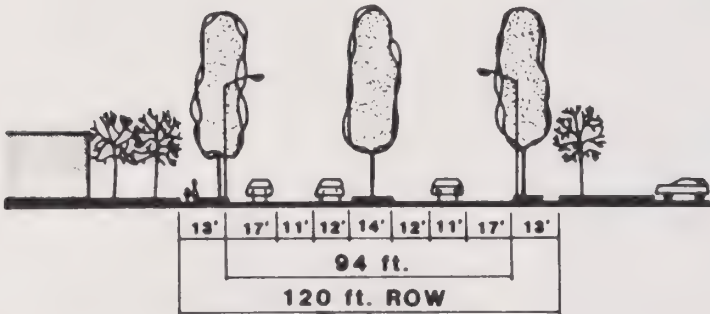


# SUBAREA 8 (Continued)

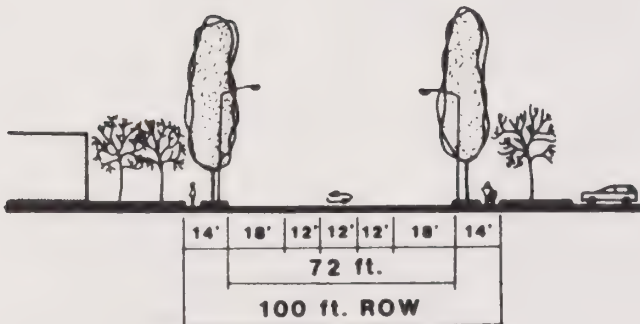
Petroleum Products Storage  
Public Assembly  
Public Safety and Utility Services  
Religious Assembly

## Access and Circulation

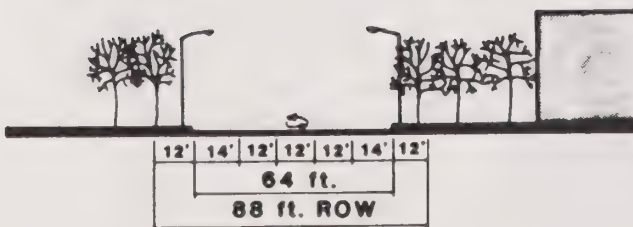
120' Right-Of-Way - Day Creek Boulevard  
Milliken



100' Right-Of-Way - Arrow (No median on Arrow)  
Etiwanda  
Rochester

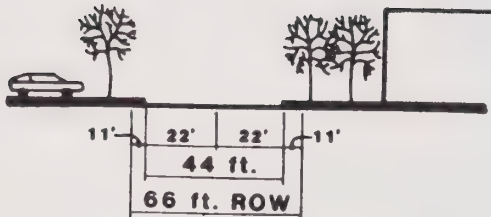


88' Right-Of-Way - Jersey



## SUBAREA 8 (Continued)

### 66' Right-Of-Way - All other Local Streets



#### Minimum Parcel Size

One-Half (1/2) acre

#### Setback Requirements (Measured from ultimate face of curb)

	Average Landscaping	Parking	Building
Day Creek Blvd.	45'	25'	45'
Milliken	45'	25'	45'
Arrow	45'	25'	45'
Rochester	45'	25'	45'
Etiwanda	45'	25'	45'
Jersey	35'	20'	35'
All Other Local Streets	25'	15'	25'

#### Landscaping Requirements

12% of net lot area

#### Performance Standards

Noise: The maximum allowable noise level of any use shall not exceed 75 Ldn as measured at the lot line of the lot containing the use. Where a use occupies a lot abutting residentially zoned land, the noise level shall not exceed 65 Ldn as measured at the common lot line. Noise caused by motor vehicles and trains are exempted from this standard.

Vibration: All uses shall be operated so as not to generate vibration discernible without instruments by the average persons beyond the lot upon which the source is located. Vibration caused motor vehicles, trains, and temporary construction or demolition is exempted from this standard.

## SUBAREA 8 (Continued)

Particulate Matter and Air Contaminants: In addition to compliance with the AQMD standards, all uses shall be operated so as not to emit particulate matter or air contaminants which are readily detectable without instruments by the average person beyond any lot line of the lot containing such uses.

Odor: All uses shall be operated so as not to emit matter causing unpleasant odors which are perceptible to the average person beyond any lot line of the lot containing such uses.

Humidity, Heat and Glare: All uses shall be operated so as not to produce humidity, heat, glare or high-intensity illumination which is perceptible without instruments by the average person beyond the lot line of any lot containing such uses.

### Special Consideration

Due to the potential conflicts between uses within this subarea and the uses within the Industrial Park Category of adjacent Subarea 6 & 7, building setback from abutting property for Medium Manufacturing and greater intensity shall be no less than 45'. Class C Performance Standards of the adjoining Minimum Impact/Heavy Industrial area (Subarea 9), allow existing businesses and future Industrial uses which produce noise, particulate matter and air contaminants, vibration, odor, humidity, heat, glare or high intensity. Uses sensitive to these conditions, such as precision manufacturing or office projects, should consider these factors prior to location in the vicinity of Minimum Impact/Heavy Industrial property.

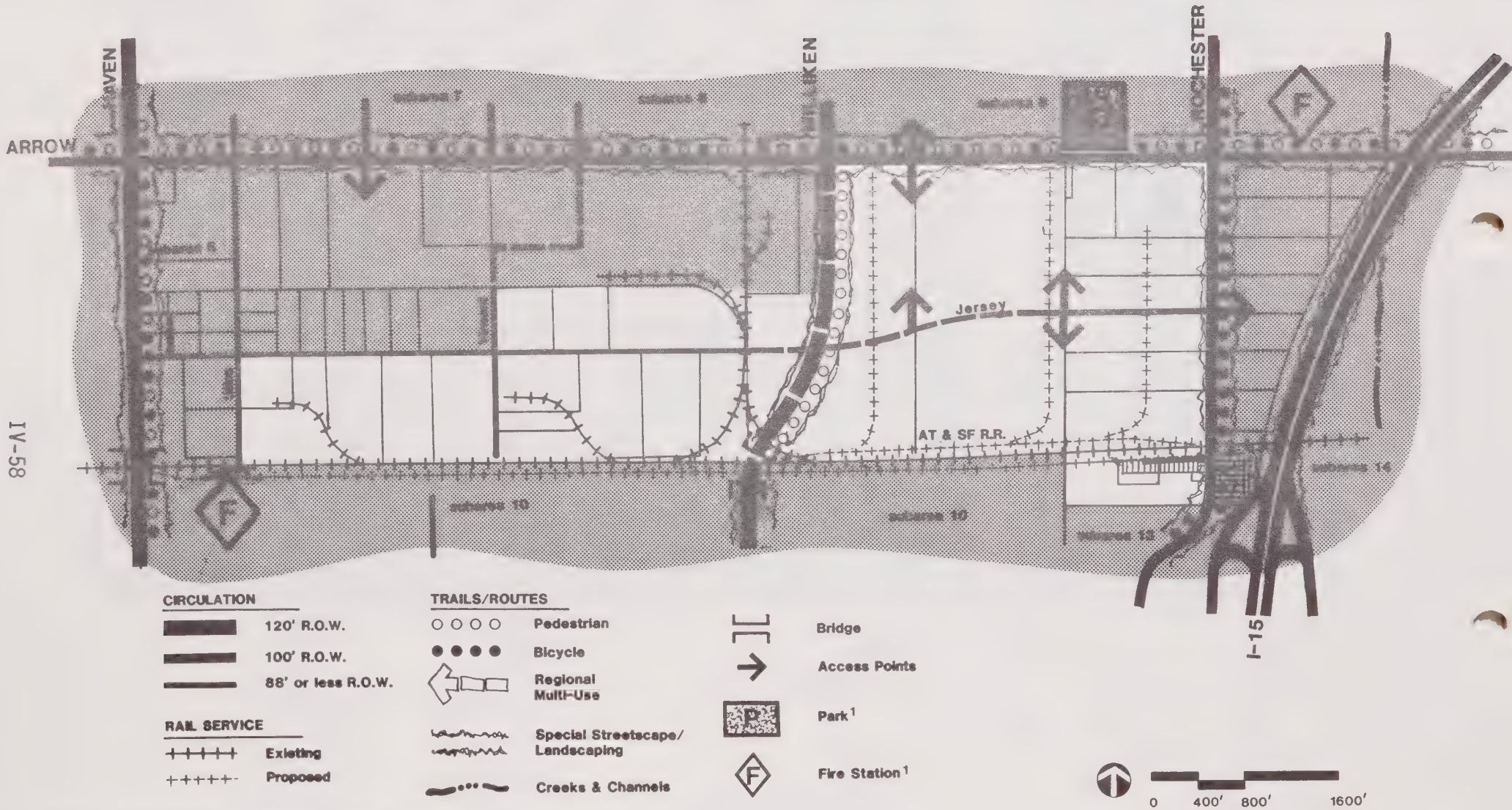
Both the potential park site location and fire station location are shown to indicate approximate location.

Development within 600' of the I-15 Freeway shall have outdoor storage areas completely screened from view along the freeway. Because of the existing rail service, users with rail service needs are permitted within the area west of Milliken Avenue.

To preserve and enhance the image of the community special considerations shall be given to the quality of site design, architecture, and landscaping of all properties adjacent to the I-15 Freeway. Attractive screening of outdoor work, loading, storage areas, roof and ground mounted equipment from significant freeway points of view shall be required.



FIG. IV-11  
SUBAREA 9



Note: Parcel lines and lot configurations are shown as approximation only.

<sup>1</sup>The sites shown may not be currently owned nor is the location site specific. The depiction of a site is an indication of a projected future need that may be adjusted over time as the City develops.



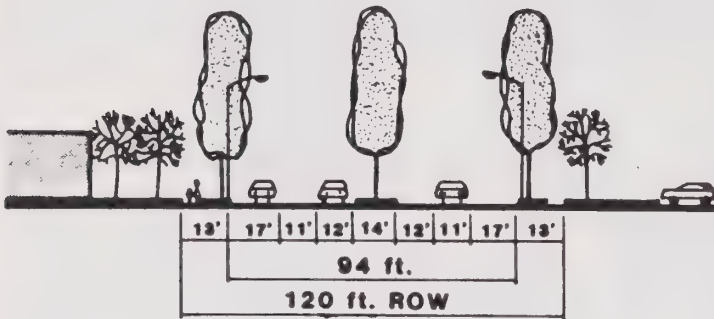
## SUBAREA 9

General Plan Designation	Minimum Impact Heavy Industrial
Primary Function	<p>The function of this subarea is to provide for the continuation of the existing uses and to provide the opportunity of the expansion of manufacturing and warehousing activity similar and compatible with the existing uses.</p> <p>This subarea extends from Arrow Route south to AT&amp;SF tracks, east of Haven and west of Devore Freeway including industrial users south of AT&amp;SF tracks west of Devore Freeway. The subarea is traversed by the main line of AT&amp;SF railroad on the south with several rail spurs serving the subarea. Substantial portions of the area around Jersey Avenue are developed with both medium and some heavy industrial users.</p>
Rail Service Function	<p>It is the City's policy to encourage the development of Rail Service facilities on parcels with strong rail development potential. Standards have been established as a part of this Specific Plan (Part III, Section IV.G) which require Rail Service development on all parcels which adjoin existing or proposed lead/spur rail lines.</p>
Permitted Uses	<p>Medium Manufacturing Minimum Impact Heavy Industrial Manufacturing Light Wholesale, Storage, and Distribution Medium Wholesale, Storage and Distribution Heavy Wholesale, Storage, and Distribution Building and Light Equipment Supplies and Sales Eating and Drinking Establishments Repair Services Flood Control/Utility Corridor</p>
Conditional Uses	<p>Public Storage Automotive Fleet Storage Business Support Services Communication Services Convenience Sales and Services Food and Beverage Sales Heavy Equipment Sales and Rentals Petroleum Products Storage Recreational Facilities Extensive Impact Utility Facilities Public Safety and Utility Services</p>

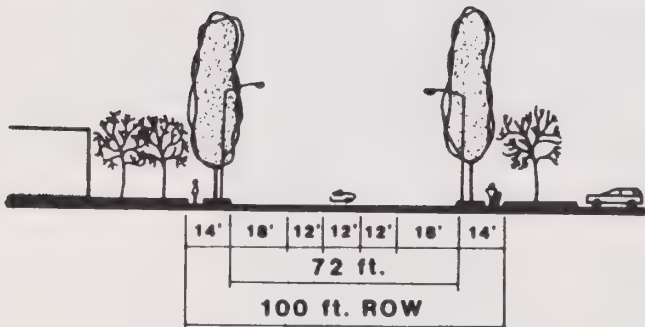
SUBAREA 9 (Continued)

Access and Circulation

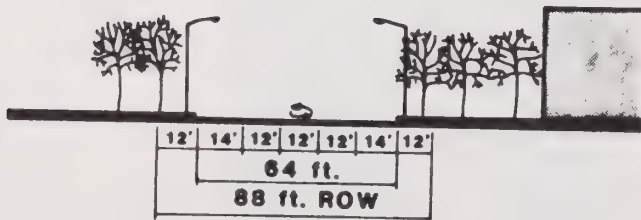
120' Right-of-Way - Milliken



100' Right-of-Way - Rochester  
Arrow (No median on Arrow)



88' Right-of-Way - Cleveland  
Jersey



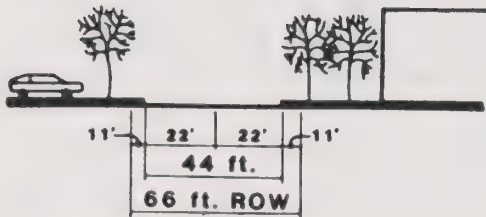
## SUBAREA 9 (Continued)

66' Right-of-Way - 7th

Vincent

Utica

and all other local streets



Minimum Parcel Size

Two (2) acres in the area between Arrow Highway extending 1000' south, five (5) acres in remaining areas

Setback Requirements  
(Measured from ultimate face of curb)

	Average Landscaping	Parking	Building
Milliken	45'	25'	45'
Arrow	"	"	45'
Rochester	"	"	45'
Cleveland	35'	20'	35'
Jersey	"	"	35'
7th	25'	15'	25'
8th	"	"	25'
9th	"	"	25'
Vincent, Utica, and all other local streets	"	"	25'

Landscape Requirements

10% of the net lot area in the area between Arrow Highway extending 1000' south

5% of the net lot area in the remaining area

## SUBAREA 9 (Continued)

### Performance Standards

Noise: The maximum allowable noise level of any use shall not exceed 85<sub>Ldn</sub> as measured at the lot line of the lot containing the use. Where a use is within 200 feet of residentially zoned land, the noise level shall not exceed 65<sub>Ldn</sub>. Where a use occupies a lot abutting or separated by a street from a lot within the designated Class A or B performance standard or residential property, the performance standard of the abutting property shall apply at the common or facing lot line.

Vibration: All uses shall be operated so as not to generate vibration discernible without instruments by the average person beyond 600' from where the source is located. Vibration caused by motor vehicles, trains, and temporary construction and demolition is exempted from this standard.

Particulate Matter and Air Contaminants: In addition to compliance with the Air Quality Maintenance District (AQMD) standards, all uses shall be operated so as not to emit particulate matter or air contaminants which: (a) are injurious to the health of either persons engaged in or related to the use of the lot, or persons residing, working, visiting, or recreating in neighboring areas; (b) substantially and adversely affect the maintenance of property in nearby areas; (c) are disruptive of industrial processes carried on in other parts of the industrial area. Where a use occupies a lot abutting or separated by a street lot with designated Class A or B performance standard for particulate matter and air contaminants shall apply at the common or facing lot line.

Odor: All uses shall be operated so as not to emit matter causing unpleasant odors which are perceptible to the average person 600' beyond any lot line of the lot containing such uses.

Humidity, Heat, and Glare: All uses shall be operated so as not to produce humidity, heat, glare, or high-intensity illumination which is perceptible without instruments by the average person while on any lot zoned for residential purposes or any industrial property with Class A or B performance standard designation.



## SUBAREA 9 (Continued)

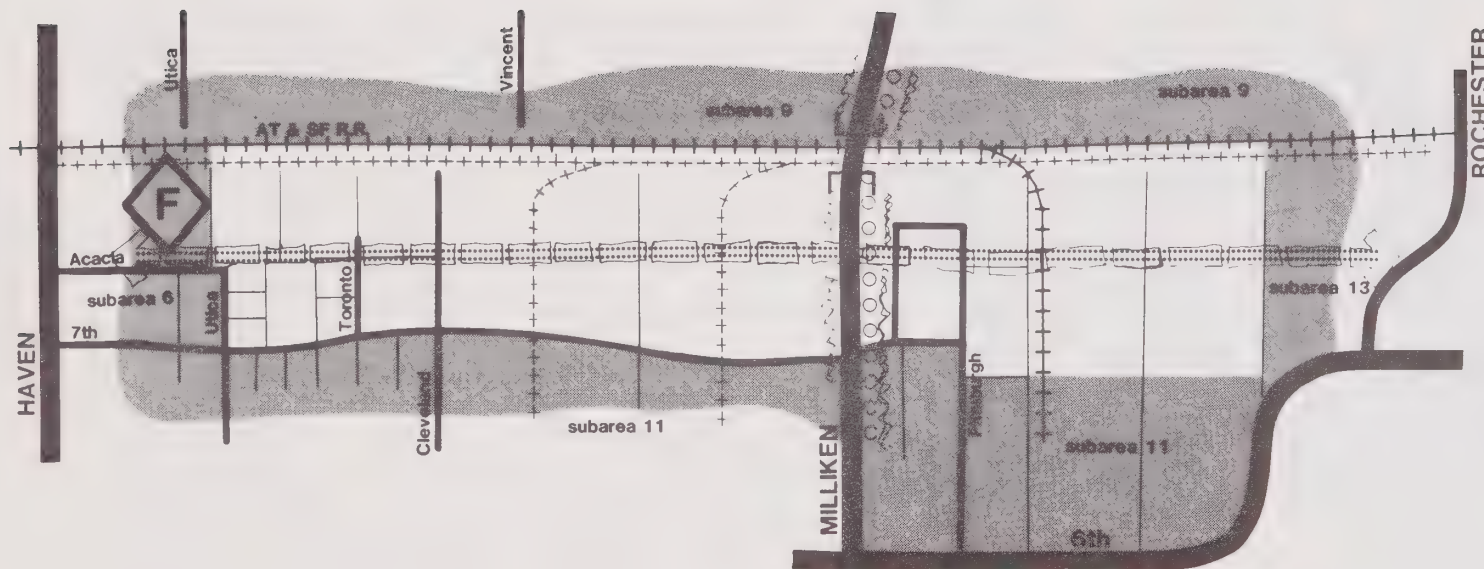
### Special Consideration

Parcels which adjoin existing or proposed lead/spur rail lines as indicated on the Subarea Map are required to be developed in accordance with Rail Service Standards (Part III, Section IV.G; Appendix D).

To preserve and enhance the view of the community to travelers, special consideration shall be given to the quality of site design, architecture, and landscaping of all properties adjacent to the I-15 Freeway. Attractive screening of outdoor work, loading, storage areas, roof and ground mounted equipment from significant freeway points of view shall be required.




Outdoor storage within 600' of the Devore Freeway is discouraged; however, where necessary it shall be completely screened from view along the freeway.

To mitigate potential land use conflicts, new development within this subarea abutting Subareas 6 and 7 (Industrial Park Category) shall provide a minimum 45' building setback to the abutting property line on interior lot lines abutting residential districts and 45' from ultimate face of curb on abutting streets.





**FIG. IV-12**  
**SUBAREA 10**

**CIRCULATION**

-  120' R.O.W.
-  100' R.O.W.
-  88' or less R.O.W.

**RAIL SERVICE**

-  Existing
-  Proposed

**TRAILS/ROUTES**

-  Pedestrian
-  Bicycle
-  Regional Multi-Use
-  Special Streetscape/Landscaping
-  Power Line/Utility Easement
-  Creeks & Channels
-  Bridge
-  Access Points
-  Fire Station<sup>1</sup>
-  Sheriff Station



Note: Parcel lines and lot configurations are shown as approximation only.

<sup>1</sup> The sites shown may not be currently owned nor is the location site specific. The depiction of a site is an indication of a projected future need that may be adjusted over time as the City develops.

## SUBAREA 10

### Land Use Designation

General Industrial

### Primary Function

The subarea has direct access to rail service and should serve to locate distribution facilities in need of rail service. Subarea 10 is located south of the AT&SF railroad tracks to approximately 7th Street, east of Haven and west of existing Rochester.

### Rail Service Function

It is the City's policy to encourage the development of Rail Service facilities on parcels with strong rail development potential. Standards have been established as a part of this Specific Plan (Part III, Section IV.G) which requires Rail Service development on all parcels which adjoin existing or proposed lead/spur rail lines.

### Permitted Uses

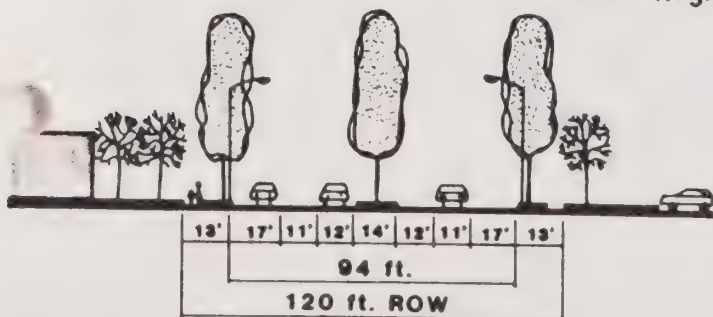
Custom Manufacturing  
Light Manufacturing  
Medium Manufacturing  
Research Services  
Light Wholesale, Storage, and Distribution  
Medium Wholesale, Storage and Distribution  
Agricultural/Nursery Supplies and Services  
Eating and Drinking Establishments  
Repair Services  
Flood Control/Utility Corridor

### Conditional Uses

Public Storage  
Heavy Wholesale, Storage, and Distribution  
Building and Light Equipment Supplies and Sales  
Business Support Services  
Convenience Sales and Services  
Food and Beverage Sales  
Heavy Equipment Sales and Rentals  
Petroleum Products Storage  
Recreation Facilities  
Extensive Impact Utility Facilities  
Public Safety and Utility Services

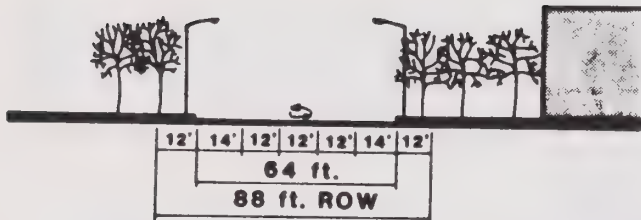
### Access and Circulation

120' Right-of-Way - Milliken

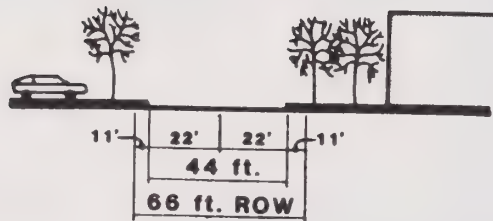


## SUBAREA 10 (Continued)

### 88' Right-of-Way - Cleveland



### 66' Right-of-Way - Old Rochester 7th Street All other Local Streets



Minimum Parcel Size

One (1) acre

Setback Requirements  
(Measured from ultimate face of curb)

	Average Landscaping	Parking	Building
Milliken	45'	25'	45'
Cleveland	35'	20'	35'
(Old) Rochester	25'	15'	25'
7th and all other local streets	"	"	"

Landscaping Required

10% of the net lot area

Performance Standards

Noise: The maximum allowable noise level of any use shall not exceed 75<sub>Ldn</sub> as measured at the lot line of the lot containing the use. Where a use occupies a lot abutting residentially zoned land, the noise level shall not exceed 65<sub>Ldn</sub> as measured at the common lot line. Noise caused by motor vehicles and trains are exempted from this standard.



## SUBAREA 10 (Continued)

Vibration: All uses shall be operated so as not to generate vibration discernible without instruments by the average person beyond the lot upon which the source is located. Vibration caused by motor vehicles, trains, and temporary construction or demolition is exempted from this standard.

Particulate Matter and Air Contaminants: In addition to compliance with the Air Quality Maintenance District (AQMD) standards, all uses shall be operated so as not to emit particulate matter or air contaminants which are readily detectable without instruments by the average person beyond any lot line of the lot containing such uses.

Odor: All uses shall be operated so as not to emit matter causing unpleasant odors which are perceptible to the average person beyond any lot line of the lot containing such uses.




Humidity, Heat and Glare: All uses shall be operated so as not to produce humidity, heat, glare or high-intensity illumination which is perceptible without instruments by the average person beyond the lot line of any lot containing such uses.

### Special Considerations:



Parcels which adjoin existing or proposed lead/spur rail lines as indicated on the Subarea Map are required to be developed in accordance with Rail Service Standards (Part III, Section G, Appendix D).

**FIG. IV-13**  
**SUBAREA 11**




**CIRCULATION**

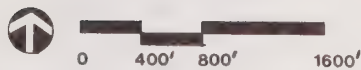
-  120' R.O.W.
-  100' R.O.W.
-  88' or less R.O.W.

**RAIL SERVICE**

-  Existing
-  Proposed

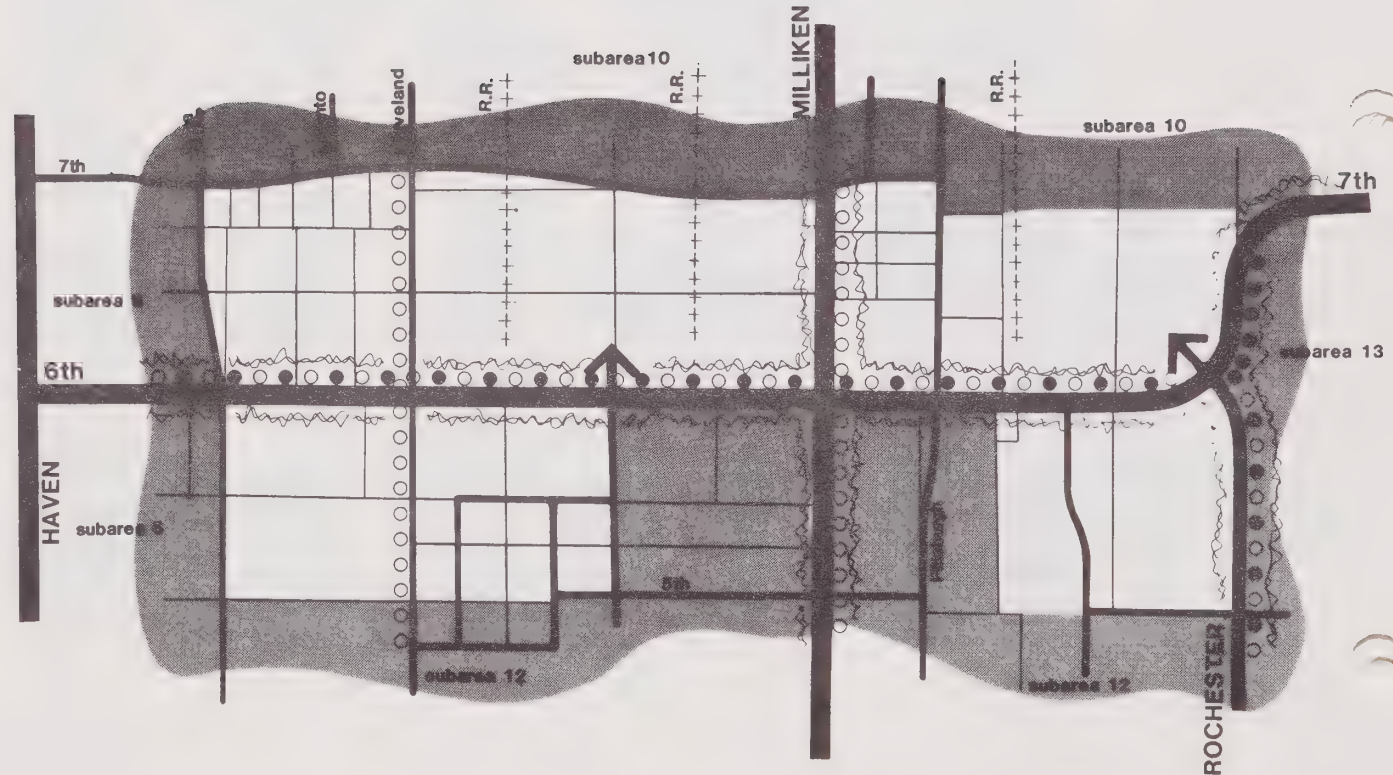
**TRAILS/ROUTES**

-  Pedestrian
-  Bicycle
-  Regional Multi-Use
-  Special Streetscape/Landscaping
-  Power Line/Utility Easement
-  Creeks & Channels
-  Bridge
-  Access Points
-  Fire Station<sup>1</sup>
-  Sheriff Station



Note: Parcel lines and lot configurations are shown as approximation only.

<sup>1</sup> The sites shown may not be currently owned nor is the location site specific. The depiction of a site is an indication of a projected future need that may be adjusted over time as the City develops.



## SUBAREA 11

### Land Use Designation

General Industrial

### Primary Function

This area should provide for the light and medium manufacturing and distribution activities. Rail served activities would be permitted north of 6th Street. This area serves as a transition from the industrial park activities to the greater range of industrial use in both general and heavy industrial land use categories. Subarea 11 is located east of Haven and west of (old) Rochester, south of 7th Street and extending to approximately the future alignment of 5th Street with the exception of a portion next to Milliken. Approved projects for the area provide for light industrial and warehousing distribution facilities.

### Rail Serve Function

Portions of the Subarea have lead/spur rail lines to accommodate Rail Service needs of industrial uses. It is the City's policy to encourage the development of Rail Service facilities on parcels with strong rail development potential. Standards have been established as a part of this Specific Plan (Part III, Section IV.G) which require Rail Service development on all parcels which adjoin existing or proposed lead/spur rail lines.

### Permitted Uses

Custom Manufacturing  
Light Manufacturing  
Medium Manufacturing  
Research Services  
Light Wholesale, Storage, and Distribution  
Medium Wholesale, Storage and Distribution  
Agricultural/Nursery Supplies and Services  
Automotive Rental/Leasing  
Automotive/Light Truck Repair-Minor  
Automotive/Truck Repair-Major  
Building Contractor's Offices and Yards  
Building Maintenance Services  
Building and Light Equipment Supplies and Sales  
Building Supply Retail Sales and Services  
Business Support Services  
Communication Services  
Eating and Drinking Establishments  
Laundry Services  
Repair Services  
Administrative Civic Services



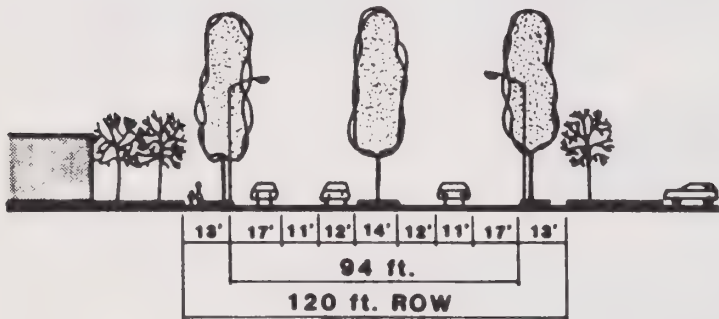
## SUBAREA 11 (Continued)

### Conditional Uses

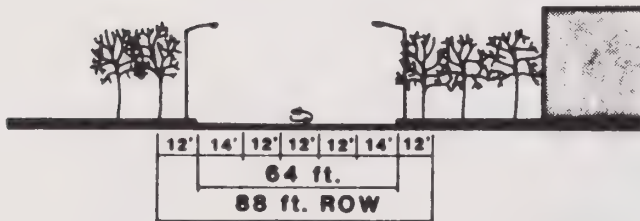
Administrative and Office  
 Professional/Design Services  
 Public Storage  
 Animal Care  
 Automotive Fleet Storage  
 Automotive Service Station  
 Convenience Sales and Services  
 Fast Food Sales  
 Financial, Insurance and Real Estate Services  
 Food and Beverage Sales  
 Heavy Equipment Sales and Rentals  
 Medical/Health Care Services  
 Personal Services  
 Petroleum Products Storage  
 Recreation Facilities  
 Extensive Impact Utility Facilities  
 Public Assembly  
 Public Safety and Utility Services  
 Religious Assembly

### Access and Circulation

120' Right-Of-Way - 6th Street  
 Milliken  
 Cleveland



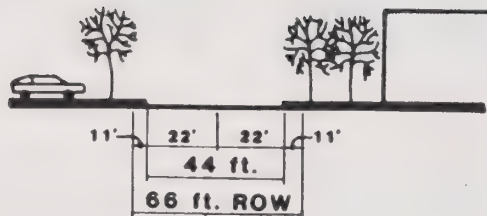
88' Right-Of-Way - Cleveland





# SUBAREA 11 (Continued)

66' Right-Of-Way - 7th Street (west of Rochester)  
All other Local Streets



Minimum Parcel Size

One-half (1/2) acre

Setback Requirements  
(Measured from ultimate face of curb)

	Average Landscaping	Parking	Building
6th	45'	25'	45'
Milliken	"	"	"
New Rochester	"	"	"
Cleveland	35'	20'	35'
7th and all other local streets	25'	15'	25'

Landscaping Requirements

12% of the net lot area

Performance Standards

Noise: The maximum allowable noise level of any use shall not exceed 75<sub>Ldn</sub> as measured at the lot line of the lot containing the use. Where a use occupies a lot abutting residentially zoned land, the noise level shall not exceed 65<sub>Ldn</sub> as measured at the common lot line. Noise caused by motor vehicles and trains are exempted from this standard.

Vibration: All uses shall be operated so as not to generate vibration discernible without instruments by the average persons beyond the lot upon which the source is located. Vibration caused by motor vehicles, trains, and temporary construction or demolition is exempted from this standard.

## SUBAREA 11 (Continued)

Particulate Matter and Air Contaminants: In addition to compliance with Air Quality Maintenance District (AQMD) standards, all uses shall be operated so as not to emit particulate matter or air contaminants which are readily detectable without instruments by the average person beyond any lot line of the lot containing such uses.

Odor: All uses shall be operated so as not to emit matter causing unpleasant odors which are perceptible to the average person beyond any lot line of the lot containing such uses.

Humidity, Heat and Glare: All uses shall be operated so as not to produce humidity, heat, glare or high-intensity illumination which is perceptible without instruments by the average person beyond the lot line of any lot containing such uses.

### Special Consideration

Parcels which adjoin existing or proposed lead/spur rail lines as indicated on the Subarea Map are required to be developed in accordance with Rail Service Standards (Part III, Section IV.G, Appendix D).

Rail served industrial activities may occur within the area north of 6th shall be of a character that is compatible with the surrounding land use along the 6th Street frontage.

## SUBAREA 12

### Land Use Designation

Industrial Park

### Primary Function

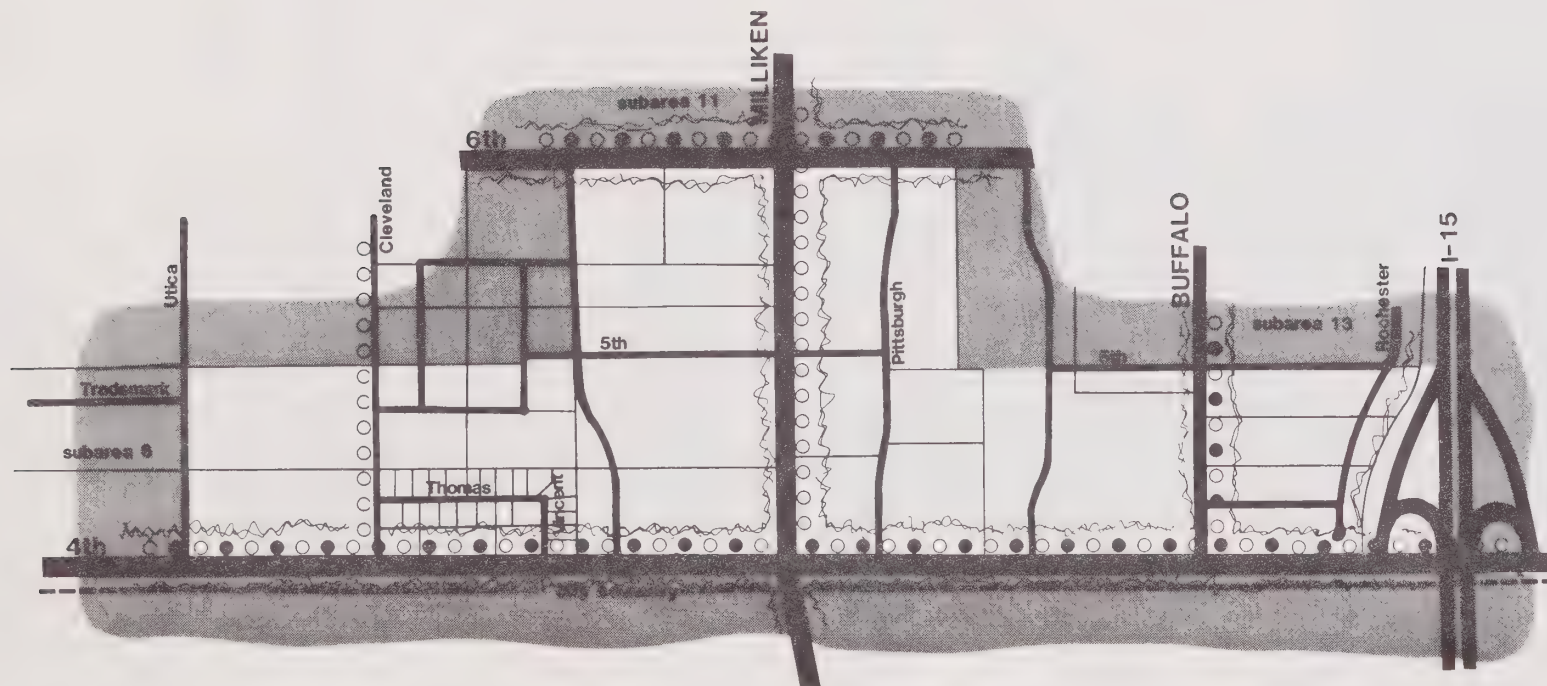
This area will provide for a high quality character to several entryways to the City. This area will also provide an opportunity for tourist oriented uses such as hotels and motels which relate to the airport activities. The subarea is located east of Haven, west of Devore Freeway, south of future alignment of 5th Street to 4th Street and extends along Milliken to 6th Street.

### Permitted Uses

Custom Manufacturing  
Light Manufacturing  
Administrative and Office  
Professional/Design Services  
Research Services  
Light Wholesale, Storage and Distribution  
Building Maintenance Services  
Building Supply Retail & Services  
Business Support Services  
Communication Services  
Eating and Drinking Establishments  
Financial, Insurance and Real Estate Services  
Hotel/Motel  
Recreational Facilities  
Administrative Civic Services  
Flood Control/Utility Corridor

### Conditional Uses

Automotive Rental/Leasing  
Automotive Sales  
Automotive Service Station  
Convenience Sales and Services  
Entertainment  
Fast Food Sales  
Food and Beverage Sales  
Medical/Health Care Services  
Personal Services  
Cultural  
Public Assembly  
Public Safety and Utility Services  
Religious Assembly



Note: Parcel lines and lot configurations are shown as approximation only.

<sup>1</sup> The sites shown may not be currently owned nor is the location site specific. The depiction of a site is an indication of a projected future need that may be adjusted over time as the City develops.

**FIG. IV-14**  
**SUBAREA 12**

#### CIRCULATION

	120' R.O.W.
	100' R.O.W.
	88' or less R.O.W.

#### RAIL SERVICE

	Existing
	Proposed

#### TRAILS/ROUTES

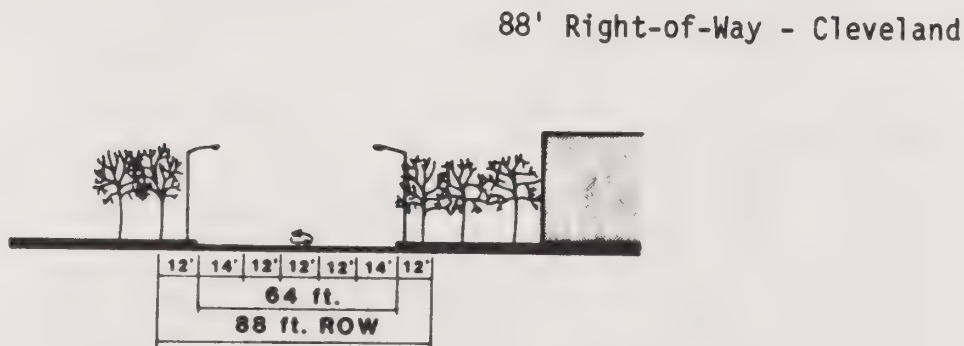
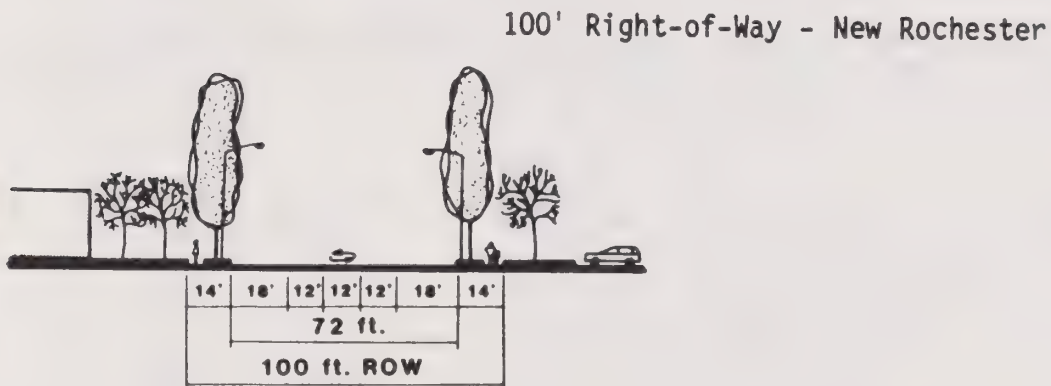
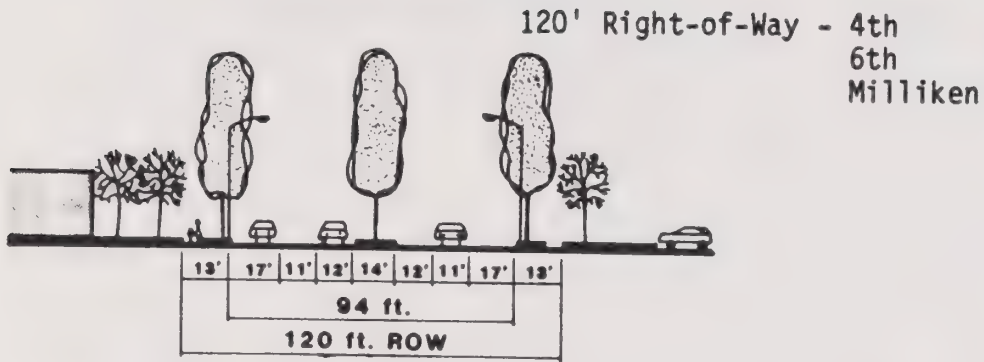
	Pedestrian
	Bicycle
	Regional Multi-Use
	Special Streetscape/Landscaping
	Power Line/Utility Easement
	Creeks & Channels
	Bridge
	Access Points
	Fire Station <sup>1</sup>
	Sheriff Station





SUBAREA 12 (Continued)

Access and Circulation

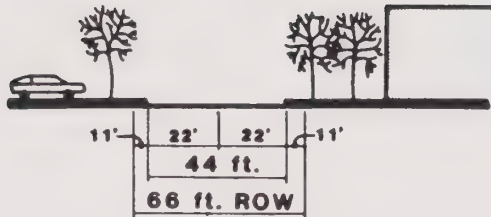


## SUBAREA 12 (Continued)

66' Right Of Way - (Old) Rochester

Pittsburgh

All other Local Streets



Minimum Parcel Size

Two (2) acres on parcels adjacent to 4th Street; and  
One (1) acre on remaining area.

Setback Requirements  
(Measured from face  
of curb)

	Average Landscaping	Parking	Building
4th	45'	25'	45'
6th	"	"	"
Milliken	"	"	"
New Rochester	" "	"	
Cleveland	35'	20'	35'
Old Rochester	25'	15'	25'
Pittsburgh and all other local streets	" "	"	

Landscaping Required

15% of net lot area

Performance Standards

Noise: The maximum allowable exterior noise level of any use shall not exceed  $65_{Ldn}$  as measured by any location on the lot occupied by such uses. Where a structure is occupied by more than one use, the noise level shall not be in excess of  $60_{Ldn}$  as measured within the interior space of the neighboring establishment. Noise caused by motor vehicles are exempted from this standard.

## SUBAREA 12 (Continued)

Vibration: All uses shall be operated so as not to generate vibration discernible without instruments by the average person while on or beyond the lot upon which the source is located or within an adjoining enclosed space if more than one establishment occupies a structure. Vibration caused by motor vehicles, trains, and temporary construction or demolition work is exempted from this standard.

Particulate Matter and Air Contaminants. In addition to compliance with the Air Quality Maintenance District (AQMD) standards, all uses shall be operated so as not to emit particulate matter or air contaminants which are readily detectable without instruments by the average person while on the lot containing such uses.

Odor. All uses shall be operated so as not to emit matter causing unpleasant odors which are perceptible to the average person while within or beyond the lot containing such uses.

Humidity, Heat and Glare: All uses shall be operated so as not to produce humidity, heat, glare or high-intensity illumination which is perceptible without instruments by the average person while on or beyond the lot containing the use.

### Special Consideration

To preserve and enhance the image of the community special consideration shall be given to the quality of site design, architecture, and landscaping of all properties adjacent to the I-15 Freeway. Attractive screening of outdoor work, loading, storage areas, roof and ground mounted equipment from significant freeway points of view shall be required.



FIG. IV-15

# SUBAREA 13

## CIRCULATION

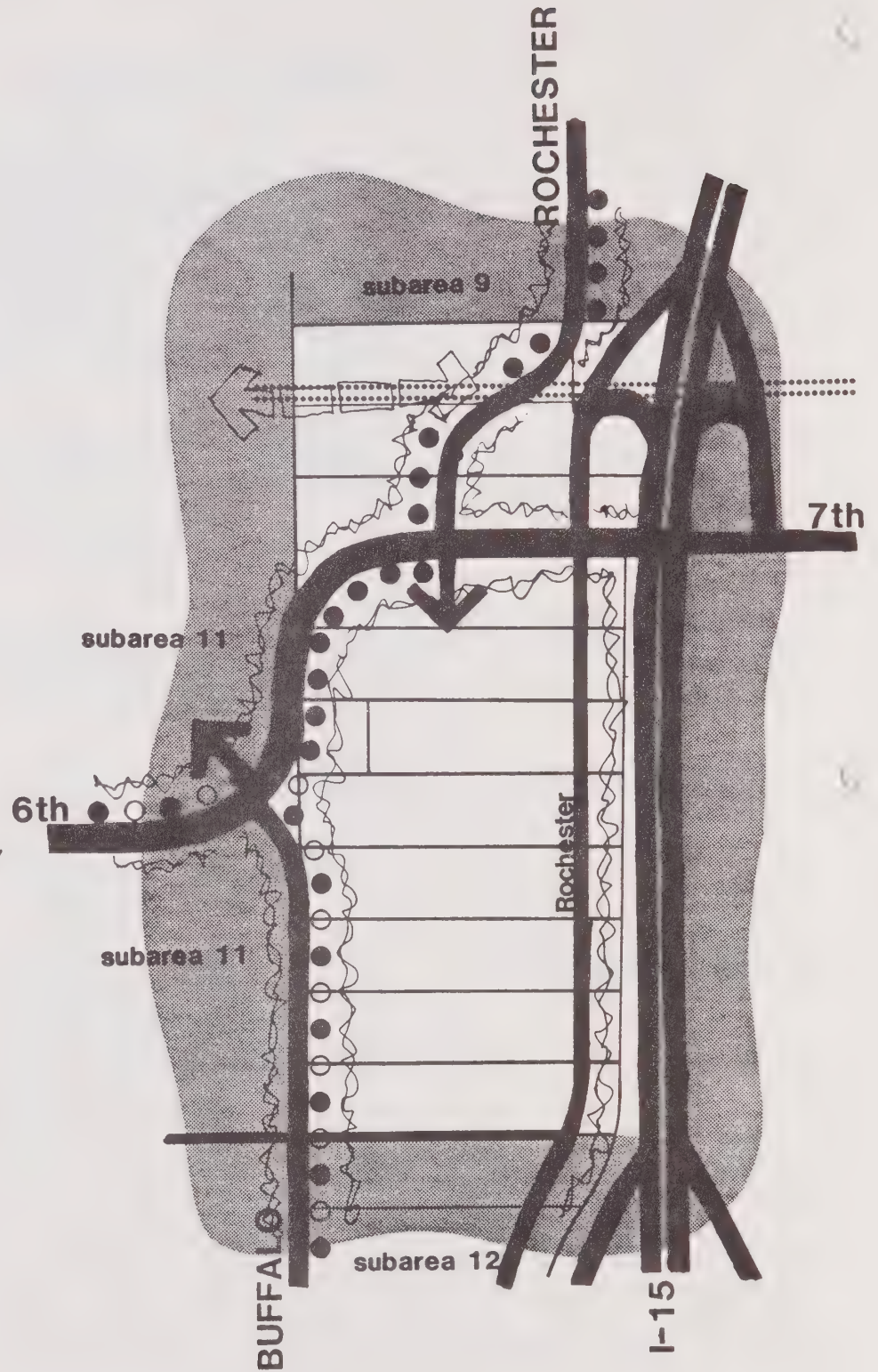
	120' R.O.W.
	100' R.O.W.
	88' or less R.O.W.

## RAIL SERVICE

	Existing
	Proposed

## TRAILS/ROUTES

	Pedestrian
	Bicycle
	Regional Multi-Use
	Special Streetscape/Landscaping
	Power Line/Utility Easement
	Access Points



Note: Parcel lines and lot configurations are shown as approximation only.



## SUBAREA 13

### Land Use Designation

General Industrial

### Primary Function

The function of the subarea is to provide for general industrial development for those businesses which have business activities associated with Minimum Impact Heavy Industry of Subarea 9. This subarea is located between New Rochester and I-15 Freeway, south of existing industrial users by the AT&SF railroad tracks and extends to approximately 600' north of 4th Street. This area is adjacent to the Devore Freeway and is subject to design considerations regarding views from the Devore Freeway. Uses allowed are primarily light and medium manufacturing.

### Permitted Uses

Custom Manufacturing  
Light Manufacturing  
Medium Manufacturing  
Research Services  
Light Wholesale, Storage, and Distribution  
Automotive/Light Truck Repair - Minor  
Building Contractor's Offices and Yards  
Building Maintenance Services  
Business Supply Retail Sales and Services  
Business Support Services  
Communication Services  
Eating and Drinking Establishments  
Laundry Services  
Recreation Facilities  
Repair Services  
Administrative Civic Services  
Flood Control/Utility Corridor

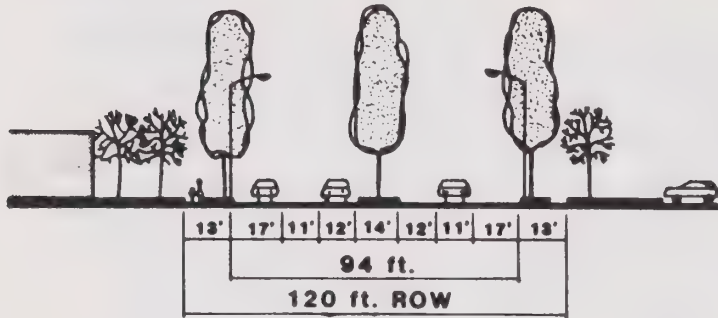
### Conditional Uses

Administrative and Office  
Professional/Design Services  
Animal Care  
Automotive/Truck Repair - Major  
Automotive Service Station  
Building and Light Equipment Supplies and Sales  
Convenience Sales and Services  
Entertainment  
Food and Beverage Sales  
Medical/Health Care Services  
Personal Services  
Cultural  
Public Assembly  
Public Safety and Utility Services  
Religious Assembly

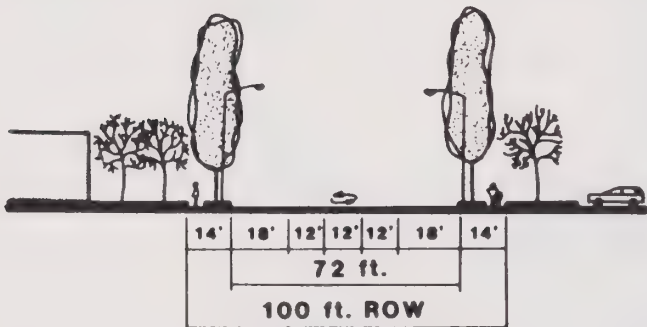
## SUBAREA 13 (Continued)

### Access and Circulation

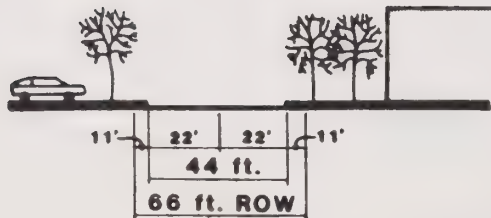
120' Right-of-Way - 7th (west of Devore Frwy)



100' Right-of-Way - New Rochester



66' Right-of-Way - (Old) Rochester  
All other Local Streets



Minimum Parcel Size

One-half (1/2) acre

# SUBAREA 13 (Continued)

## Setback Requirements (Measured from ultimate face of curb)

	Average Landscaping	Parking	Building
7th (west of Devore)	45'	25'	45'
New Rochester (Old Rochester)	"	"	"
All other local streets	25'	15'	25'
	"	"	"

## Landscaping Requirements

12% of net lot area

## Performance Standards

Noise: The maximum allowable noise level of any use shall not exceed 75<sub>Ldn</sub> as measured at the lot line of the lot containing the use. Where a use occupies a lot abutting residentially zoned land, the noise level shall not exceed 65<sub>Ldn</sub> as measured at the common lot line. Noise caused by motor vehicles and trains are exempted from this standard.

Vibration: All uses shall be operated so as not to generate vibration discernible without instruments by the average persons beyond the lot upon which the source is located. Vibration caused by motor vehicles, trains, and temporary construction or demolition is exempted from this standard.

Particulate Matter and Air Contaminants: In addition to compliance with the Air Quality Maintenance District (AQMD) standards, all uses shall be operated so as not to emit particulate matter or air contaminants which are readily detectible without instruments by the average person beyond any lot line of the lot containing such uses.

Odor: All uses shall be operated so as not to emit matter causing unpleasant odors which are perceptible to the average person beyond any lot line of the lot containing such uses.

### SUBAREA 13 (Continued)

Humidity, Heat, and Glare: All uses shall be operated so as not to produce humidity, heat, glare of high-intensity illumination which is perceptible without instruments by the average person beyond the lot line of any lot containing such use.

#### Special Consideration

To preserve and enhance the image of the community special consideration shall be given to the quality of site design, architecture, and landscaping of all properties adjacent to the I-15 Freeway. Attractive screening of outdoor work, loading, storage areas, roof and ground mounted equipment from significant freeway points of view shall be required.

All development in this area shall be required to completely screen any outdoor storage areas from public view on the I-15 Freeway. Future considerations will be made for abandonment of a portion of Rochester right-of-way. Future development between Old Rochester right-of-way and I-15 Freeway will be limited because of the inherently small lot depth of parcels within this area.

Important Note: Class C Performance Standards of the adjoining Minimum Impact/Heavy Industrial area (Subarea 9), allow existing businesses and future Industrial uses which produce noise, particulate matter and air contaminants, vibration, odor, humidity, heat, glare or high intensity. Uses sensitive to these conditions, such as precision manufacturing or office projects, should consider these factors prior to locating in the vicinity of Minimum Impact/Heavy Industrial property.



## SUBAREA 14

### Land Use Designation

General Industrial

### Primary Function

This area is to serve the broadest range of industrial activities typically light and medium manufacturing and wholesale distribution functions. This subarea provides a transition and buffer from the Heavy Industrial activity within Subarea 15. This subarea extends east of Devore Freeway approximately 1000' from AT&SF tracks to 4th Street and along 4th Street frontage approximately 600' deep. Special design considerations for storage area screening will be required along I-15 Freeway corridor.

### Rail Service Function

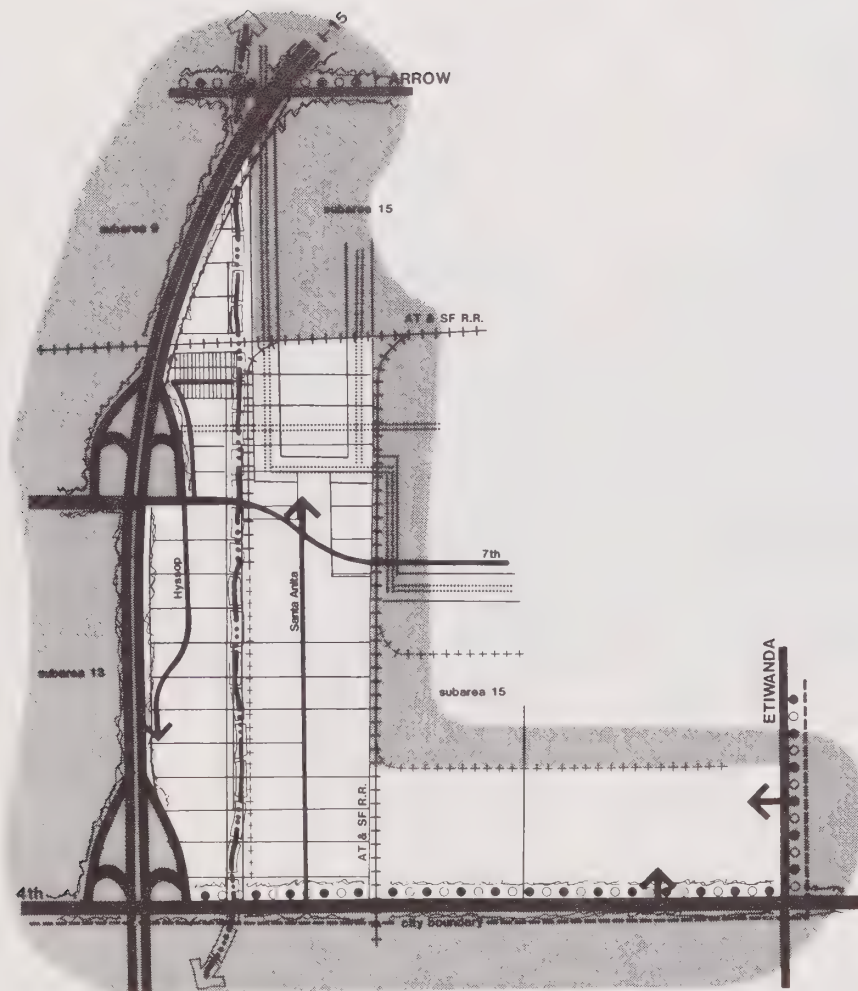
Portions of this Subarea have lead/spur rail lines to accommodate Rail Service needs of industrial uses. It is the City's policy to encourage the development of Rail Service facilities on parcels with strong rail development potential. Standards have been established as a part of this Specific Plan (Part III, Section IV.G) which require Rail Service development on all parcels which adjoin existing or proposed lead/spur rail lines.

### Permitted Uses

Custom Manufacturing  
Light Manufacturing  
Medium Manufacturing  
Research Services  
Light Wholesale, Storage and Distribution  
Medium Wholesale, Storage and Distribution  
Automotive/Light Truck Repair-Minor  
Building Contractor's Offices and Yards  
Building Maintenance Services  
Business Supply Retail Sales and Services  
Business Support Services  
Communication Services  
Eating and Drinking Establishments  
Laundry Services  
Recreation Facilities  
Repair Services  
Administrative Civic Services  
Flood Control/Utility Corridor

FIG. IV-16

# SUBAREA 14



## CIRCULATION

- 120' R.O.W.
- 100' R.O.W.
- 88' or less R.O.W.

## RAIL SERVICE

- Existing
- Proposed

## TRAILS/ROUTES

- Pedestrian
- Bicycle
- Regional Multi-Use
- Special Streetscape/Landscaping
- Power Line/Utility Easement
- Creeks & Channels
- Bridge
- Access Points
- <sup>1</sup>Park
- Fire Station



Note: Parcel and lot configurations are shown as approximation only.

<sup>1</sup>The sites shown may not be currently owned nor is the location site specific. The depiction of a site is an indication of a projected future need that may be adjusted over time as the City develops.

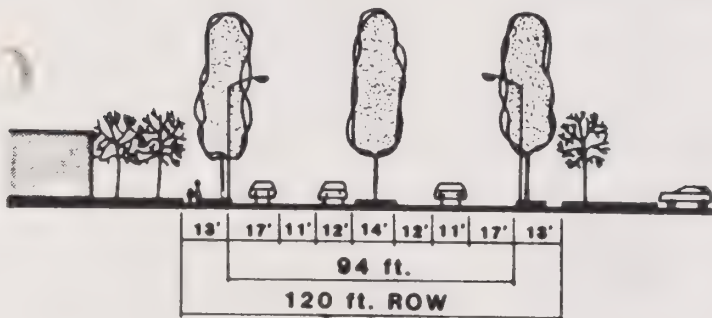
## SUBAREA 14 (Continued)

### Conditional Uses

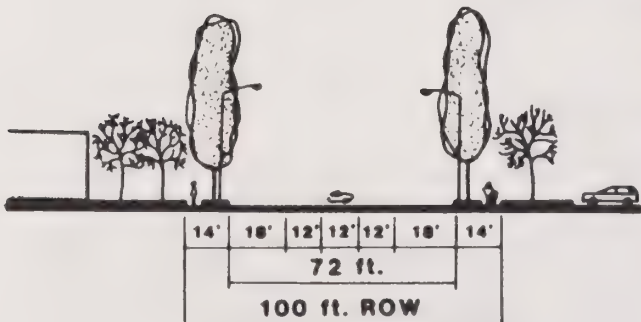
Administrative and Office  
 Professional/Design Services  
 Public Storage  
 Animal Care  
 Automotive/Truck Repair - Major  
 Automotive Service Station  
 Building and Light Equipment Supplies and Sales  
 Convenience Sales and Services  
 Fast Food Sales  
 Food and Beverage Sales  
 Medical/Health Care Services  
 Personal Services  
 Cultural  
 Public Assembly  
 Public Safety and Utility Services  
 Religious Assembly

### Access and Circulation

#### 120' Right-of-Way - 4th

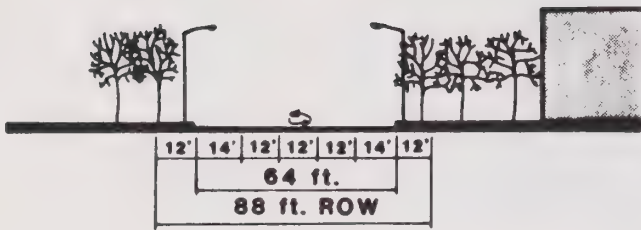


#### 100' Right-of-Way - Etiwanda (Median only north of Arrow)

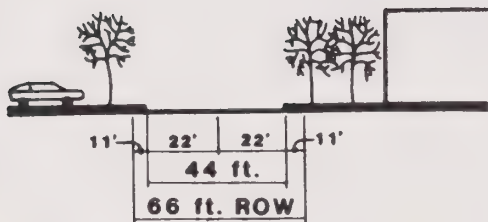


# SUBAREA 14 (Continued)

## 88' Right-of-Way - 7th



## 66' Right-of-Way - Santa Anita All other Local Streets



Minimum Parcel Size

One-half (1/2) acre

Setback Requirements  
(Measured from ultimate face of curb)

	Average Landscaping	Parking	Building
4th	45'	25'	45'
Etiwanda	"	"	"
7th	35'	20'	35'
Santa Anita	25'	15'	25'
All other local streets	"	"	"

Landscaping Requirements

12% of the net lot area.

Performance Requirements

Noise: The maximum allowable noise level of any use shall not exceed 75  $L_{dn}$  as measured at the lot line of the lot containing the use. Where a use occupies a lot abutting residentially zoned land, the noise level shall not exceed 65  $L_{dn}$  as measured at the common lot line. Noise caused by motor vehicles and trains are exempted from this standard.



## SUBAREA 14 (Continued)

Vibration: All uses shall be operated so as not to generate vibration discernible without instruments by the average person beyond the lot upon which the source is located. Vibration caused by motor vehicles, trains, and temporary construction or demolition is exempted from this standard.

Particulate Matter and Air Contaminants: In addition to compliance with the Air Quality Maintenance District (AQMD) standards, all uses shall be operated so as not to emit particulate matter or air contaminants which are readily detectable without instruments by the average person beyond any lot line of the lot containing such uses.

Odor: All uses shall be operated so as not to emit matter causing unpleasant odors which are perceptible to the average person beyond any lot line of the lot containing such uses.

Humidity, Heat and Glare: All uses shall be operated so as not to produce humidity, heat, glare or high-intensity illumination which is perceptible without instruments by the average person beyond the lot line of any lot containing such uses.

### Special Considerations

To preserve and enhance the image of the community special consideration shall be given to the quality of site design, architecture, and landscaping of all properties adjacent to the I-15 Freeway. Attractive screening of outdoor work, loading, storage areas, roof and ground mounted equipment from significant freeway points of view shall be required.

Uses within 600' of I-15 Freeway shall screen from view any outdoor storage areas from the freeway corridor.

Parcels which adjoin existing or proposed lead/spur lines as indicated on the Subarea Map are required to be developed in accordance with Rail Service Standards (Part III, Section IV.G; Appendix D).

FIG. IV-17

# SUBAREA 15

## CIRCULATION

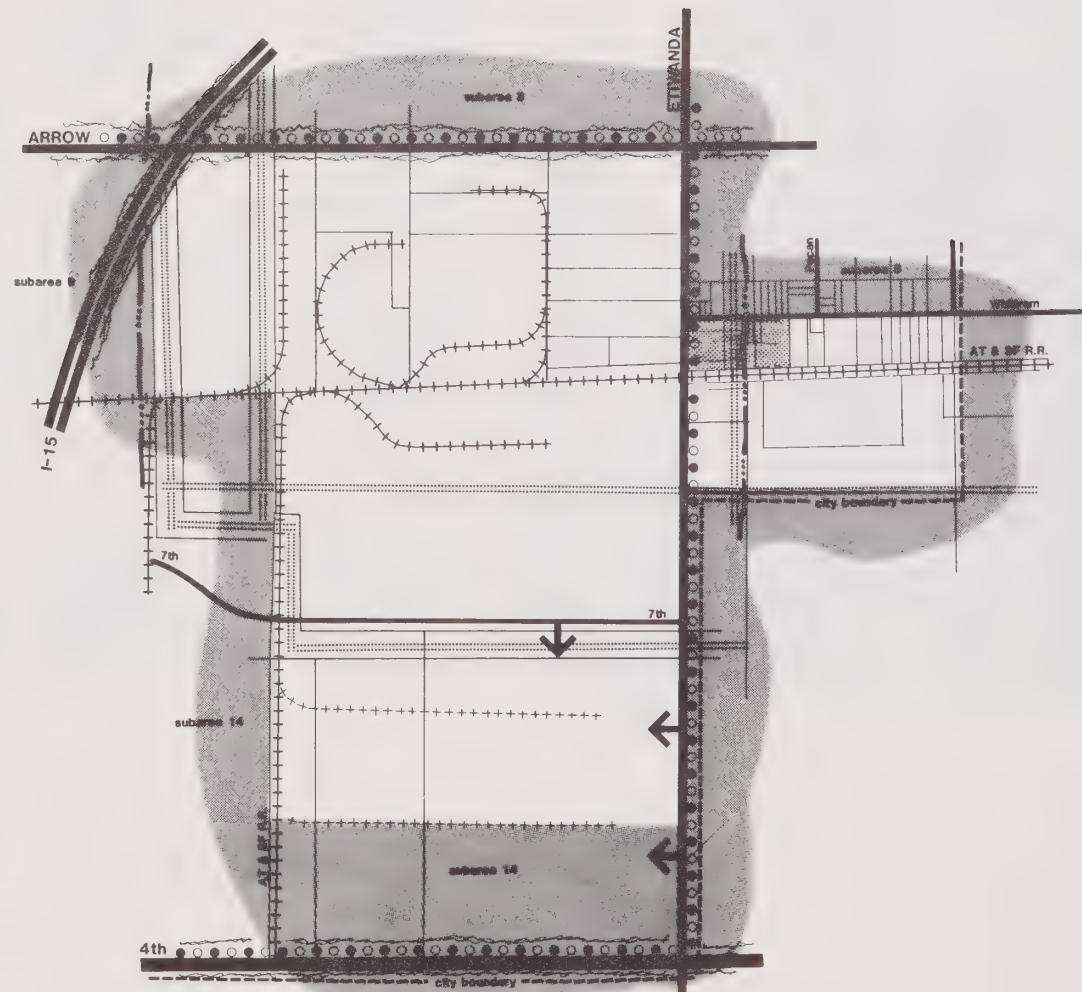
- 120' R.O.W.
- 100' R.O.W.
- 88' or less R.O.W.

## RAIL SERVICE

- Existing
- Proposed

## TRAILS/ROUTES

- Pedestrian
- Bicycle
- Regional Multi-Use
- Special Streetscape/Landscaping
- Power Line/Utility Easement
- Creeks & Channels
- Bridge
- Access Points
- <sup>1</sup>Park
- Fire Station



Note: Parcel lines and lot configurations are shown as approximations only.

<sup>1</sup>The sites shown may not be currently owned nor is the location site specific. The depiction of a site is an indication of a projected future need that may be adjusted over time as the City develops.

## SUBAREA 15

### Land Use Designation

Heavy Industrial

### Primary Function

This area provides for the continuation and promotion of the heaviest of industrial users which would by nature be annoying to other less intense industrial activities. Subareas located south of Arrow Route extending to the project boundary limits on the east and approximately 600' north of 4th Street adjacent to the Devore Freeway north of AT&SF tracks and along the utility easement south of AT&SF tracks. This area contains some of the City's heaviest industrial uses and along the southeast portion of this area, outside the City Limits, is the Kaiser Steel Plant.

### Rail Service Function

Portions of the Subarea have lead/spur rail lines to accommodate Rail Service needs of industrial uses. It is the City's policy to encourage the development of Rail Service facilities on parcels with strong rail development potential. Standards have been established as a part of this Specific Plan (Part III, Section IV.G) which require Rail Service development on all parcels which adjoin existing or proposed lead/spur rail lines.

### Permitted Uses

Medium Manufacturing  
Minimum Impact Heavy Industrial Manufacturing  
Heavy Manufacturing  
Public Storage  
Light Wholesale, Storage and Distribution  
Medium Wholesale, Storage and Distribution  
Heavy Wholesale, Storage and Distribution  
Agricultural/Nursery Supplies and Services  
Building Contractor's Office & Yards  
Building Contractor Storage Yards  
Building and Light Equipment Supplies and Sales  
Eating and Drinking Establishments  
Heavy Equipment Sales and Rentals

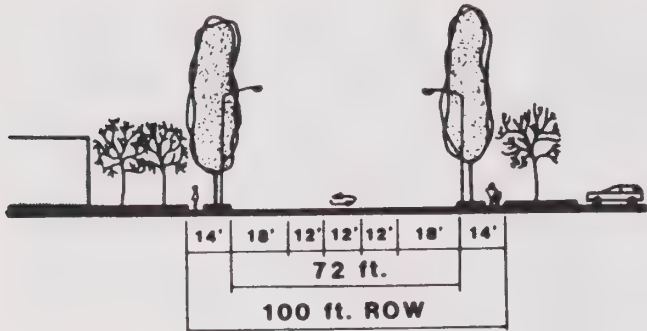
### Conditional Uses

Business Support Services  
Communication Services  
Convenience Sales and Services  
Extensive Impact Commercial  
Food and Beverage Sales  
Petroleum Products Storage  
Repair Services  
Scrap Operations  
Extensive Impact Utility Facilities  
Public Safety and Utility Services

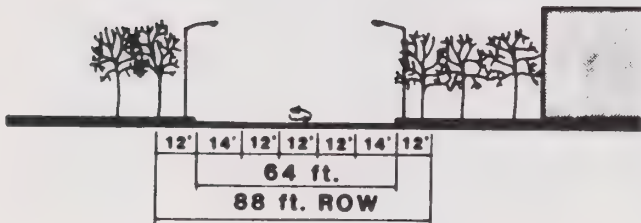
# SUBAREA 15 (Continued)

## Access and Circulation

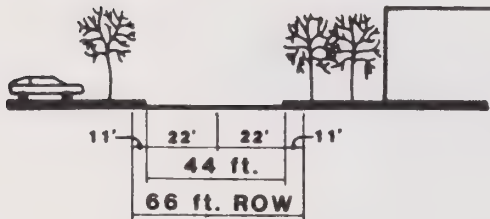
100' Right-of-Way - Arrow  
Etiwanda (Median only north  
of Arrow).



88' Right-of-Way - 7th



66' Right-of-Way - Whittram  
All other local streets



Minimum Parcel Size

Five (5) acres



## SUBAREA 15 (Continued)

Setback Requirements  
(Measured from ultimate face of curb)

	Average Landscaping	Parking	Building
Arrow	45'	25'	45'
7th	35'*	20'	35'
Whittram	25'	15'	25'
All other local streets	" "	"	

\* Exception see special consideration in this Section.

Landscaping Requirements

10% of net lot area in the area between Arrow Highway extending 1000' south, 5% of the net lot in the remaining area.

Performance Standards

Noise: The maximum allowable noise level of any use shall not exceed 85<sub>L<sub>dn</sub></sub> as measured at the lot line of the lot containing the use. Where a use is within 200 feet of residentially zoned land, the noise level shall not exceed 65<sub>L<sub>dn</sub></sub>. Where a use occupies a lot abutting or separated by a street from a lot within the designated Class A or B performance standard or residential property, the performance standard of the abutting property shall apply at the common or facing lot line.

Vibration: All uses shall be operated so as not to generate vibration discernible without instruments by the average person beyond 600' from where the source is located. Vibration caused by motor vehicles, trains, and temporary construction or demolition is exempted from this standard.

Particulate Matter and Air Contaminants: In addition to compliance with the Air Quality Management District (AQMD) standards, all uses shall be operated so as not to emit particulate matter or air contaminants which: (a) are injurious to the health of either persons engaged in or related to the use of the lot, or persons residing, working, visiting, or recreating in neighboring areas; (b) substantially and adversely affect the maintenance of property in nearby areas; (c) are disruptive of industrial processes carried on in other parts of the industrial area. Where a use occupies a lot abutting or separated by a street lot with designated Class A or B performance standard for particulate matter and air contaminants shall apply at the common or facing lot line.

## SUBAREA 15 (Continued)

Odor: All uses shall be operated so as not to emit matter causing unpleasant odors which are perceptible to the average person 600' beyond any lot line of the lot containing such uses.

Humidity, Heat and Glare: All uses shall be operated so as not to produce humidity, heat, glare or high-intensity illumination which is perceptible without instruments by the average person while on any lot zoned for residential purposes or any industrial property with Class A or B performance standard designation.

### Special Considerations

Areas within 600' of Devore Freeway shall be required to completely screen outdoor storage from view along Devore Freeway corridor. Heavy manufacturing users adjacent to Subarea 8 and Subarea 14 are required to have a minimum 100' building setback along abutting property lines.

Where a Rail Service Spur is necessary adjacent to Seventh Street frontage, its landscape setback may be maintained at no less than 20' from ultimate face of curb, extending to the Rail Service right-of-way maintained outside the landscape setback. The landscaping within the area shall be designed in order to help screen loading activities.

Parcels which adjoin existing or proposed lead/spur lines as indicated on the Subarea Map are required to be developed in accordance with Rail Service Standards (Part III, Section IV.G; Appendix D).

Scrap operations shall be required to conform to the following standards:

Fences and Walls. All operations and storage, including all equipment used in such businesses shall be within an enclosed building, or within an area enclosed by a solid wall or solid view obscuring fence.

All fences and walls shall be of a uniform height in relation to the ground upon which they stand and shall be a minimum of eight feet (8') not to exceed sixteen feet (16') in height. All yard gates shall be solid view obscuring and no less than eight feet (8') in height.

SUBAREA 15 (Continued)

All fences and walls open to view from any public street shall be constructed of solid masonry.

All fences and walls shall be uniform color and design and constructed in a workmanlike manner.

Landscaping. Dense trees shall be planted and maintained along side and rear property lines to aid in screening the yard area.

## SUBAREA 16

### Land Use Designation

### Industrial Park

### Primary Function

This Subarea serves as a transition zone from more intensive industrial or commercial activities to residential areas in the southwest corner of the City. As such, new development must be sensitive to the surroundings with appropriate architecture and site planning to mitigate potential conflicts. Land uses within the industrial area should be compatible with surrounding uses north of 6th Street and along Archibald to provide for use activities associated with airports such as tourist commercial. This subarea is located between 6th Street and 4th Street and Archibald and Hellman and contains property substantially undeveloped. It lies adjacent to a direct access to the Ontario International Airport and is located at a gateway to the City.

### Permitted Uses

Custom Manufacturing  
Light Manufacturing  
Administrative and Office  
Professional/Design Services  
Research Services  
Light Wholesale, Storage, and Distribution  
Building Maintenance Services  
Business Supply Retail Sales and Services  
Business Support Services  
Communication Services  
Eating and Drinking Establishments  
Financial, Insurance and Real Estate Services  
Hotel/Motel  
Medical/Health Care Services  
Recreation Facilities  
Administrative Civic Services

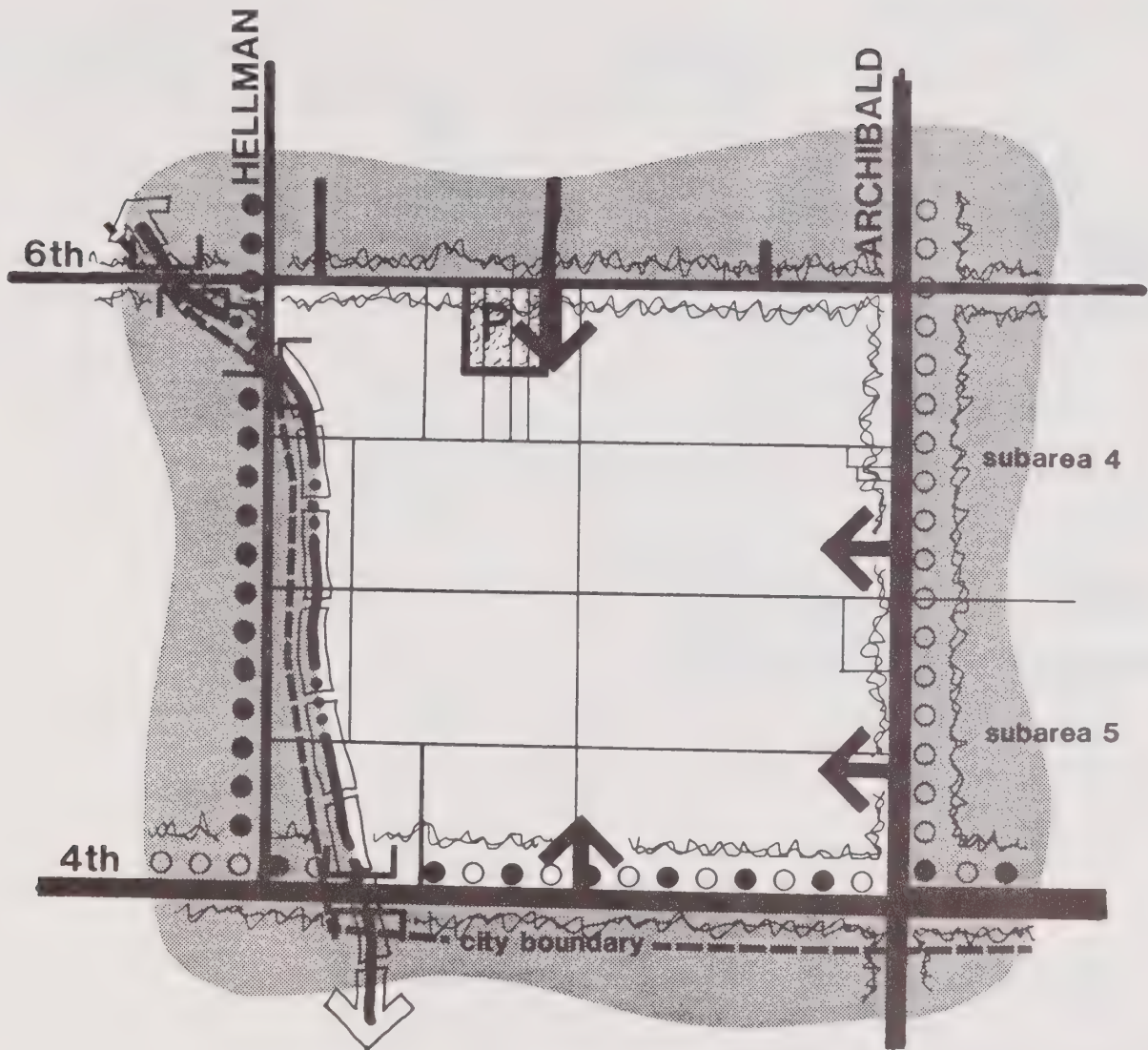
### Conditional Uses

Automotive Rental/Leasing  
Automotive Service Station  
Convenience Sales and Services  
Entertainment  
Fast Food Sales  
Food and Beverage Sales  
Funeral and Crematory Services  
Personal Services  
Cultural  
Public Assembly  
Public Safety and Utility Services  
Religious Assembly



FIG. IV-18

# SUBAREA 16



## CIRCULATION

- 120' R.O.W.
- 100' R.O.W.
- 88' or less R.O.W.

## RAIL SERVICE

- Existing
- Proposed

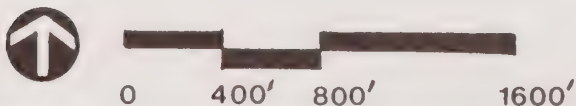
## TRAILS/ROUTES

- Pedestrian
- Bicycle
- Regional Multi-Use
- Bridge
- Access Points

Creeks & Channels

Park<sup>1</sup>

Special Streetscape/Landscaping



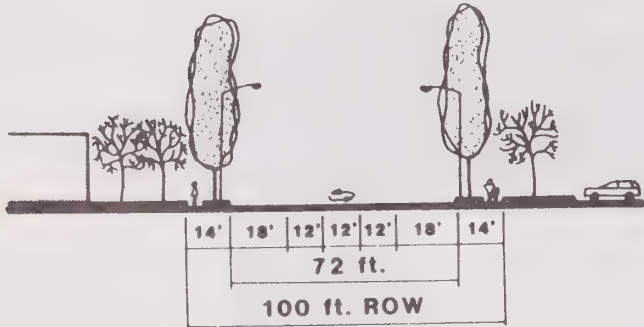
Note: Parcel lines and lot configurations are shown as approximation only.

<sup>1</sup>The sites shown may not be currently owned nor is the location site specific. The depiction of a site is an indication of a projected future need that may be adjusted over time as the City develops.

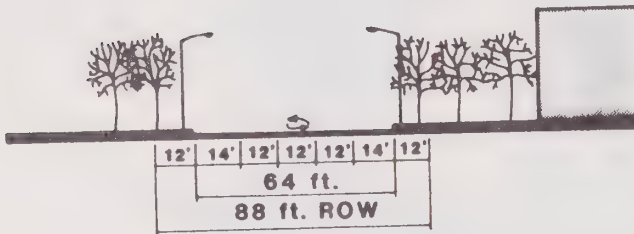
SUBAREA 16 (Continued)

Access and Circulation

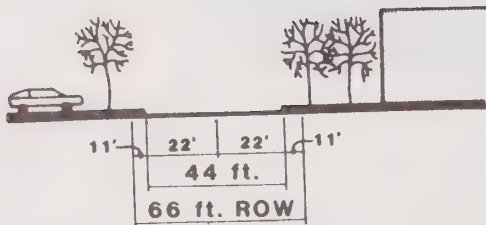
100' Right-of-Way - Archibald  
4th



88' Right-of-Way - Hellman  
6th



66' Right-of-Way - All local streets



Future left turn movement directly onto property will be restricted in order to provide for future development of a median island with 4th Street.

Minimum parcel size: one (1) acre.

## SUBAREA 16 (Continued)

### Setback Requirements (Measured from ultimate face of curb)

	Average Landscaping	Parking	Building
4th	45'	25'	45'
6th	"	35'	"
Archibald	"	"	"
Hellman	35'	20'	35'
All local streets	25'	15'	25'

### Landscaping Requirements

15% of net lot area

### Performance Standards

Noise: The maximum allowable exterior noise level of any use shall not exceed 65<sub>ldn</sub> as measured by any location on the lot occupied by such uses. Where a structure is occupied by more than one use, the noise level shall not be in excess of 60<sub>ldn</sub> as measured within the interior space of the neighboring establishment. Noise caused by motor vehicles are exempted from this standard.

Vibration: All uses shall be so operated as not to generate vibration discernible without instruments by the average person while on or beyond the lot upon which the source is located or within an adjoining enclosed space if more than one establishment occupies a structure. Vibration caused by motor vehicles, trains, and temporary construction or demolition is exempted from this standard.

Particulate Matter and Air Contaminants: In addition to compliance with the Air Quality Maintenance District (AQMD) standards, all uses shall be operated so as not to emit particulate matter or air contaminants which are readily detectable without instruments by the average person beyond any lot line of the lot containing such uses.

Odor: All uses shall be operated so as not to emit matter causing unpleasant odors which are perceptible to the average person while within or beyond the lot containing such uses.

Humidity, Heat and Glare: All uses shall be operated so as not to produce humidity, heat, glare, or high-intensity illumination which is perceptible without instruments by the average person while on or beyond the lot containing the use.



## SUBAREA 16 (Continued)

### Special Considerations

Because this subarea is located adjacent to residential development, property owners within 500' of the subarea shall be notified at the time of any development considerations.

A conceptual Master Plan which outlines access, circulation, drainage and timing of improvements has been approved for this Subarea (see Development Review File Number 82-16). All new development must be consistent with this Master Plan, or the appropriate revisions approved.

Properties adjacent to residential areas shall have a minimum of 45' building setback from property line on interior lot lines abutting residential districts and 45' from ultimate face of curbs on streets abutting residential districts. Minimum front parking setback shall be 35' for such parcels.

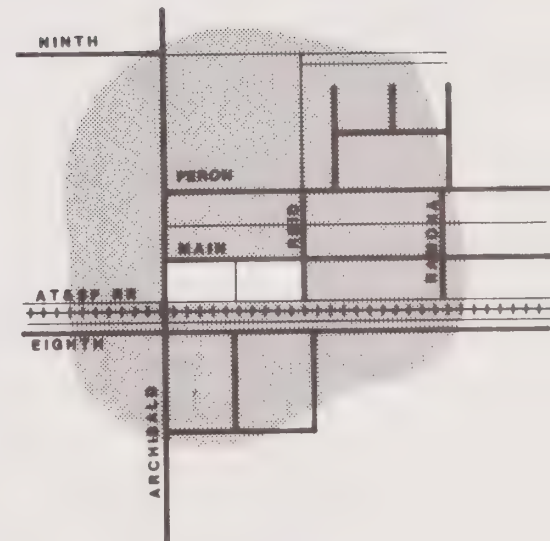
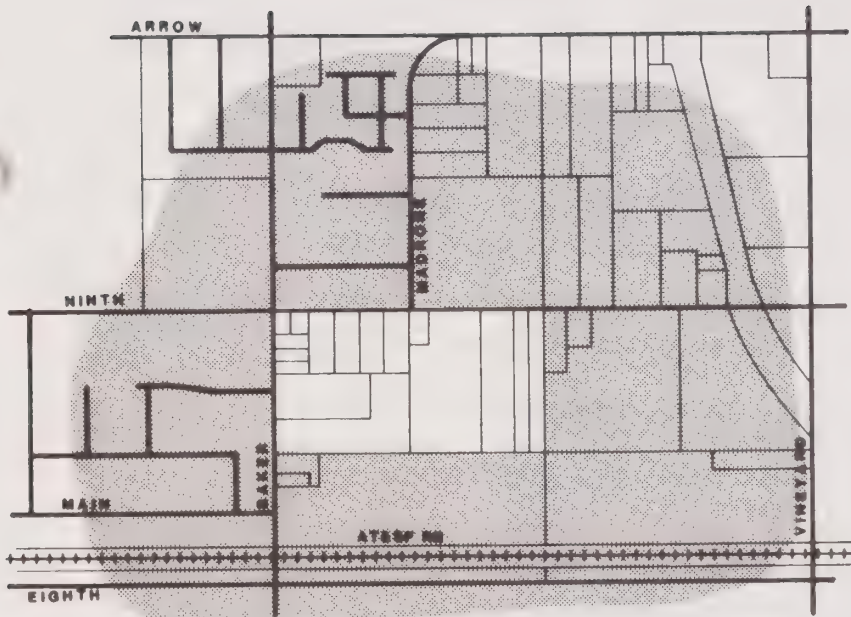
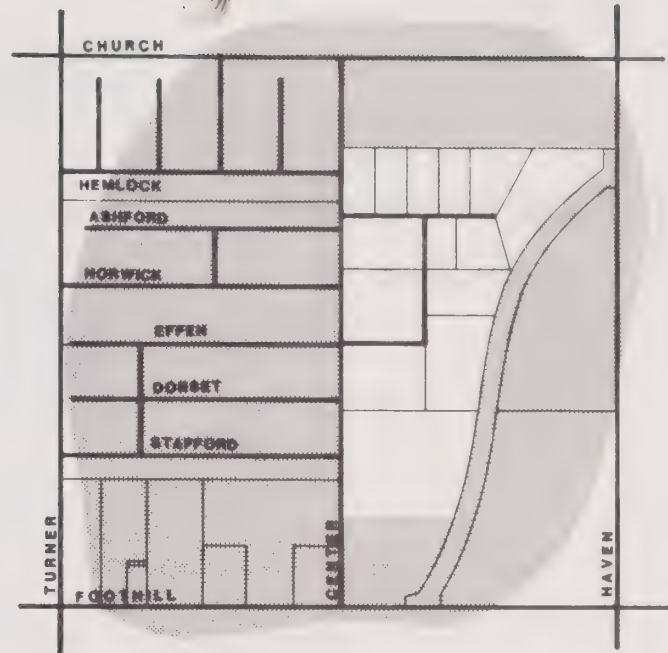
In areas which abut residential uses, screening of industrial buildings, parking, storage, and equipment areas is required through the use of walls, fences, and/or berms. The screening, in conjunction with the streetscape setback and landscaping will soften the edge of the industrial development and provide an attractive design element. Screen wall/fence height shall not exceed 3 feet inside the 40' parking setback.

In areas abutting residential uses, access via internal circulation in place of residential streets is encouraged, when feasible, in order to minimize impact upon existing residential, vehicular, and pedestrian circulation patterns.

To preserve and enhance the image of the community special consideration shall be given to the quality of site design, architecture and landscaping of all properties adjacent to the I-15 Freeway. Attractive screening of outdoor work, loading, storage areas, roof and ground mounted equipment from significant freeway points of view shall be required.



**FIG. IV-19**  
**SUBAREA 17**



**CIRCULATION**

- 120' R.O.W.
- 100' R.O.W.
- 88' or less R.O.W.

**RAIL SERVICE**

- Existing
- Proposed

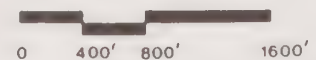
**TRAILS/ROUTES**

- Pedestrian
- Bicycle
- Regional Multi-Use
- Special Streetscape/Landscaping
- Power Line/Utility Easement

- Creeks & Channels
- Bridge
- Access Points
- Park<sup>1</sup>

Note: Parcel lines and lot configurations are shown as approximation only.

<sup>1</sup>The sites shown may not be currently owned nor is the location site specific. The depiction of a site is an indication of a projected future need that may be adjusted over time as the City develops.



## SUBAREA 17

### Land Use Designation

### Industrial Park

### Primary Function

This Subarea serves as a transition zone from more intensive industrial or commercial activities to residential areas. As such, new development must be sensitive to the surroundings with appropriate architecture and site planning to mitigate potential conflicts.

Subarea 17 is located in three separate areas of the Industrial Plan: the southeast corner of Baker and Ninth; the southeast corner of Archibald and Main; and, on the east side of Center Avenue, between Foothill Boulevard and Church.

### Permitted Uses

Custom Manufacturing  
Light Manufacturing  
Administrative and Office  
Professional/Design Services  
Research Services  
Light Wholesale, Storage and Distribution  
Building Maintenance Services  
Business Supply Retail Sales and Services  
Business Support Services  
Communication Services  
Eating and Drinking Establishments  
Financial, Insurance and Real Estate  
Hotel/Motel  
Recreational Facilities  
Administrative Civic Services

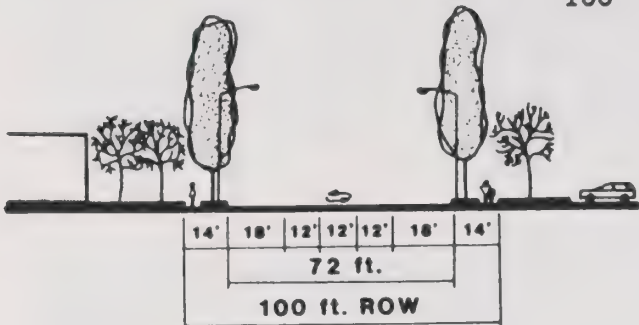
### Conditional Uses

Automotive Rental/Leasing  
Automotive Sales  
Convenience Sales and Services  
Entertainment  
Fast Food Sales  
Food and Beverage Sales  
Medical/Health Care Services  
Personal Services  
Cultural  
Public Assembly  
Public Safety and Utility Services  
Religious Assembly

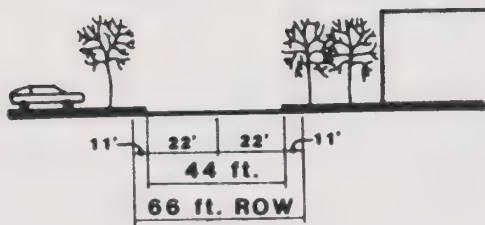
## SUBAREA 17 (Continued)

### Access and Circulation

#### 100' Right-Of-Way - Archibald



#### 66' Right-Of-Way - All Other Streets



### Minimum Parcel Size

One-half (1/2) acre

### Setback Requirements (Measured from ultimate face of curb)

	Average Landscaping	Parking	Building
Archibald	45'	25'	45'
Baker	35'	*20'	45'*
Center	"	"	"
9th "	"	"	"
Main	"	"	"
All other local streets	25'	15'	25'

\*45' setback adjacent to residential

### Landscape Requirements

15% of net lot area

## SUBAREA 17 (Continued)

### Performance Standards

Noise: The maximum allowable exterior noise level of any use shall not exceed 65 Ldn as measured by any location on the lot occupied by such uses. Where a structure is occupied by more than one use, the noise level shall not be in excess of 60 Ldn as measured within the interior space of the neighboring establishment. Noise caused by motor vehicles are exempted from this standard.

Vibration: All uses shall be operated so as not to generate vibration ##?discerible without instruments by the average person while on or beyond the lot upon which the source is located or within an adjoining enclosed space if more than one establishment occupies a structure. Vibration caused by motor vehicles, trains, and temporary construction or demolition work is exempted from this standard.

Particulate Matter and Air Contaminants: In addition to compliance with the Air Quality Maintenance District (AWMD) standards, all uses shall be operated so as not to emit particulate matter or air contaminants which are readily detectable without instruments by the average person while on the lot containing such uses.

Odor: All uses shall be operated so as not to emit matter causing unpleasant odors which are perceptible to the average person while within or beyond the lot containing such uses.

Humidity, Heat and Glare: All uses shall be operated so as not to produce humidity, heat, glare or high-intensity illumination which is perceptible without instruments by the average person while on or beyond the lot containing the use.

### Special Consideration

Properties adjacent to residential areas shall have a minimum of 45' building setback from property line on interior lot lines abutting residential districts and 45' from ultimate face of curbs on streets abutting residential districts. Minimum front parking setback shall be 35' for such parcels.

In areas which abut residential uses, screening of industrial buildings, parking, storage, and equipment areas is required through the use of walls, fences, and/or berms. The screening, in conjunction with the streetscape setback and landscaping will soften the



edge of the industrial development and provide an attractive design element. Screen wall/fence height shall not exceed 3 feet inside the 40' parking setback.

In areas abutting residential uses, access via internal circulation in place of residential streets is encouraged, when feasible, in order to minimize impact upon existing residential, vehicular, and pedestrian circulation patterns.



## **PART V**

### **IMPLEMENTATION PROGRAM**

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<b>Implementation Program</b>	<b>Page</b>
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<b>II. Regulatory Procedures</b>	<b>V-1</b>
<b>Development Code</b>	<b>V-1</b>
<b>III. Sources of Financing</b>	<b>V-1</b>
<b>State Community Redevelopment Law</b>	<b>V-2</b>
<b>Reimbursement District</b>	<b>V-2</b>
<b>Assessment Taxing District</b>	<b>V-3</b>
<b>Drainage and Sewer Facilities</b>	<b>V-3</b>
<b>Bridges and Major Thoroughfares</b>	<b>V-4</b>
<b>Street and Highways</b>	<b>V-4</b>
<b>Other Improvements</b>	<b>V-4</b>
<b>Other Special Assessment and Bonding Act</b>	<b>V-5</b>





## PART V      IMPLEMENTATION PROGRAM

### I.      INTRODUCTION

Attainment of the Specific Plan objectives will require the coordinated use of the development review procedures and the implementation of Specific plan objectives through sources of financing. Material in this section discusses the regulatory procedures which are necessary for development within the Industrial Area. The regulatory procedures rely heavily on the existing City Code provisions and are incorporated within the Appendix of this document.

Highlighted in this section is a discussion of various sources of financing implementation. This discussion is intended to provide a broad overview which the City may wish to consider in the future in more detail.

### II.      REGULATORY PROCEDURES

#### Development Code

All regulatory procedures (Conditional Use Permits, Variances, Development Review, Non-Conforming Uses, Appeals, Land Use District (Zone) Changes, and Amendments to Specific Plan Boundaries) of the Industrial Specific Plan shall be as contained in the latest adopted edition of the City's Development Code.

### III.      SOURCES OF FINANCING

If the Industrial Area is to be developed in an orderly manner and in a way ensuring proper utilization of the City's industrial land supply, means must be found to finance area-serving facilities, such as new roads, grade-separated crossing of roads and railroads, and storm drainage facilities. These are all needed to serve future industrial development, and in most cases, are prerequisite for such development. Summarized below are possible methods of financing such improvements.

### State Community Redevelopment Law

This Act allows communities to utilize tax increment financing to carry out redevelopment activities, by applying tax increments obtained in the project area to finance planning, administrative, acquisition, and improvement activities. The Act permits a redevelopment agency to finance land acquisition for public purposes, construction of public facilities, such as roads, parks, and sewers, and administrative, legal, planning, and engineering costs related to the project. The redevelopment agency, established by the City Council, would designate a project area and would prepare a redevelopment plan. The agency, then would issue bonds to finance project area improvements and administrative costs, and would apply the tax increments derived in the project area to pay the debt Service on the bonds. Tax increments are those tax revenues received during the Base Year preceding adoption of a Redevelopment Plan.

The excess revenues thus produced are used to pay off bonds issued to finance the expenses of the redevelopment process such as administration, planning, acquisition, and construction of public facilities. Current and projected development could provide a substantial revenue base from which to finance major improvements.

### Reimbursement District

Under Articles 5 and 6 of the California Subdivision Map Act (as amended in January 1977), fees can be collected to cover the costs of public improvements including roads, bridges, drainage and sanitary sewer facilities. Under these provisions, the City could enter into an agreement with the developer to reimburse him or her for that portion of improvements to serve his or her property only and the actual cost of such improvements. The City would then reimburse the developer by levying a charge on any real property similarly benefited or by establishing and maintaining a local benefit district to levy and collect charges or costs from other benefited properties. It is possible, under this approach, to defer payment of charges or costs by other benefited properties until such property is developed.

### Assessment Taxing District

California law authorizes a variety of assessment procedures which could be employed to finance Industrial Area improvements. These assessment procedures potentially can be applied to finance construction of roads, bridges or grade-separated crossings, flood protection facilities, storm drainage facilities, and open space. They can also be used to provide maintenance Service or other specialized services, such as transit. Assessment district procedures provide an equitable way of assigning costs because they operate directly on benefited properties and are based on assessed valuation. With the passage of Proposition 13, the use of assessment procedures has special relevance in an industrial and commercial area since many recent fiscal analyses have concluded that residential properties are, or will be, paying an increasing portion of the tax burden.

The procedures for establishing assessment districts, however, vary substantially depending on pertinent enabling legislation. In some instances, the legislative body can establish the district, in other instances approval by registered voters in approval by a majority of property owners is mandated. Thus the potential application of assessment procedures depends on the initiation and formulation procedures required, and by attitudes toward mandatory participation. A summation is provided below of the more relevant assessment procedures. These are discussed by the type of improvement or Service financed by the procedure.

### Drainage and Sewer Facilities

Section 66410-66499.30 of the Government Code and the Subdivision Map Act of Government Code Sections 66483-66484.5 authorize payment of fees to defray the costs of building drainage facilities for the removal of surface and stormwaters from local and neighborhood drainage areas. To enact fees, an ordinance requiring payment of fees must be in effect for a period of at least 30 days prior to the filing of a tentative map (or parcel map if no tentative map is required). The ordinance refers to a drainage or sanitary sewer area which contains an estimate of the total costs of constructing the local drainage or sanitary sewer facilities required in the plan. The governing body is the legislative body that has adopted the drainage or sanitary sewer plan.



The cost to be imposed, whether actual or estimated, is based upon the findings of the legislative body, that subdivision and development of property within the planned drainage or local sanitary sewer area will require construction of the facilities described in the plan.

#### Bridges and Major Thoroughfares

Section 66484 of the Government Code authorizes design, acquisition of rights-of-way, administration of construction contracts and actual construction. Local ordinance must refer to the circulation element of the General Plan and to the provisions of such element which identify those major thoroughfares whose primary purpose is to carry through traffic and to provide a network connecting to the state highway system. If one-half of the owners within the area of benefit protest the improvements, then proceedings are abandoned. The local ordinances may require the payment of a fee as a condition of approval of a final map or as a condition of issuing a building permit. An ordinance adopted pursuant to this section may provide for the acceptance of consideration in lieu of payment of fees. The agency imposing fees may incur an interest-bearing indebtedness for the construction of bridge facilities or major thoroughfares.

#### Street and Highways

Section 22585-22594 of the Streets and Highway Code allows the legislative body to construct or install improvements and to provide for the maintenance or servicing of those improvements. The assessment district is initiated by legislative resolution. Proceedings for the assessment may be abandoned if there is a majority protest representing property owners owning more than 50 percent of the area of assessable lands within the proposed district. A four-fifths vote of all legislative body members can overrule the protest.

#### Other Improvements

Special Municipal Tax Districts are authorized under Section 60000-60160 of the Government Code. The district created can maintain and operate any public improvement or utility of local necessity or convenience, furnish or perform any special local



Service including music, recreation, or advertising. The governing body may appoint officers and employees for the district as it deems necessary. Officers and employees serve at the pleasure of the legislative body and are not subject to civil Service provisions. Formulation is initiated by a petition of residents living within the proposed district. Ten percent of the registered voters within the proposed district must sign the petition. The legislative body adopts a resolution of intention and, if no objections are sustained, submits questions of formation of district and levy tax to residents of the district. A majority vote in favor of the district allows the legislative body to declare the district formed and levy the special tax. The district has the authority to levy taxes upon taxable property not to exceed \$1 per year on each \$100 of assess valuation.

#### Other Special Assessment and Bonding Acts

A host of other assessment district acts exist in California, many of which could be used in the area. These include: The Improvement Act of 1911; the Municipal Improvement Act of 1913; the Parking District Law of 1951; and the Pedestrian Mall Law of 1960. All of these allow for the issuance of bonds to represent unpaid assessments. This allows the owner to pay lien to installments over the period of years the particular bond act and proceeding use provides. The bonds themselves can be issued against single owners or against a group of owners.

One area which should be explored further are Industrial Development Bonds. These are obligations of local governments to obtain funds for acquisition or improving facilities, plant sites and buildings, and have occasionally been used even to subsidize industry by financing machinery purchase. However, subsidies from local government are not essential. The advantage is that interest on the bonds is exempt from income taxes. Local government does not issue these as general obligation bonds but merely pledges revenues in various forms. The availability of this technique is uncertain due to unclear state enabling law.

Various other state and federal programs, such as the Urban Development Action Grant Program, are focused largely on declining communities and are likely to have limited potential for Rancho Cucamonga.



## **APPENDIX**

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<b>Appendix</b>	<b>Page</b>
<b>A Traffic Analysis</b>	<b>A-1</b>
<b>B Proposed Rochester-6th-7th Realignment</b>	<b>B-1</b>
<b>C Right-of-Way Requirements AT &amp; SF RR</b>	<b>C-1</b>
<b>D Rail Service Standards</b>	<b>D-1</b>
<b>E Haven Avenue Overlay District Exhibits</b>	<b>E-1</b>





## APPENDIX A

### TRAFFIC ANALYSIS

#### TRAFFIC VOLUME PROJECTIONS

Future traffic volumes were projected for the Industrial Area by a traffic model previously developed for Rancho Cucamonga by DKS Associates. The traffic model utilized City-wide and use projections and proposed future highway network to develop trip generation figures, trip distributions, and trip assignments. This information is used to determine traffic volumes on selected streets.

Trip generation rates are factors assigned to land uses according to equations developed mainly by Institute of Transportation Engineers (ITE) and presented in its Trip Generation manual. For purposes of traffic modeling a lower rate for industrial activity was used based upon effective implementation of a TSMP. Trip generation rates were reduced from 3.0 trips per employee to 2.5 trips.

Trip distribution is the process by which the movement of trips between traffic zones is estimated, yielding a table of zone-to-zone daily trips movements. A traffic zone is a geographical area delineated by topographical, political and physically-bound land areas. Figure A-1 shows the general geographic distribution of vehicle trips to and from the Industrial Area at full build-out. The majority (62%) of all industrial trips are projected to leave the City, while most of the remaining trips are to the northern parts of the City.

#### MIDBLOCK ROADWAY CAPACITY

Midblock roadway capacities were computed for the major street segments in the Industrial Area. Projected ADT volumes were compared with street segment capacities at Level of Service "D". Table A-1 shows the major streets, their roadway classification and number of through lanes.

Projected ADT volumes are compared with street capacities at Level of Service "D" and result in volume-to-capacity (VC) ratios. VC ratios greater than 1.0 imply unacceptable conditions at Level of Service "D".

FIG. A-1

DISTRIBUTION OF PROJECTED INDUSTRIAL AREA TRAFFIC

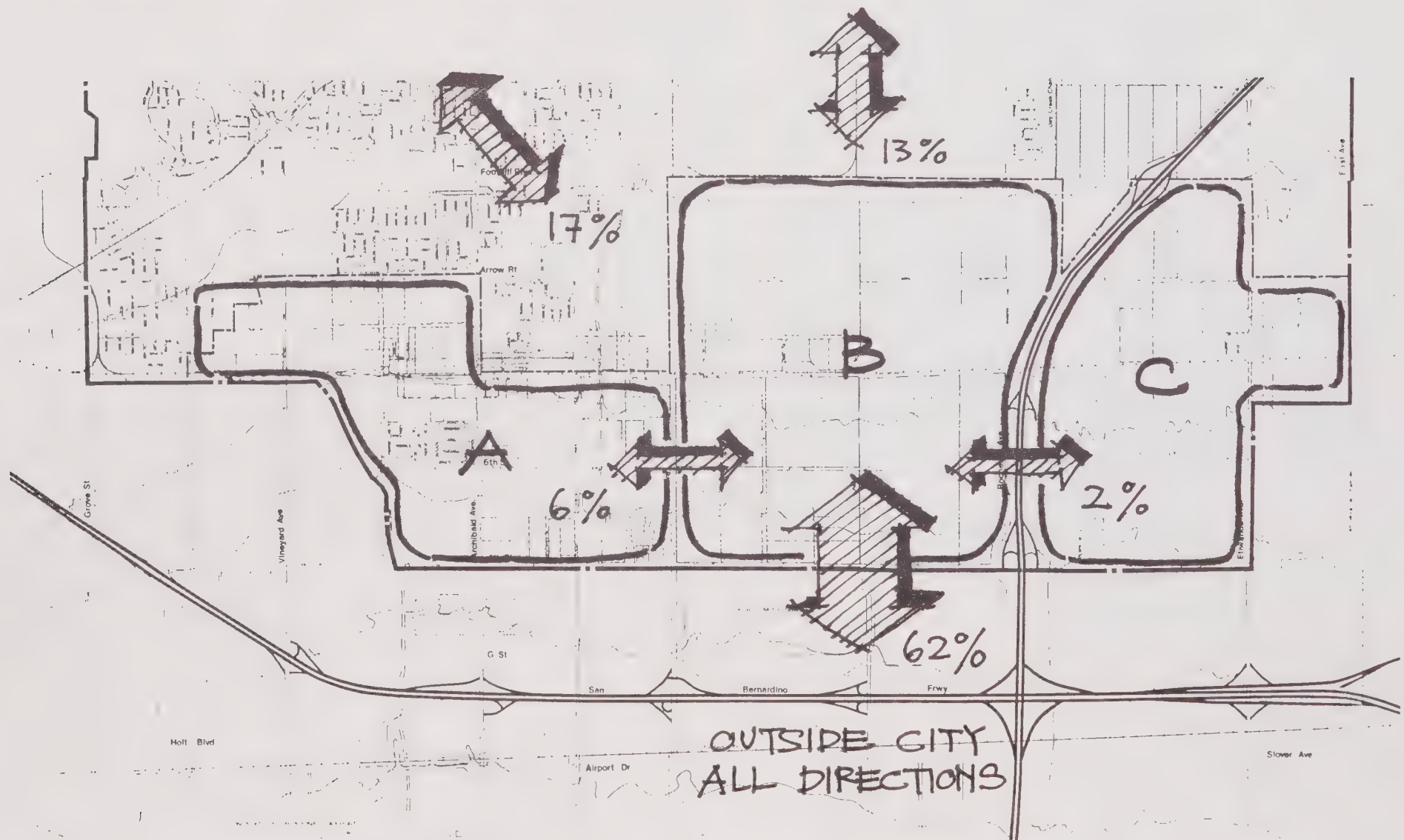


TABLE A-1  
ROADWAY MIDBLOCK VOLUME/CAPACITY (V/C) RATIOS AT LEVEL OF SERVICE "D"

<u>Street</u>	<u>Segment</u>	<u>Class.</u>	<u>Through Lanes</u>	<u>Projected ADT Vol.</u>	<u>Capacity (Veh/day)</u>	<u>V/C</u>
Vineyard	4th-Foothill	Sec.	4	28,000	26,000	1.1
Hellman	4th-Foothill	Sec.	4	8,000	23,000	0.3
Archibald	4th-Foothill	Art.	4	22,000	28,000	0.8
Turner	4th-Foothill	Sec.	4	5,000	23,000	0.2
Haven	4th-Foothill	Art.	6	60,000	43,000	1.4
Milliken	4th-Foothill	Art.	6	40,000	43,000	0.9
Rochester	4th-6th	Art.	4	6,000	26,000	0.2
	6th-Arrow	Art.	4	17,000	26,000	0.7
	Arrow-Foothill	Art.	4	10,000	26,000	0.4
Foothill	Haven-Devore Fwy	Art.	6	40-57,000	43,000	0.9-1.3
Arrow Rte.	Vineyard- Archibald	Art.	4	11,000	26,000	0.4
	Haven-Devore Fwy	Art.	4	16,000	26,000	0.6
7th Street	Rochester- Devore Fwy.	Art.	4	29,000	28,000	1.0
6th Street	Vineyard-Haven	Sec.	4	25,000	26,000	1.0
	Haven-Rochester	Art.	6	20,000	43,000	0.5
4th Street	Vineyard- Devore Fwy	Art.	6	17,000	43,000	0.4

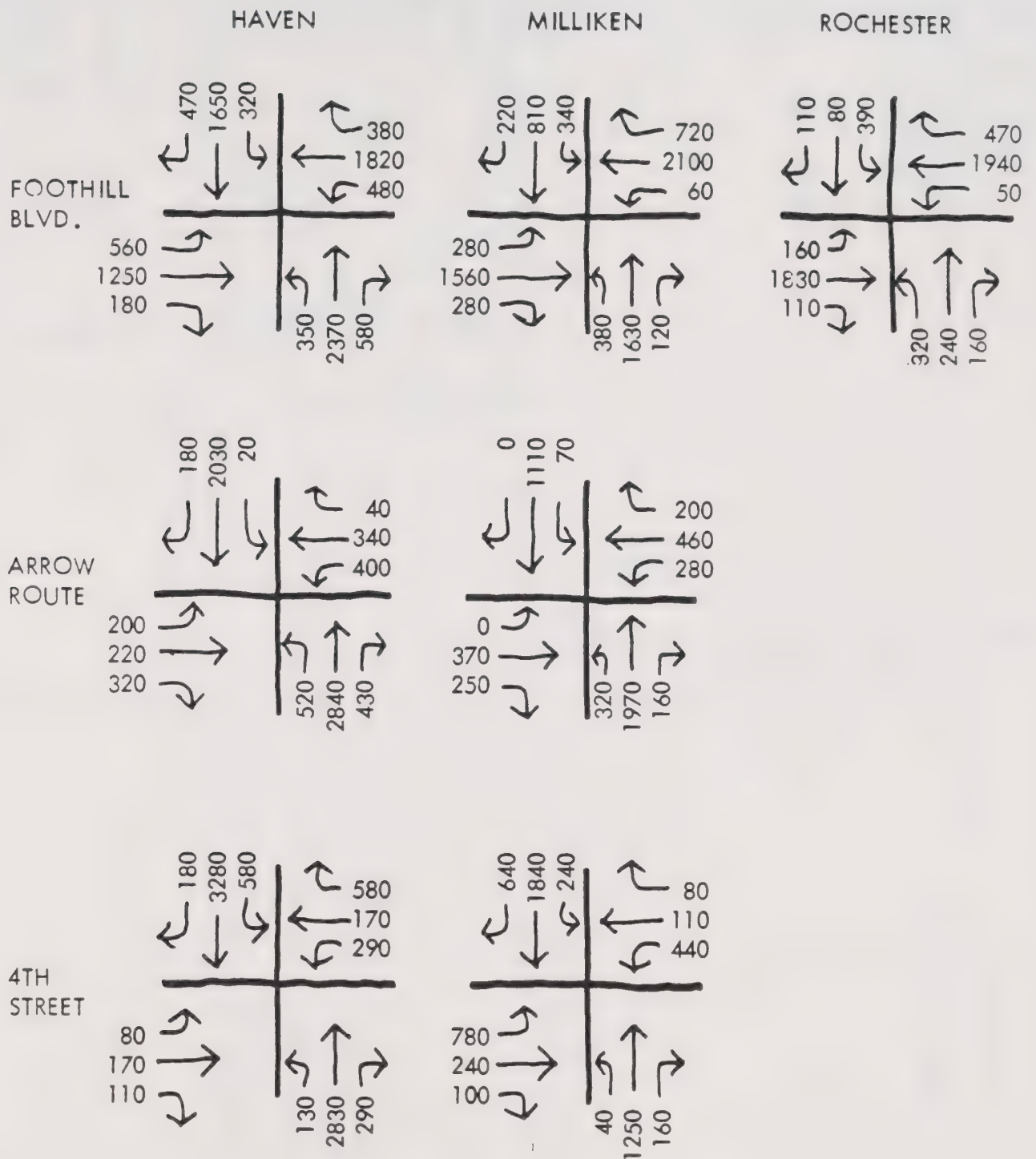
### PEAK PERIOD INTERSECTION VOLUMES

Figure A-2 indicates seven intersections within the Industrial Area that were initially selected for analysis and peak period turning volumes and the volumes projected for them. The projected peak hour traffic volumes were utilized to determine the number of lanes needed for the intersection to operate at Level of Service "D". For each direction of traffic, critical flows are determined by taking the maximum through traffic plus opposing left-turn volumes for each direction of traffic. This volume is compared with a standard intersection capacity operating at Level of Service "D".

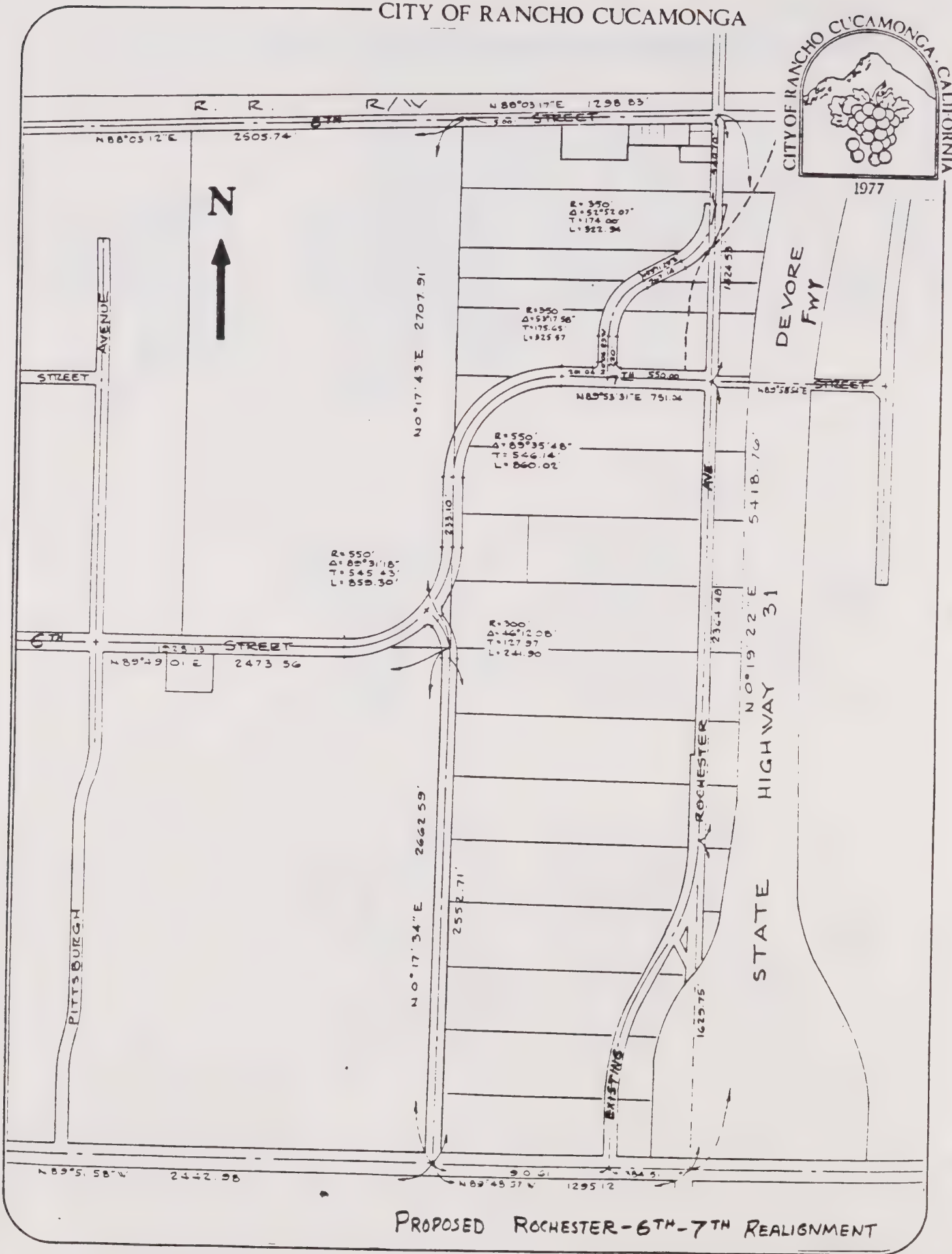


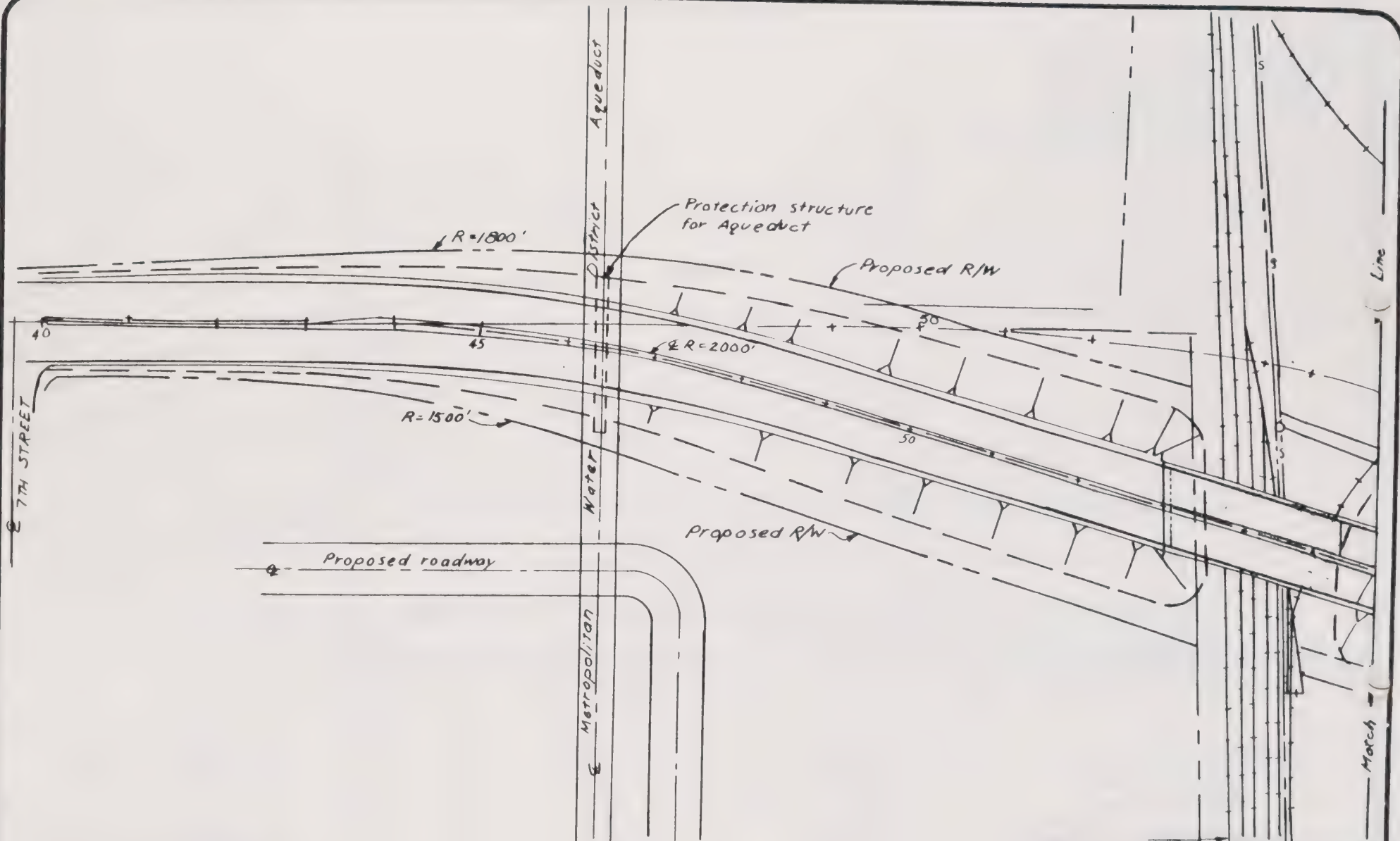
FIG. A-2

PROJECTED INTERSECTION PM PEAK HOUR TRAFFIC VOLUMES



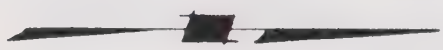
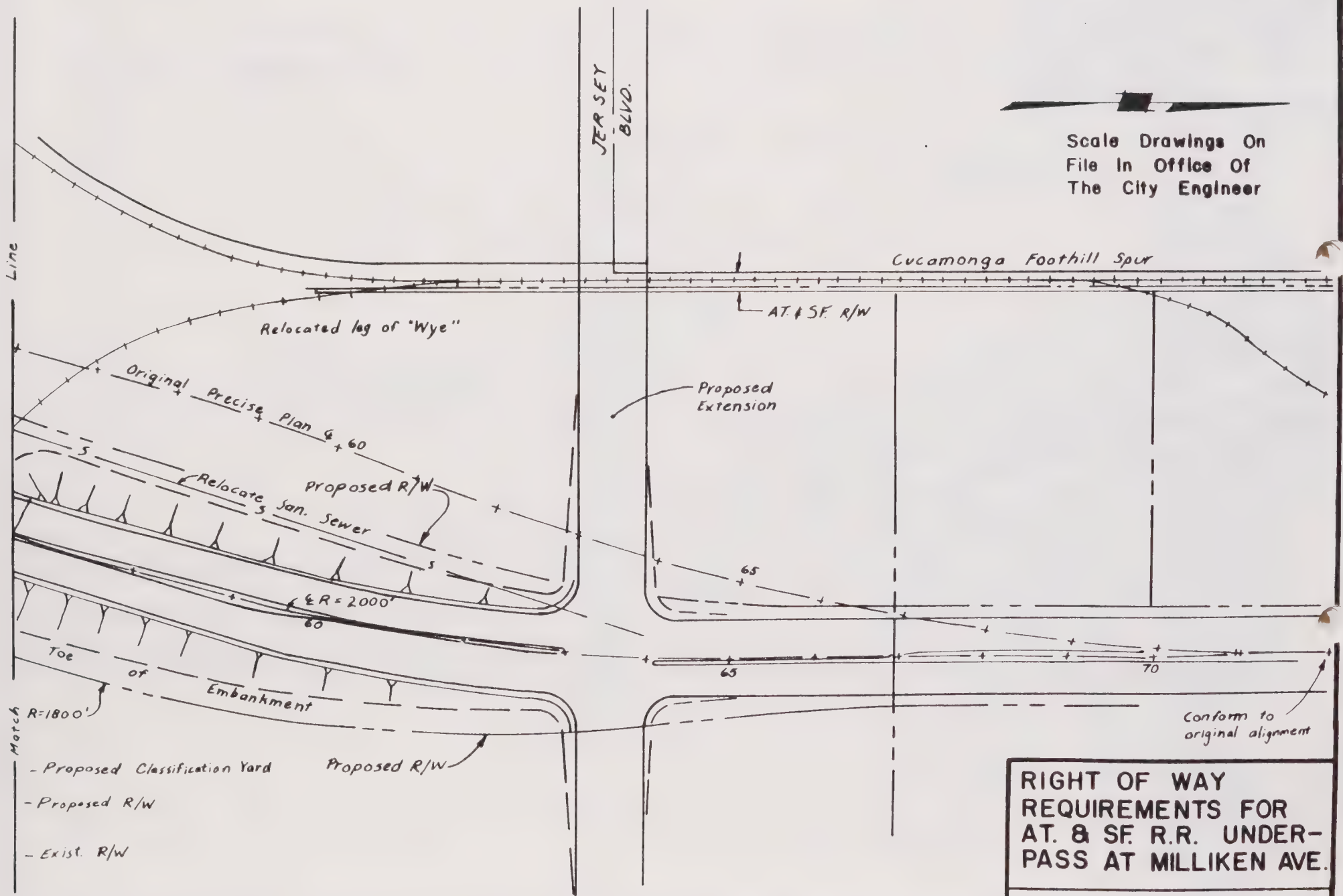
CITY OF RANCHO CUCAMONGA





RIGHT OF WAY  
REQUIREMENTS FOR  
AT. & SF. R.R. UNDER-  
PASS AT MILLIKEN AVE.

C-2



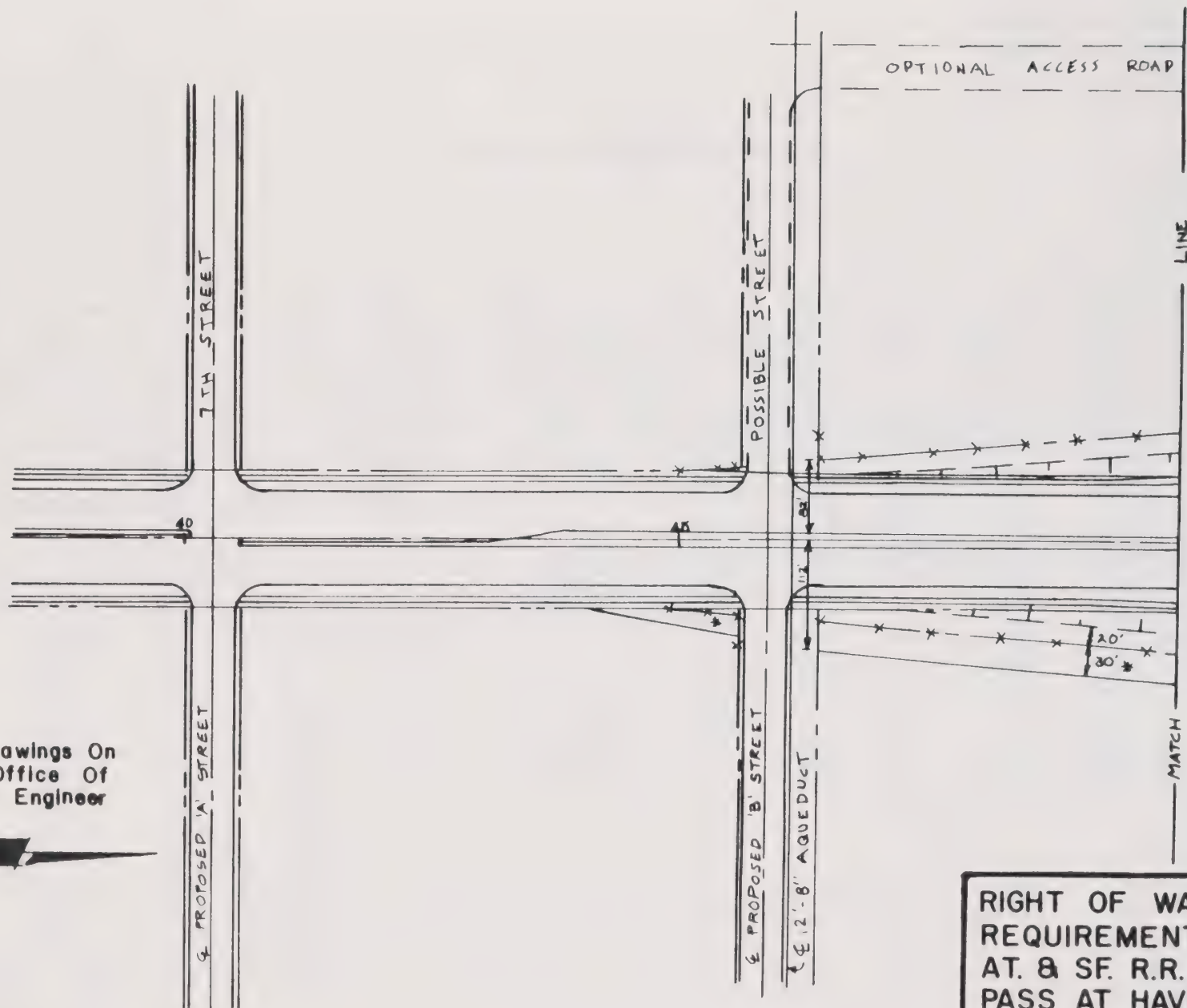
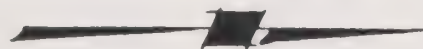
Scale Drawings On  
File In Office Of  
The City Engineer

**RIGHT OF WAY  
REQUIREMENTS FOR  
AT. & SF R.R. UNDER-  
PASS AT MILLIKEN AVE.**

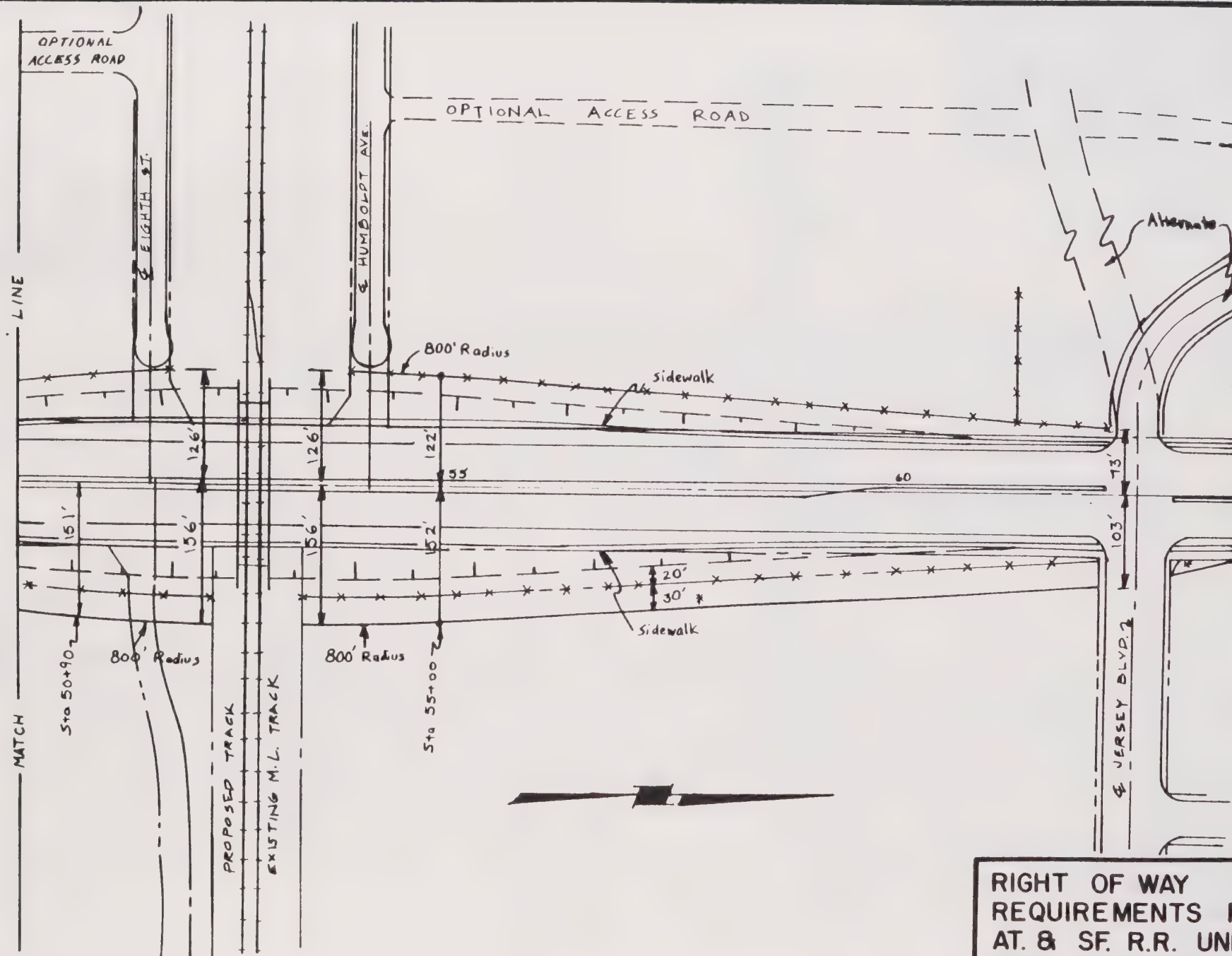
Appendix C



Scale Drawings On  
File in Office Of  
The City Engineer



RIGHT OF WAY  
REQUIREMENTS FOR  
AT. & SF. R.R. UNDER-  
PASS AT HAVEN AVE.



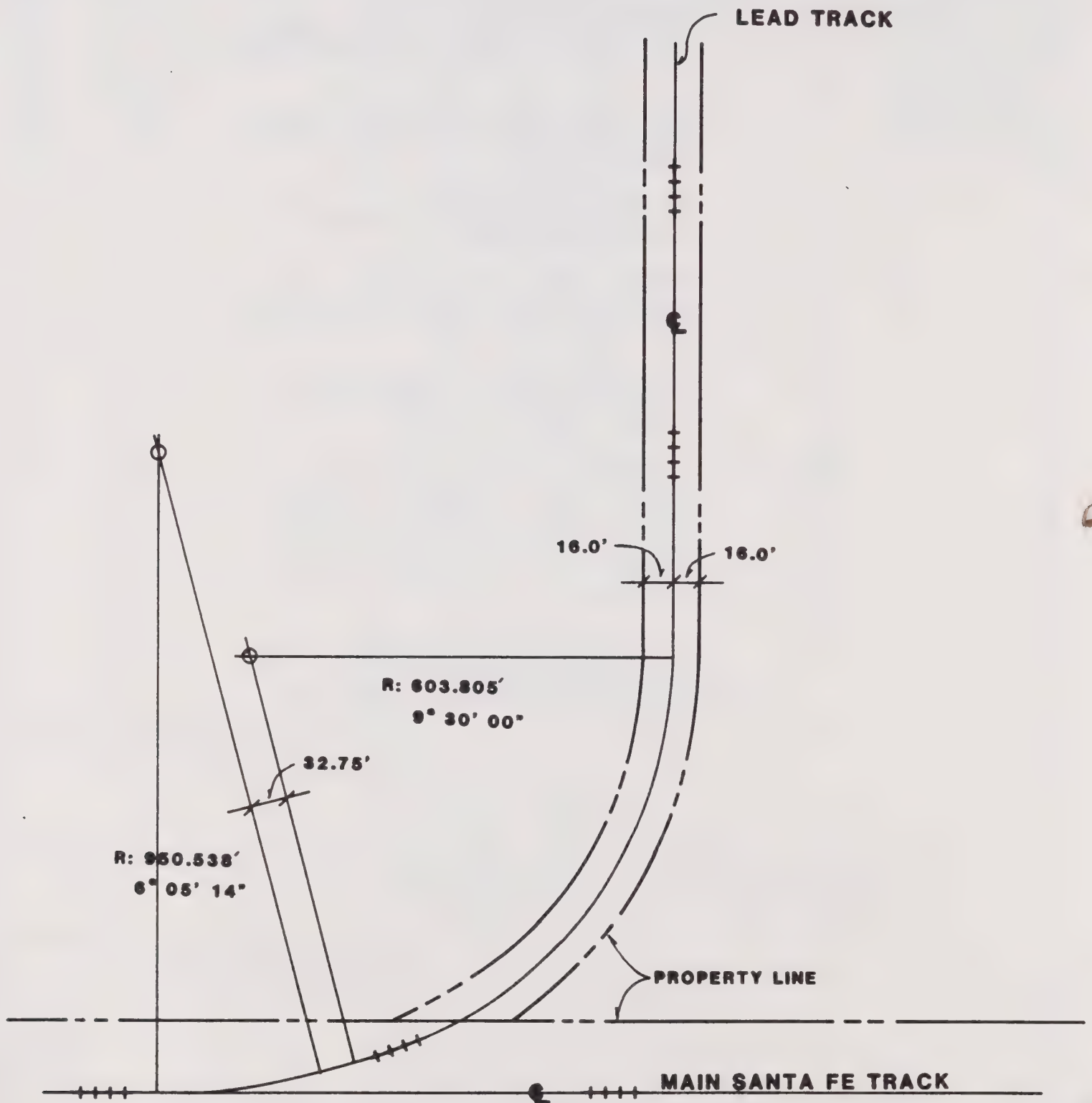
RIGHT OF WAY  
REQUIREMENTS FOR  
AT. & SF. R.R. UNDER-  
PASS AT HAVEN AVE.

## APPENDIX D

The following are fix Rail Service Standards which should be considered in the site design of projects with or adjacent to Rail Service Use:

- a. Easement width for a Lead Line single rail tract 32', and for a double rail tract 41'.
- b. Rail lead tract and nearby street elevation shall approximate one another in height.
- c. The minimum radius of curvature for tract shall be 180'.
- d. Maximum permissible gradient along spur tracks shall be no greater than 1-1/2 to 2%.
- e. Dock height shall be set between 4-1/2 to 5 feet above top of rail of the spur track.
- f. At grade road crossings by railroads should be avoided wherever possible. Rail crossing and any spur construction must be approved by the railroad and the Public Utility Commission.
- g. Spur trackage is not permitted along any building frontage and must be confined to the side or rear portions of the buildings.

# LEAD TRACK DESIGN SPECIFICATIONS





LINKING PLAZAS  
AND WALKWAYS

DIAGONAL PEDESTRIAN  
AND VISUAL CORRIDOR

ENRICHED PEDESTRIAN  
WALKWAYS

CORNER FOCAL  
ARCHITECTURE  
AND USES

REAR ACCESS  
PARKING

SHARED PEDESTRIAN PLAZAS  
BUILDINGS OPEN TO OUTDOOR SPACES

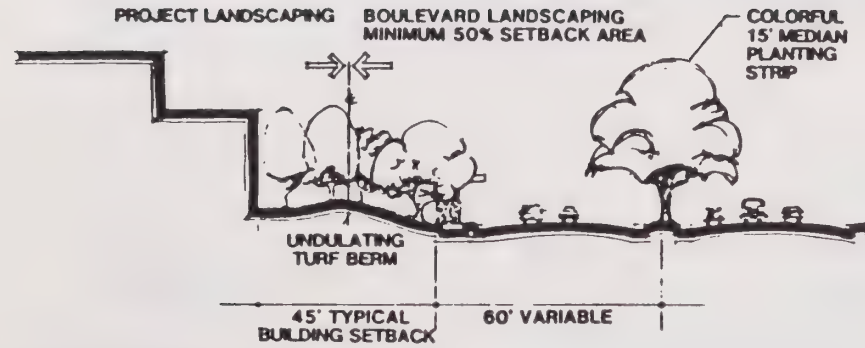
MASTER PLANNED  
CONTIGUOUS PARCELS

FRONT DOOR  
VISIBLE TO STREET

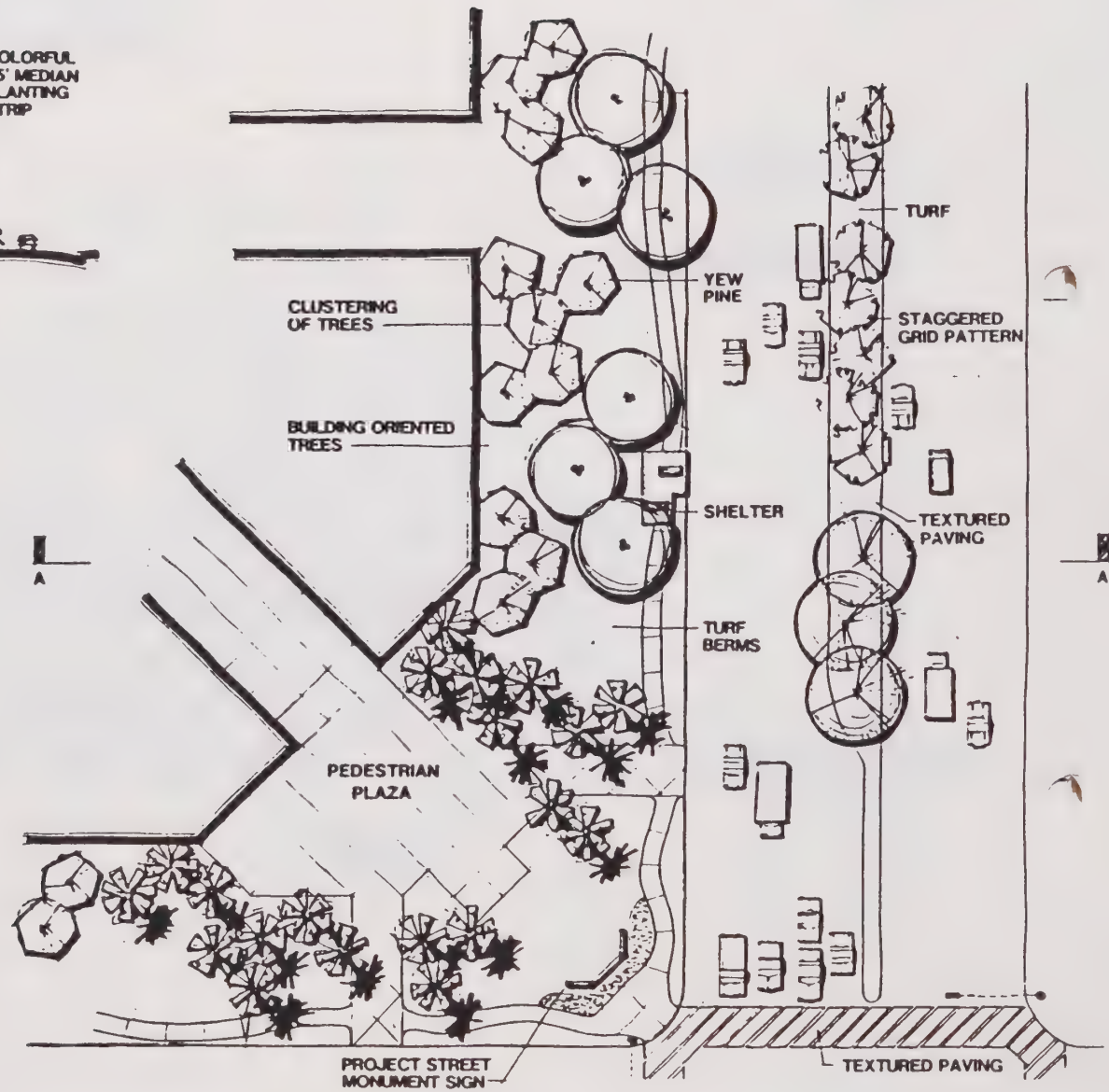
SHARED DRIVE  
ACCESS/PARKING

# RANCHO CUCAMONGA HAVEN AVENUE OVERLAY DISTRICT MIDBLOCK SCENES

"low and midrise buildings."



SECTION A

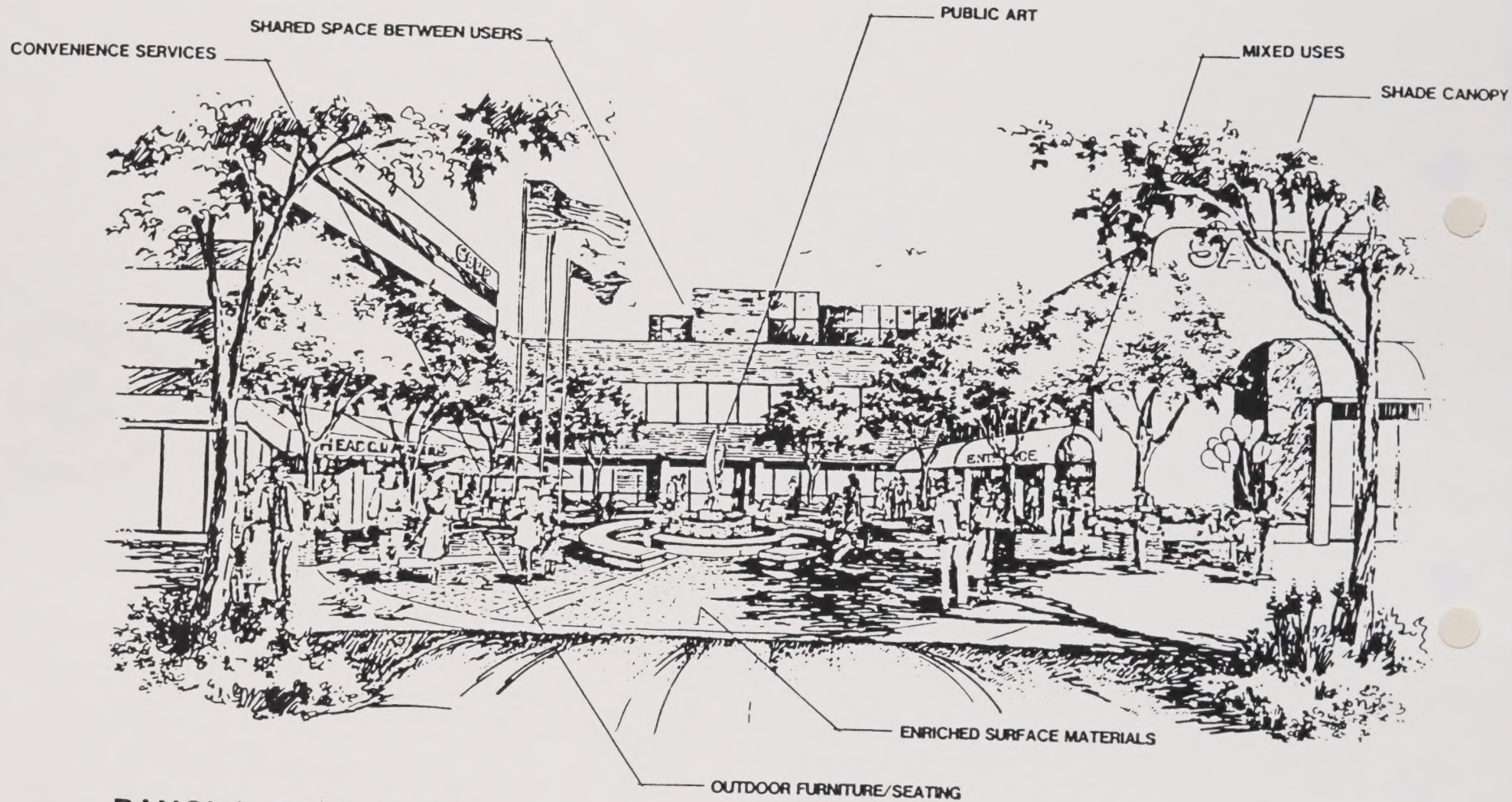


PLAN VIEW

# RANCHO CUCAMONGA HAVEN AVENUE OVERLAY DISTRICT STREETSCAPE

"...create visual interest, enhance architecture and buffer parking."





RANCHO CUCAMONGA  
HAVEN AVENUE OVERLAY DISTRICT  
PEDESTRIAN PLAZA

"... an attractive, comfortable, and functional setting with a sense of place."

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